



**Santa Cruz County Regional  
Transportation Commission's  
BICYCLE ADVISORY COMMITTEE**

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**AGENDA**

**Monday, June 11, 2018**

**6:00 pm to 8:30 pm**

<b>RTC Office 1523 Pacific Ave Santa Cruz, CA 95060</b>
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**COMMITTEE MEMBERSHIP**

<u>Member</u>	<u>Alternate</u>	<u>Representing</u>
Grace Voss	Janneke Strause	District 1
Shea Johnson	Casey Beyer	District 2
Peter Scott	Will Menchine	District 3
Vacant	Vacant	District 4
Rick Hyman	Vacant	District 5
Vacant	Vacant	City of Capitola
Amelia Conlen, Chair	Vacant	City of Santa Cruz
Vacant	Vacant	City of Scotts Valley
Murray Fontes	Vacant	City of Watsonville
Kira Ticus	Piet Canin	Ecology Action
Leo Jed	Jim Langley	Comm. Traffic Safety Coalition

The majority of the Committee constitutes a quorum for the transaction of business.

1. Call to Order
2. Introductions
3. Announcements – RTC staff
4. Oral communications – members and public

*The Committee will receive oral communications during this time on items not on today's agenda. Presentations must be within the jurisdiction of the Committee, and may be limited in time at the discretion of the Chair. Committee members will not take action or respond immediately to any Oral Communications presented, but may choose to follow up at a later time, either individually, or on a subsequent Committee agenda.*

5. Additions or deletions to consent and regular agendas

## CONSENT AGENDA

*All items appearing on the consent agenda are considered to be minor or non-controversial and will be acted upon in one motion if no member of the Committee or public wishes an item be removed and discussed on the regular agenda. Members of the Committee may raise questions, seek clarification or add directions to Consent Agenda items without removing the item from the Consent Agenda as long as no other committee member objects to the change.*

6. Approve draft minutes of the April 9, 2018 Bicycle Advisory Committee meeting (pages 5-8)
7. Accept summary of Hazard Reports (pages 9-16)

## REGULAR AGENDA

8. North Coast Rail Trail: Project Update and Schedule for Release of Draft Environmental Impact Report (EIR) – Grace Blakeslee, RTC Transportation Planner (pages 17-22)
9. Measure D: Five-Year Programs of Projects for Regional Projects Update – Anais Schenk, RTC Transportation Planner (pages 23-36)
10. Procedure for updating committee on ongoing projects – Anais Schenk, RTC Transportation Planner
11. Green Hills / Glen Canyon Paving Project – Anais Schenk, RTC Transportation Planner (pages 37-58)
12. Pending new member nominations – Anais Schenk, RTC Transportation Planner (pages 59-78)
13. Updates related to Committee functions
14. Adjourn

**NEXT MEETING:** The next Bicycle Committee meeting is scheduled for August 13, 2018 from 6:00pm to 8:30pm at the RTC office, 1523 Pacific Ave, Santa Cruz, CA.

### HOW TO REACH US

Santa Cruz County Regional Transportation Commission  
1523 Pacific Avenue, Santa Cruz, CA 95060  
phone: (831) 460-3200 / fax (831) 460-3215  
email: [info@sccrtc.org](mailto:info@sccrtc.org) / website: [www.sccrtc.org](http://www.sccrtc.org)

### AGENDAS ONLINE

To receive email notification when the Bicycle Committee meeting agenda packets are posted on our website, please call (831) 460-3209 or email [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org) to subscribe.

### ACCOMMODATIONS FOR PEOPLE WITH DISABILITIES

The Santa Cruz County Regional Transportation Commission does not discriminate on the basis of disability and no person shall, by reason of a disability, be denied the benefits of its services, programs, or activities. This meeting location is an accessible facility. If you wish to attend this meeting and require special assistance in order to participate, please contact RTC staff at 460-3200 (CRS 800/735-2929) at least three working days in advance of this meeting to make arrangements. People with disabilities may request a copy of the agenda in an alternative format. As a courtesy to those person affected, Please attend the meeting smoke and scent-free.

**SERVICIOS DE TRADUCCIÓN/TRANSLATION SERVICES**

*Si gusta estar presente o participar en esta junta de la Comisión Regional de Transporte del condado de Santa Cruz y necesita información o servicios de traducción al español por favor llame por lo menos con tres días laborables de anticipo al (831) 460-3200 para hacer los arreglos necesarios. (Spanish language translation is available on an as needed basis. Please make advance arrangements (at least three days in advance by calling (831) 460-3200.*

**TILE VI NOTICE**

*The RTC operates its programs and services without regard to race, color and national origin in accordance with Title VI of the Civil Rights Act. Any person believing to have been aggrieved by the RTC under Title VI may file a complaint with RTC by contacting the RTC at (831) 460-3212 or 1523 Pacific Avenue, Santa Cruz, CA, 95060 or online at [www.sccrtc.org](http://www.sccrtc.org). A complaint may also be filed directly with the Federal Transit Administration to the Office of Civil Rights, Attention: Title VI Program Coordinator, East Building, 5th Floor-TCR, 1200 New Jersey Ave., SE, Washington, DC 20590.*

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Santa Cruz County Regional  
Transportation Commission's

BICYCLE ADVISORY COMMITTEE

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Minutes

Monday, April 9, 2018  
6:00 pm to 8:30 pm

RTC Office  
1523 Pacific Ave  
Santa Cruz, CA 95060

1. Call to Order: Chair, Amelia Conlen called the meeting to order at 6:05 pm.

2. Introductions

**Members Present:**

Amelia Conlen, City of Santa Cruz, Chair  
Grace Voss, District 1  
Janneke Strause, District 1 (Alt)  
Shea Johnson, District 2  
Will Menchine, District 3 (Alt.)  
Rick Hyman, District 5  
Murray Fontes, City of Watsonville  
Kira Ticus, Ecology Action/Bike-to-Work  
Leo Jed, CTSC

**Staff:**

Cory Caletti, Sr. Transportation Planner  
Anais Schenk, Transportation Planner  
Sarah Christensen, Sr. Engineer

**Guests:**

Kelly Bond  
Becky Steinbruner  
Theresia Rogerson, CTSC

**Unexcused Absences:**

**Excused Absences:**

Casey Beyer, District 2 (Alt.)  
Peter Scott, District 3  
Jim Langley, CTSC (Alt.)  
Piet Canin, Ecology Action/Bike to Work (Alt)

**Vacancies:**

District 4 – Voting and Alternate  
District 5 – Alternate  
City of Santa Cruz – Alternate  
City of Scotts Valley – Voting and Alternate  
City of Capitola – Voting and Alternate  
City of Watsonville – Alternate

3. Announcements – Cory Caletti announced the unfortunate passing of Kem Akol, voting member of the Bicycle Advisory Committee (Committee) for District 4 and previously, for District 1. Kem was a member of the Committee for 25 years and a tireless advocate for bicyclists in the community. Following a moment of silence, a condolence card for Kem's family was distributed for Committee members to sign. Cory also announced that after 15 years she would be stepping down as staff to the Committee and that Anais Schenk will be the new staff person.

Anais Schenk announced that the Final Draft of the 2040 Regional Transportation Plan (RTP) was approved by the Regional Transportation Commission on April 5, 2018. The next step is for the

Association of Monterey Bay Area Governments (AMBAG) to certify the Environmental Impact Report (EIR) which is currently scheduled for June 13, 2018. After the EIR has been certified by AMBAG, RTC will consider adoption of the EIR findings and adoption of the final 2040 RTP.

4. Oral communications – Amelia Conlen announced she has accepted a position with Ecology Action, but would remain on the Committee representing the City of Santa Cruz. Becky Steinbruner asked the Committee to consider a grant to provide helmets and lights to farm workers as well as to write a letter to the County to request the installation of bicycle racks as part of the Aptos Village improvements. She also voiced concerns about the bus stop relocation for the inbound 71 bus at Trout Gulch and Soquel which she commented does not have a pullout large enough for buses to clear the bike lane. She also stated that she observed motorists queuing in the bike lane at this intersection for right turns. She asked the Committee to provide some recommendations for improvements to safety for bicyclists at that intersection.
5. Additions or deletions to consent and regular agendas – Anais Schenk requested that item 15, Draft “5-year Program of Projects” and item 17, Update on Vision Zero Program be moved to follow item 11, Officer Elections.

### **CONSENT AGENDA**

A motion (Hyman/Fontes) was made to approve the consent agenda. The motion passed unanimously with members Conlen, Voss, Johnson, Hyman, Fontes, Ticus, Menchine and Jed voting in favor.

6. Approve draft minutes of the December 11, 2017 Bicycle Advisory Committee meeting (pages 5-8)
7. Accept summary of Hazard Reports (pages 9-14)
8. Approve recommendation to the RTC to approve Ecology Action’s Transportation Development Act allocation request for \$60,000 for the Bike to Work Program (pages 15-32)
9. Approve recommendation to the RTC to approve the Health Service Agency’s TDA allocation request for \$130,000 for the Community Traffic Safety Coalition and the Ride ‘n Stride Bicycle and Pedestrian Education Programs (pages 33-60)

### **REGULAR AGENDA**

10. Report on new appointments, reappointments, resignations, and updated roster – Cory Caletti, RTC Senior Transportation Planner summarized the current status of appointments which include some recent reappointments and a number of vacancies. A recruitment process will start this month to fill vacancies. Janneke Straus asked about the appointment process. Cory replied that interested individuals are asked to attend one or two Committee meetings before applying, but it is not required. Applications are online and can be submitted to Anais Schenk once completed. Becky Steinbruner asked about the amount of time required for participation in the Committee. Amelia Conlen replied that a minimum of two hours every two months is required but that there are opportunities for further engagement in ad-hoc committees for those who are interested. Becky Steinbruner commented that the Committee should consider creating an ad-hoc committee to follow the Portola Specific Plan process.
11. Officer Elections – Cory Caletti, RTC Senior Transportation Planner summarized the officer election process. Formerly officers were elected every year; however, the Rules and Regulations

were recently updated to allow for two year appointments. Cory appreciated Amelia Conlen for her service and asked for nominations for the Chair and Vice-Chair positions. A motion was made (Ticus/Fontes) to nominate Amelia Conlen for Chair. The motion passed unanimously with members Conlen, Voss, Johnson, Hyman, Fontes, Ticus and Jed voting in favor. Another motion was made (Ticus/Voss) to nominate Murray Fontes for Vice-Chair. The motion passed unanimously with members Conlen, Voss, Johnson, Hyman, Fontes, Ticus, Menchine and Jed voting in favor. The Chair and Vice-Chair will serve through March, 2020.

12. Draft "5-year Program of Projects" recommendations for Measure D funding allocations: Highway Corridors, Active Transportation and Rail Corridor investment categories, the San Lorenzo Valley Highway 9 Corridor Improvements and the Highway 17 Wildlife Crossing – Sarah Christensen, Senior Transportation Engineer, presented the 5-year program for Highway 1. Changes to the draft program of projects include: consolidating the auxiliary lane projects for Park Avenue to Bay/Porter and State Park Drive to Park Avenue, adding funding for these projects earlier in the project life to make them competitive for grant funding sources and moving funds for the Mar Vista Drive overcrossing out one year. Cory Caletti, Senior Transportation Planner reported that the RTC is currently conducting a multimodal study for the Highway 9 / San Lorenzo Valley Corridor which will include preliminary recommendations for projects. Cory also reported on the funding for the Rail Corridor regional project which includes continued funding for the Unified Corridor Investment Study, the bridge analysis and rehabilitation as well as storm damage repair. Cory also reported on the Active Transportation/Monterey Bay Sanctuary Scenic Trail – Rail Trail regional 5-year program which included a recommendation to add \$200,000 for Segment 10 (17<sup>th</sup> Avenue to 47<sup>th</sup> Avenue) for preliminary design and right-of-way research in FY 22/23. A motion was made (Hyman/Voss) to express support to the RTC for allocation of funding towards Segment 10. The motion passed unanimously with members Conlen, Voss, Johnson, Hyman, Fontes, Ticus, Menchine and Jed voting in favor.

The Committee requested that in the future the 5 year program of projects include reports on what was allocated in previous years and what was spent versus carried over into future years. Cory replied that RTC staff would further refine the allocation spreadsheets that will go to the RTC based on feedback and provide approved Measure D allocation spreadsheets to the Committee in June for members' information. The Committee also requested more information on the Highway 9 / San Lorenzo Valley Corridor. Cory mentioned that the project managers were planning on attending the Committee meeting in June to present on that project.

13. Update on Vision Zero Program – Theresia Rogerson, Health Services Agency staff provided a background on the philosophy of Vision Zero and reported on the progress of the County Vision Zero program. Recently the City of Watsonville passed a resolution to implement a Vision Zero program. The City of Santa Cruz and the County of Santa Cruz are also in discussion regarding adoption of a resolution and development of a program. The CTSC continues to collect anecdotal data and police reports to tally and map the locations of severe crashes and fatalities. The work program for the CTSC for the next two years, approved as part of item #9 above, includes continuing efforts around implementing Vision Zero.
14. Nomination of member for participation in Downtown Watsonville Complete Streets Plan – Murray Fontes provided a brief synopsis of the Watsonville Downtown Complete Streets project and noted that the outreach process included forming a stakeholder committee. He asked for volunteers from the Committee to participate in the stakeholder committee. Amelia Conlen, Janneke Strause, Kira Ticus and Theresia Rogerson all expressed interest.
15. Procedure for sharing contact information – Anais Schenk noted that in the past the roster was included in every packet with the contact information for members of the Committee. However,

the current RTC practice is to redact contact information; therefore, the current roster does not include emails or phone numbers in recent packets. She asked the Committee if they would like contact information to continue to be published in the packet or if they would like that information to be provided on a Google spreadsheet. The Committee indicated that they would prefer a Google spreadsheet.

16. Draft letter from the Bicycle Advisory Committee to the City of Santa Cruz recommending that the Branciforte Trail remain open – Rick Hyman summarized the issue that prompted the Committee to draft a letter: due to neighbor complaints the City of Santa Cruz began discussions about closing the Branciforte Creek Trail. In December, the Committee voted to draft a letter opposing the closure. After starting the draft letter Rick learned that the City was no longer considering closing the trail. However, since the Committee had voted to provide the letter he finished a draft for discussion and inclusion in the packet. Amelia Conlen provided additional background stating that while the City was no longer considering a full closure, there were still ongoing discussions with the neighbors. Will Menchine added that the path is now gated and shut at night. A motion (Jed/Fontes) was made to provide a letter to the City of Santa Cruz with language encouraging communication to resolve safety and access issues while keeping the path open and to consider lighting for the path. The motion passed with members Voss, Johnson, Hyman, Fontes, Ticus, Menchine and Jed voting in favor. Committee member Amelia Conlen abstained.
17. Status update on the Countywide Bicycle Wayfinding Project – Anais Schenk, Transportation Planner reported that input from the Committee and stakeholders had been incorporated into the signage database. Additionally, a scope change request was submitted to and approved by Caltrans and California Transportation Commission staff to account for the changed number of signs and locations. The next steps for the project include applying for the construction allocation and going out to bid for construction.
18. Updates related to Committee functions - None
19. Adjourn – 8:20 pm

**NEXT MEETING:** The next Bicycle Committee meeting is scheduled for June 11, 2018 from 6:00pm to 8:30pm at the RTC office, 1523 Pacific Ave, Santa Cruz, CA.

Minutes respectfully prepared and submitted by:  
Anais Schenk, Transportation Planner

June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
06/04/18	Danielle	Venable	<a href="mailto:Danielle.Venable@santacruzcounty.us">Danielle.Venable@santacruzcounty.us</a>	169 Capitola Rd	Harbor View Ct.	Santa Cruz	Rough pavement or Potholes	Water gathers in a low spot and the concrete is getting really bad. There's a hole developing and lots of cracks that are getting deeper.	DPW JBurr	06/05/18	
06/03/18	Adam	Millard-Ball	<a href="mailto:adammb@hotmail.com">adammb@hotmail.com</a>	636 Bonita Drive	Freedom blvd	Aptos	Rough pavement or potholes	Large pothole that swallows a bicycle tire. This is especially dangerous because it is on a downhill, and just beyond a speed bump that impairs visibility. The speed bump also has poor visibility, as the white markings are faded.	DPW	06/04/18	On 06/04/18 DPW replied: I am in receipt of your issues for the bike lane on Bonita Dr. I will be forwarding your report to out to our Brommer maintenance yard Thank you for taking the time to report this issue.
05/31/18	Tom	Roberts	<a href="mailto:rotom@ucsc.edu">rotom@ucsc.edu</a>	Bay St	Escalona Drive	Santa Cruz	Plant overgrowth or interference	Tons of overgrown plants in the bike lane on Bay St. going south in the area north of Escalona Dr. Interferes with using the full extent of the lane (to a pretty extreme degree)	Jim Burr	06/01/18	6/1/2018 Jim Burr Replied: Tom, With this email I am notifying the appropriate staff to correct this issue. Thanks
05/31/18	Tom	Roberts	<a href="mailto:rotom@ucsc.edu">rotom@ucsc.edu</a>	Ocean Street	Hwy 1	Santa Cruz	Rough pavement or potholes, Hazardous drain grate	There are two hazards in the bike lanes on Ocean Street around the junction with Highway 1 (and to be frank, the whole area feels very dangerous as a bicyclist) In the southbound lane, there is a pothole in the street directly in the path of the bike lane in front of the offramp. Frequently it requires swerving into the traffic lane to avoid, because cars creep around the corner from the offramp. In the NB lane in between the 2 overpasses, is a hazardous drain grate in the bike lane. The paved part of the bike lane is already extremely narrow, but I have had a number of close calls with this drain gate. Truthfully, this entire stretch of Ocean feels pretty dangerous. (Finally, there is a trash can that is in the bike lane ALL THE TIME on the northbound side. Is this an appropriate place to report it? I have a few places where this is a habitual issues	Jim Burr	06/01/18	6/1/2018 Jim Burr Replied: Tom With this email I am notifying the appropriate staff to correct this issue. Thanks

June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
05/31/18	Peter	Stanger	<a href="mailto:pjlsb@att.net">pjlsb@att.net</a> t	Freedom Blvd	Hwy 1	Freedom	"other"	This intersection was designed in the late 60's and built in the early 70's when bicycling wasn't even a consideration. Freedom Blvd was constructed as wide as Highway 1 and the on-ramps to Highway 1 were constructed to allow motor vehicles to easily access the highway with minimal reduction in speed with a wide and sweeping curve to the on-ramp. The Caltrans Safe Intersections Guidelines now suggests that such intersections be made a sharp right angles to induce reduction in motor vehicle speed. The problem that I am reporting is that the "green lane" is nearly 180 feet long as it passes the conspicuously wide freeway on-ramp. The "green lane" is on a uphill slope that slow the forward progress of cyclists. All cyclists using the Pacific Coast Bike Route must cross this area. Freedom Blvd is posted 45mph and the motor vehicles don't reduce speed when a cyclist is in bike lane. Freedom Blvd roadbed has a excessively wide paved area on right hand side marked w/white to indicate to motorists that the area is NOT a lane. Motorists, upon seeing cyclists laboring uphill in "green lane" simply drive into the right hand paved area to continue onto the on-ramp so they won't have to reduce speed or wait for the cyclists to clear the access to the on-ramp. For cyclists, this creates the unnerving experience of cars passing at 45mph on the left to get to the on-ramp & cars passing at 45mph to the right to get to the on-ramp. The wide paved area to the right of the motor vehicle lane that currently only has white paint to deter this driving behavior needs to have additional deterrents, such as white bots or the pavement grinding to alert the motorists that it's not part of the motor vehicle lane. Better yet, the freeway should have been constructed using the criteria of the Caltrans Safe Intersection guidelines booklet.	CalTrans & DPW	05/31/18	
05/25/18	Peter	Stanger	<a href="mailto:pjlsb@att.net">pjlsb@att.net</a> t	Soquel Drive	Trout Gulch	Aptos	Debris on shoulder or bikeway, Bikeway not clearly marked	Comments at end of this document	DPW	05/25/18	Responses at end of document

June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
05/13/18	Cary	Townsend	<a href="mailto:carytownsend@yahoo.com">carytownsend@yahoo.com</a>	1100 Green Hills Rd	Falcon Ridge Rd	Scotts Valley	Potholes		DPW/ Scotts Valley	05/25/18	
05/13/18	Rick	Hyman	<a href="mailto:bikerick@att.net">bikerick@att.net</a>	Hospital Dr	Soquel Drive	Soquel	Traffic Signal problem	The traffic signal at the exit of Dominican Hospital to travel onto Soquel Ave. does not change for bicycles. I reported this problem almost 2 years ago, and it has not been fixed yet.	County/ DPW	05/14/18	5/15/2018 DPW replied: I am in receipt of your traffic signal issue at Dominican Hospital and Soquel Drive. I will be forwarding your request to Road Traffic Transportation engineers for their attention and reply.
05/12/18	Sandrine	Georges	<a href="mailto:sandrinesantacruz@yahoo.com">sandrinesantacruz@yahoo.com</a>	4343 Soquel Ave		Soquel	Debris on shoulder or bikeway	Small pieces of broken glass in bike lane	County DPW	05/14/18	5/15/2018 DPW replied: I am in receipt of your issue of broken glass in the bikeway at 4343 Soquel Drive. I will be forwarding your request for work to our Brommer maintenance yard (831) 477-3999 . Thank you for taking the time to report this issue.
05/07/18	Ryan	Moroney	<a href="mailto:ryan_moroney@yahoo.com">ryan_moroney@yahoo.com</a>	Soquel Ave	Trout Gulch	Aptos	Debris on shoulder	The Aptos Village project appears to be creating a lot of debris along Trout Gulch and Soquel. The Contractor should clear the bike lane/ shoulder of debris weekly at minimum as part of BMPs. In addition, a lot of debris (glass, gravel, etc.) has built up along Soquel Drive along entire north bound commute length (Aptos to Santa Cruz) and needs to be street swept.	County DPW	05/07/18	5/7/2018 DPW replied: I am in receipt of your issue debris at Trout Gulch part of the Aptos Village Improvements Project. I will be forwarding this request to our Construction division and Design division engineers of Public Works . I will also be forwarding your request for ...Sweeping the bike lanes on Soquel Drive to our Brommer maintenance yard

June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
05/04/18	Brad	Cramer	<a href="mailto:cramer4eyes@yahoo.com">cramer4eyes@yahoo.com</a>	Bean Creek Rd	Scotts Valley Drive	Scotts Valley	Overgrowth or interference, bikeway not clearly marked	Bushes are overgrown into the entirety of the bike lane forcing bikes into a 30mph lane of traffic while pedaling uphill.	Scotts Valley Public Works	05/07/18	
05/04/18	Brad	Cramer	<a href="mailto:cramer4eyes@yahoo.com">cramer4eyes@yahoo.com</a>	Scotts Valley Drive	Bean Creek	Scotts Valley	Debris on shoulder	The bike lanes on Scotts valley drive are covered with gravel, broken glass, and construction detritus. The construction companies do a very poor job of cleaning up their mess and creates a constant flat tire hazard	Scotts Valley Public Works	05/07/18	5/7/2018: Jessica Kahn stated: Mr. Cramer, Thank you for bringing this to our attention. The street sweeper is being scheduled this week for to service the entirety of Scotts Valley Drive. Please let me know if you have any additional questions or concerns
04/30/18	Noe	Parker	<a href="mailto:noeparker17@gmail.com">noeparker17@gmail.com</a>	Soquel Ave	Porter Street	Soquel	Debris in bike path	Organic debris (branches, leaves, acorns, etc.), gravel, and other obstructions along Soquel Drive in the bike lane. Have popped my tire on my commute twice in the past week	DPW	04/30/18	On 5/1/2018 DPW stated: . I will be forwarding your request for work to our Brommer maintenance yard (831) 477-3999. Thank you for taking the time to report this issue.
4/26/2018	Jessica	Guild	<a href="mailto:jroseg2004@yahoo.com">jroseg2004@yahoo.com</a>	1521 Mission Street	Bay	Santa Cruz	Rough pavement or potholes	Pothole in bike lane at the intersection of Bay and Mission requires biker to veer out of bike lane and into vehicle lane	Jim Burr	04/26/18	4/26/18 Forwarded to Street Maintenance
4/26/2018	Jessica	Guild	<a href="mailto:jroseg2004@yahoo.com">jroseg2004@yahoo.com</a>	Glenn E. Coolidge Memorial Bridge		Santa Cruz	Rough pavement or potholes, Hazardous drain grate	There is a large bump as the road meets the bridge which can pop bike wheels if you hit it too fast coming down the hill. Additionally, the drainage in this spot is very bad - during large rain storms the bike lane is completely submerged requiring the biker to merge into the car lane.	Jim Burr	04/26/18	4/26/18 Forwarded to Street Maintenance on 4/27/2018: Richard Smith of the street department stated: We will grind that bump off this morning. The water issue will be addressed before next winter.



June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
4/10/2018	Cindy	Pierce	<a href="mailto:cindygpierce@gmail.com">cindygpierce@gmail.com</a>	Beach Street Bike Lane	3rd street to Pacific	Santa Cruz	Pedestrians unawareness	Most visitors to the boardwalk are unaware that there is a bike lane on Beach St parallel to the sidewalk. They wander aimlessly into the bike lane unaware that a bike is approaching. Often they literally walk backwards into the bike lane setting themselves up for a photo shoot, unaware that they are stepping off the curb into a traffic lane.	Jim Burr	04/11/18	On 4/12/18 Amelia state: I'm happy to report that the City has received funding to widen the trestle bridge over the San Lorenzo River. This project is currently in the design phase, and we hope to go to construction in late 2018. This is a long-standing issue and we are looking forward to providing a better facility. We have also received funding for design and permitting of Segments 8 and 9 of the rail trail, which includes the cycle track in front of the Boardwalk. This project is farther out and we still need to find construction funding, but we are aware of bike/ped conflicts and will be looking for ways to mitigate
4/25/2018	Steve	Matson	<a href="mailto:smatson@pabell.net">smatson@pabell.net</a>	Hames Road	Pleasant Valley	Fredom/Watsonville	Plant overgrowth /Interference	North side of Hames Rd. Over the top of the hill from Alfaros Winery. oak trees have grown into and over the road. The shoulder is completely overgrown. It is necessary to ride in the road to avoid getting hit by tree branches. There are tree branches broken off from trucks hitting them.	DPW	04/25/18	I am in receipt of your issue of trees in bike lane. I will be forwarding your request for work to our Brommer maintenance yard (831) 477-3999. Thank you for taking the time to report this issue.
04/17/18	Becky	Steinbruner	<a href="mailto:ki6tkb@yahoo.com">ki6tkb@yahoo.com</a>	Glen Canyon	Rodeo Gulch	County	Debris on shoulder or bikeway	ALOT of gravel fines strewn all over the roadway and on curves, due to recent oil and screen pothole filling work.	DPW	04/19/18	4/20/2018 DPW "I am in receipt of your bike lane hazard report forwarded from SCCRTC. I will be forwarding your hazard report out to our Brommer maintenance yard (831) 477-3999. Thank you for taking the time to report this issue."

June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
04/17/18	Becky	Steinbruner	<a href="mailto:ki6tkb@yahoo.com">ki6tkb@yahoo.com</a>	Soquel	Trout Gulch	County	Debris on shoulder or bikeway	Westbound bike lane has significant amount of crushed rock strewn about from cars spinning out in the crushed road base adjacent to the bike lane at the new traffic light intersection	DPW	04/19/18	4/20/2018 DPW "I am in receipt of your bike lane hazard report forwarded from SCCRTC. I will be forwarding your hazard report out to our Brommer maintenance yard (831) 477-3999. Thank you for taking the time to report this issue.
4/12/2018	Meiya	Sparks	<a href="mailto:mspark@students.kirby.org">mspark@students.kirby.org</a>	Encinal	River St.	Santa Cruz	Bike Lane Requested	"I am a senior at Kirby School, I am interested in getting a bike lane put in on Encinal for students who bike to school."	Jim Burr	04/12/18	On 4/26 J Burr replied: The City will make sure this is included in the Active Transportation Plan for evaluation and consideration as part of the overall system. Many projects from that plan are constructed installed annually, so this is not any kind of dismissal of the idea, but staff do need to prioritize on a Citywide basis.
4/10/2018	Cindy	Pierce	<a href="mailto:cindypierce@gmail.com">cindypierce@gmail.com</a>	Train Trestle Bridge	3rd street/East Cliff	Santa Cruz	Passage is too narrow	I would like to propose a railing that runs along the curb forcing pedestrians to make an effort before making a move. This is a long stretch of road so I see the dollars adding up, but something as simple as posts and 2 rows of draping ropes or wires would force people to notice before acting. It would take concerted effort to step over the rail or wires triggering people to notice what they are stepping into.	Jim Burr	04/11/18	4/12/2018 Amelia replied: I'm happy to report that the City has received funding to widen the trestle bridge over the San Lorenzo River. This project is currently in the design phase, and we hope to go to construction in late 2018. This is a long-standing issue and we are looking forward to providing a better facility. We have also received funding for design and permitting of Segments 8 & 9 of the rail trail, which includes the cycle track in front of the Boardwalk. This project is farther out and we still need to find construction funding, but we are aware of bike/ped conflicts and will be looking for ways to mitigate the issue.

June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
4/6/2018	Connie	Wilson	<a href="mailto:camt@cruzio.com">camt@cruzio.com</a>	Frederick, Water, Market	Broadway/Seabright/Goss	Santa Cruz	Rough pavement or potholes	1. Frederick Street between Star of the Sea Church and Broadway going towards the beach. In the bike lane tree roots have raised the pavement and it is not always visible and a hazard2. Water Street between Safeway and Sante Adarius brewery, also bumps in the bike lane that are not visible3. Market Street, very fast vehicle drivers, a share the road sign??? anything to slow the vehicles down would be appreciated as there are many cyclists and also peds that use this area between the freeway overpass and Avalon also lots of debris which I try to clean up as much as possible - scary with the cars speeding by.4. Market St/Goss intersection. Coming from town along Market Street, there is a ditch for cars to turn right. No room for a cyclist with the drainage ditch and no shoulder. 5. Water St/Seabright intersection is difficult for peds to cross, any chance of a flashing light for ped crossing?	Amelia Conlen	4/6/2018	4/10/18 Amelia Replied: Frederick St:streets maintenance staff is taking a look to see if there's anything they can do to smooth the roadway. Water St: same as above. Market St.:Would signage be most helpful for cars heading to town or away from town? It looks like in the outbound direction we have a 'Bikes May Use Full Lane' sign just before the curve. The City has developed plans for a sidewalk project for the west side of Market to fill in the gap between Avalon and Stoney Creek Road. This wouldn't include new bike facilities, but it would narrow the roadway, which can help to slow traffic speeds. Market St./Goss: We are applying for grant funding again this year for widening of the Market Goss Intersection. That project would include a bike lane and sidewalk at SE corner. Water St/Seabright: We don't have improvements planned for this intersection, we do have improvements funded at Water & Poplar, including bulb-outs and flashing beacons. This project, is planned to be constructed in 2019
02/21/18	Lilly Ann	Popken	<a href="mailto:lapedals2@aol.com">lapedals2@aol.com</a>	719 Cabrillo Hwy	bike path on way to Wilder		. urgent	Tree came down blocking bike path on way to Wilder Ranch. Marked as urgent because there has already been one accident.	CalTrans	02/2118	

June 2018 Bicycle Hazard Report

Date	First Name	Last Name	Contact Info	Location	Cross Street	City	Reported Hazards	Additional Comments	Forwarded To	Forwarded Date	Response
03/13/18	Stan	Hooper	hooper2@phc.net	230 Madrone Ave	Hwy 236	Boulder Creek	Bikeway not clearly marked, Sidewalk too narrow, No crosswalk or striping	An 8" high curb was added	CalTrans	3/13/2018	
03/07/18	David	Hausler	hausler@ucsc.edu	936 Coolidge Dr	UC Meadow bike path	UCSC	Debris on shoulder or bikeway	Often large chunks of asphalt in the bike traffic lane	DPW	3/7/2018	4/3/18 the basic response that I will send the service request to the maintenance yard.
05/2/18	Peter	Stanger	<p><u>Continued from above:</u> The storms that famously caused the collapse of Trout Gulch Rd @ Valencia Creek and other storm related havoc around SC County in 2016-17 also caused the hillside adjacent to the bend in the road of Soquel Dr to partially collapse into the bike lane and E/b traffic lane of Soquel Dr. Public Works cleared the E/b motorized traffic lane within weeks of the storms and placed a series of traffic warning sawhorses in the bike lane on this bend in the road. As weeks turned to months, the traffic sawhorses got hit and destroyed by motorized traffic and PW eventually removed{their remains}. The bike lane remained obstructed by dirt &amp; vegetation from the hillside as PW was too busy with whatever else they felt was more urgent than the safety of bicyclist using this area of roadway. Another year has passed and the painted white line that delineated the motorized traffic from the bike lane is obscured as motorists would curve around this bend in the roadway. Eventually the white line that delineated the bike lane was a mere shadow. This month PW repainted the white line that delineated the bike lane from the area east of the railroad trestle over-crossing of Soquel Drive just east of Aptos Village all the way to this bend in the roadway. At the bend of the road itself, they painted NOTHING AT ALL. The result is the bike lane ends just before this blind curve, and commences again about 75 yds again to the east where Soquel Dr straightens-out again. As currently exists, there is no bike lane, no sign that the bike lane ends, no sharrows, no "share the road" signage. Motorized traffic rarely travels the posted 25mph speed limit as it zooms around this blind curve &amp; sometimes suddenly comes-up upon a bicyclists. I have learned not expect much from PW when it comes to bicyclist safety. But this is really over-the-top. They couldn't clear the debris out of the bike lane, they they couldn't repaint the bike lane, they couldn't even post signage that the bike lane ends. Isn't there someone on the SCCRTC's Bicycle Advisory Committee that represents the 2nd District and the Aptos Area? Isn't that their job to see note and mitigate this issue?</p>								
05/31/2018	Anais Schenk	TO Peter Stanger:	<p>Thank you for submitting a hazard report and bringing this issue to our attention. County Public Works will respond as well, however I wanted to address your questions at the end of the report. The primary purpose of the Bicycle Advisory Committee is to advise the RTC on bicycle projects, programs and plans. While it is not their primary function on the committee, when advisory members see major maintenance issues that have already been reported to the jurisdiction but remain unaddressed then they may bring it to the committee's attention, particularly if the maintenance issue is affecting the basic function of a bicycle facility. As with any issue that is within the purview of another jurisdiction or agency we like to offer that agency or jurisdiction an opportunity to address the issue. We will track this report accordingly. I suggest that you also submit hazard reports for the other issues that you raised with Casey in order to provide county an opportunity to address those as well.</p>								
05/31/18	DPW	To Peter Stanger	<p>I am in receipt of your issues for the bike lane on Soquel Drive. I will be forwarding your report to out to our Brommer maintenance yard (831) 477-3999 and our Road perations division of Public Works for their review and reply. Thank you for taking the time to report this issue.</p>								

**TO:** Bicycle Advisory Committee

**FROM:** Grace Blakeslee, Transportation Planner

**RE:** North Coast Rail Trail– Project Update and Schedule for Release of Draft EIR

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## **RECOMMENDATIONS**

Staff recommends that the Bicycle Advisory Committee receive information about on the North Coast Rail Trail Draft Environmental Impact Report.

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## **BACKGROUND**

The 7.5-mile North Coast Rail Trail project is being implemented through a Federal Lands Access Program (FLAP) grant by the Federal Highway Administration's (FHWA) Central Federal Lands (CFL) Department. The 5.4-mile section from Wilder Ranch to Yellowbank/Panther Beach received full funding through the FLAP grant, the California Coastal Conservancy and the Land Trust of Santa Cruz County. The 2.1-mile section from Yellowbank/Panther Beach to Davenport and parking lots in Davenport and at Yellowbank/Panther Beach are additionally funded through the Land Trust and the RTC for design, environmental clearance and permitting stages; construction funding is yet to be secured. Per a signed agreement with FHWA, federal funds must be obligated by December 2020.

## **DISCUSSION**

Regional Transportation Commission (RTC) staff will provide information about the North Coast Rail Trail Draft Environmental Impact Report Schedule to the RTC at the June 14, 2018 meeting. The staff report prepared for the June 14, 2018 RTC meeting is included as Attachment 1 for informational purposes.

**AGENDA:** June 14, 2018

**TO:** Regional Transportation Commission

**FROM:** Cory Caletti and Grace Blakeslee, Senior Transportation Planners

**RE:** North Coast Rail Trail – Project Update and Schedule for Release of Draft Environmental Impact Report (EIR)

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## **RECOMMENDATIONS**

Staff recommends that the Regional Transportation Commission (RTC):

1. Accept an update on North Coast Rail Trail project implementation, and
  2. Accept anticipated schedule for release of Draft Environmental Impact Report (EIR) with a 45-day public review period and public meetings.
- 

## **BACKGROUND**

The 7.5-mile North Coast Rail Trail project (Project) is being implemented through a Federal Lands Access Program (FLAP) grant by the Federal Highway Administration's (FHWA) Central Federal Lands (CFL) Department. The 5.4-mile section from Wilder Ranch to Panther/Yellowbank Beach received full funding through the FLAP grant, the California Coastal Conservancy and the Land Trust of Santa Cruz County. The 2.1-mile section from Panther/Yellowbank Beach to Davenport and parking lots in Davenport and at Panther/Yellowbank Beach are additionally funded through the Land Trust and the RTC for design, environmental clearance and permitting stages; construction funding is yet to be secured. Per a signed agreement with FHWA, federal funds must be obligated by December 2020.

The RTC directed staff to engage an environmental consultant to prepare an Environmental Impact Report (EIR) pursuant to the California Environmental Quality Act (CEQA), to evaluate Project impacts and inform decision-making. The RTC also directed that an economic analysis be conducted in parallel with the EIR. The Final EIR and an economic analysis, to be completed as part of the Unified Corridor Investment Study, are all scheduled to be brought before the Commission by the end of this calendar year. CFL has suspended design work and federal environmental clearance on the project until the RTC selects a preferred project through the EIR process.

## **DISCUSSION**

### **Notice of Preparation**

The North Coast Rail Trail EIR was formally initiated with release of the Notice of Preparation (NOP) on September 13, 2017, and its submission to the State Clearinghouse in compliance with CEQA for distribution to state agencies. Additionally, a copy of the NOP was sent to 183 federal, state, and local agency representatives; 145 members of various organizations; and 1,190 individual members of the public who have expressed interest in RTC projects in the past. The NOP was also posted on the RTC website. The 30-day NOP review period was from September 13, 2017 to October 16, 2017.

Public scoping meetings were held on September 27, 2017 at Pacific Elementary School in Davenport, and September 28, 2017, at the Santa Cruz Police Station Community Room in Santa Cruz. Approximately 85 people attended the scoping meetings, based on sign-in sheets provided at the meetings. The scoping meetings provided another opportunity for attendees to comment on environmental issues of concern and the alternatives that should be discussed in the EIR.

In response to the NOP and public scoping meeting, the RTC received written comments from nine agencies and 132 members of organizations and the public. Comments received included requests for evaluation of a “third” alternative (Farmers’ Alternative) that would remove the rail and locate the trail on top of the tracks north of Scaroni Road, and would locate the trail along Highway 1 (instead of the rail corridor) south of Scaroni Road before returning to the rail corridor just north of Wilder Ranch. This alternative was requested by farmers, their legal representative, as well as other members of the community. Subsequently, RTC staff notified the farmers’ legal representative that this additional proposed alternative would be included among the alternatives considered, and it is being evaluated in the EIR in accordance with CEQA requirements for an alternatives analysis. Additionally, the EIR is evaluating a “trail only” alternative as described in the NOP, an inland side alternative as previously considered by RTC, and the no-project alternative as required by CEQA.

### **EIR Scope and Content**

The scope and content of the EIR is guided by the requirements set forth in the *CEQA Guidelines* and input gathered during the NOP and scoping process identified above. Although the EIR will not be formally “tiering” from the Monterey Bay Scenic Sanctuary Trail (MBSST) Network Master Plan EIR, it will make use of relevant information contained in the Master Plan EIR, including applicable mitigation measures.

This EIR will identify potentially significant environmental impacts, including project-specific and cumulative effects of the project. In addition, the EIR will identify potentially feasible mitigation measures, where possible, that would avoid, minimize, or reduce significant adverse environmental effects.

The North Coast Rail Trail environmental review includes an analysis of the Proposed Project and Project alternatives, as described below. Note that some details may be further refined as we complete the Draft EIR for publication.

Proposed Project The multi-use trail would be located on the coastal side of the existing railroad tracks, except for a short portion on the south end where the trail would utilize the existing sidetracks and Wilder Ranch maintenance road. The Project would also include parking improvements with trail connections at three locations along the alignment, including Davenport Beach, Bonny Doon Beach, and Panther/Yellowbank Beach.

The typical trail cross section would be 20 feet wide, including: 12-foot-wide paved path with striping to separate northbound and southbound, 6-foot-wide unpaved shoulder on the coastal side of the paved path, and 2-foot-wide unpaved shoulder on the inland side of the paved path.

The Proposed Project does not include rail service or improvements to the railroad tracks, but does include retention of the tracks for consistency with the policies set forth in the adopted MBSST Network Master Plan, including Policy 1.2.4, *“Develop trails in such a way so that future rail transit services along the corridor are not precluded”*, as well as other RTC agreements and contractual obligations.

Alternative 1: Trail Only - The railroad tracks and ties would be removed, and the multi-use trail would be located on the rail bed. At the southern end, this alternative may include a spur route from the trail to the Wilder Ranch parking lot and Wilder Ranch. This alternative includes the same parking improvements and access paths to the trail, as the Proposed Project.

The typical trail cross section would be 14-feet wide, including: 10-foot-wide paved path, 4-foot-wide unpaved shoulder on the coastal side of the paved path, and no shoulder on the inland side of the paved path. The typical section is narrower than the Proposed Project to remain predominantly on the existing rail bed, which would reduce some environmental impacts, and entirely within the current right-of-way limits. This alternative is analyzed at a level of detail equal to that of the Proposed Project.

Alternative 2: Inland Side - The trail would be located on the inland side of the tracks in the southern portion of the alignment, between Scaroni Road and Wilder Ranch. The northern portion, from Davenport to Scaroni Road, would be on the coastal side of the tracks along the same alignment as the Proposed Project. The trail would not be located on the inland side in the northern portion because it would require a large amount of earthwork and retaining walls to provide adequate trail width, which would increase environmental impacts. This alternative includes the same parking lot improvements and access paths to the trail, as the Proposed Project.



The typical trail cross section would be 20-feet wide, including: 12-foot wide paved path, 6-foot-wide unpaved shoulder on the coastal side, and 2-foot wide unpaved shoulder on the inland side. This alternative is analyzed with enough detail to provide a meaningful comparison to the Proposed Project, but a lesser level of detail as allowed by CEQA than the analysis of the Proposed Project and Alternative 1.

Alternative 3: Farmers' Alternative - The trail would be located outside the rail corridor and along the coastal side of Highway 1 in the southern portion of the alignment, between Scaroni Road and Wilder Ranch. The northern portion, from Scaroni Road to Davenport, would be within the rail corridor on the rail bed (tracks removed) along the same alignment as Alternative 1 (Trail Only). This alternative includes the same parking lot improvements and access paths to the trail, as the Proposed Project.

The typical cross section in the northern portion would be same as Alternative 1 (Trail Only). The typical cross section in the southern portion would be approximately 16 feet wide, including: 10-foot-wide paved path, 4-foot-wide unpaved shoulder on the coastal side, and 2-foot-wide paved or unpaved shoulder on the inland side. This alternative is analyzed with enough detail to provide a meaningful comparison to the Proposed Project, but a lesser level of detail as allowed by CEQA than the analysis of the Proposed Project and Alternative 1.

Alternative 4: No Project - The North Coast Rail Trail would not be constructed as planned along RTC's Santa Cruz Branch Line rail corridor between Davenport on the north and Wilder Ranch on the south. There would be no new trail and no parking improvements. The rail corridor would remain "as is", and the RTC would operate and maintain the rail corridor in accordance with current practice, policy and legal obligations. The No Project scenario does not include improvements to the tracks for use or removal of the tracks. Potential future use of the rail for freight or passenger service is not yet known and cannot be precluded, but is not part of the No Project alternative. The RTC is conducting the Unified Corridor Investment Study to determine transportation improvements along the community's north/south transportation corridors, including the rail corridor.

## **Schedule**

RTC staff anticipates releasing the Draft EIR in late July for a 45-day public review period that will last into early September. Two public meetings are tentatively scheduled for August 14 and 15 in Davenport and the City of Santa Cruz, respectively. RTC staff will send notices to those on the NOP distribution list and those who commented on the NOP, post information on the agency website and in local newspapers, and file notices with the County Clerk and State Clearinghouse as required by CEQA.

Following circulation of the Draft EIR, the environmental team will prepare a Final EIR. The Final EIR will include all comments received on the Draft EIR, responses to those comments, and necessary revisions to the Draft EIR. The Final EIR is

anticipated to be completed by the end of this calendar year. The length of time the RTC staff and its consultant team will require to prepare the Final EIR will in large part be a function of the volume and character of the comments received. It is possible that Final EIR preparation could take longer than currently anticipated.

The RTC will disclose the rationale for agency decision-making through the adoption of findings, addressing the disposition of all significant environmental effects identified in the EIR, and Statements of Overriding Consideration for those impacts determined to be significant and unavoidable.

## **Next Steps**

In addition to preparing the EIR and completing the CEQA process, the following additional tasks and actions are required to move forward with the North Coast Rail Trail project: 1) right-of-way certification; 2) completion of project design; 3) federal environmental compliance; and 4) award of construction contract after completion of the bidding process. Funds must be obligated by 2020 in order for the project to continue to construction.

CFL will complete project design and federal environmental clearance once the RTC certifies the EIR and selects a preferred project. The RTC will continue right-of-way certification work and continue to seek grants to complete the funding package needed in order to construct the 2.1-mile section and parking lots, along with the 5.1 miles that are funded in full.

## **Summary of Anticipated Schedule**

Late July	Release of DEIR
Late July to early September	45-day DEIR Public Review Period
August 14 <sup>th</sup> and 15 <sup>th</sup> , 2018	Public Meetings in Davenport and City of Santa Cruz
December, 2018	Completion and Certification of Final EIR (timing dependent on volume and character of comments received); Selection of Preferred Project
2018/2019	Right-of-way; Final Design; Securing Additional Funding; Federal Environmental Compliance; Bidding Process
2020	Award of Construction Contract; Construction

## **SUMMARY**

The RTC is conducting an Environmental Impact Report (EIR) for a 7.5-mile rail trail project on the north coast. RTC staff anticipates releasing the Draft EIR for public review in late July with a 45-day public review period that will last into early September. Two public meetings are tentatively scheduled for August 14 and 15, 2018 in Davenport and City of Santa Cruz. RTC staff will send out notifications, post information on the agency website and in local newspapers.

**TO:** Bicycle Committee & Elderly and Disabled Transportation Advisory Committee (E&D TAC)

**FROM:** Rachel Moriconi, Senior Transportation Planner

**RE:** Measure D: Five-Year Programs of Projects for Regional Projects Update

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## **RECOMMENDATIONS**

This item is for information only.

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## **BACKGROUND**

Measure D, the transportation ballot measure passed by more than two-thirds of Santa Cruz County voters on November 8, 2016, provides funding for five categories of projects: neighborhood projects (30% of net measure revenues), highway corridors (25%), transportation for seniors and people with disabilities (20%), active transportation (17%), and the rail corridor (8%).

Each agency receiving Measure D revenues is required to annually develop, update, hold a public hearing on, and adopt a five-year program of projects showing how they plan to use Measure D funds in the upcoming 5 years. Agencies receiving direct formula allocations (cities, the County of Santa Cruz, Santa Cruz Metropolitan Transit District (METRO) and Community Bridges/Lift Line) typically develop their five-year programs of projects as part of their annual budget and/or capital improvement program. Community members provide input on those plans directly to each recipient agency.

The Regional Transportation Commission (RTC) is responsible for developing the five-year program of projects for regional projects, as described below. The first five-year plans for Measure D regional projects were approved by the RTC in June of 2017.

## **DISCUSSION**

The Elderly and Disabled Transportation Advisory Committee (E&D TAC) and Bicycle Committee provided input in April 2018 for the 5-year plans for Measure D regional transportation categories and projects. Staff considered committee input when developing the draft FY18/19-22/23 five-year plans for each of the Measure D regional transportation categories. The 5-year plans ([Attachment 1: A-E](#) and summarized [Attachment 2](#)) reflect updated revenue projections, proposed

allocations to specific projects and work, updates on the years funds are anticipated to be expended based on current project schedules and costs, and the carryover of unspent revenues from prior years. For some projects and categories, staff proposes to reserve some funds to be allocated in future years, to serve as match for competitive grant programs or to be used in the event those grants are not secured. The 5-year plans also propose to advance future Measure D funds in order to accelerate delivery of some projects, as described below. The “5-year programs of projects” are preliminary plans for years 2-5, and are adjusted annually based on updated project schedule, cost, grant, and revenue information.

The RTC will consider approval of the 5-year plans, following a public hearing at its June 14, 2018 meeting at the Watsonville City Council Chambers.

## SUMMARY

Measure D requires recipient agencies to annually prepare and update a five-year program of projects identifying how agencies plan to invest Measure D revenues, consistent with the voter-approved Expenditure Plan. Staff considered committee input when developing the draft 5-year plans for the regional transportation categories -- Highway Corridors, Active Transportation, and the Rail Corridor, as well as San Lorenzo Valley Highway 9 Corridor Improvements and the Highway 17 Wildlife Crossing, and Community Bridges Lift Line (Attachment 1 Exhibits A-E). The RTC is scheduled to adopt the 5-year plans following a public hearing on June 14, 2018.

### Attachments:

1. Measure D 5-year programs of projects for Regional Projects/Categories
  - a. Highway Corridors 5-Year Program of Projects
  - b. Monterey Bay Sanctuary Scenic Trail Network (MBSST)/Coastal Rail Trail 5-Year Program of Projects
  - c. Rail Corridor 5-Year Program of Projects
  - d. San Lorenzo Valley Highway 9 Corridor 5-Year Program of Projects
  - e. Highway 17 Wildlife Crossing 5-Year Program of Projects
2. Summary of Measure D Regional Categories

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**DRAFT - June 2018**

**Measure D: 5-Year Program of Projects (FY18/19-FY22/23)**

Attachment 1:  
Exhibit A

**Category: Highway Corridors (25% of Measure D Revenues)**

**Measure D Revenues**

	Prior	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23
Estimated Annual Measure D Allocations	\$5,022,242	\$4,854,176	\$4,917,281	\$4,981,205	\$5,045,961	\$5,111,558

			Planned Use							
	Name/Road/Limits	Description	Prior Years	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23	Total Measure D	2018 Updates
1	Highway 1: 41 st Avenue/Soquel Drive Auxiliary Lanes and Chanticleer Bike/Pedestrian Overcrossing <sup>(1)</sup>	Freeway operational improvement, rehab roadway and drainage, improve bicycle/pedestrian access over freeway.	\$0	\$1,500,000	\$500,000	\$12,000,000	Balance of construction cost if grants not awarded		\$14-28M	Shift some funds from FY20/21 to earlier in order to advance design and right-of-way. Total will depend on grants received.
2	Highway 1: Auxiliary Lanes from State Park Drive to Bay-Porter, and Reconstruction of Capitola Avenue Overcrossing <sup>(2)</sup>	Freeway operational improvement, rehab roadway and drainage, soundwalls and retaining walls, reconstruct Capitola Ave. overcrossing with sidewalks and bike lane	\$0	\$2,600,000	\$500,000	\$500,000	\$3,200,000	\$4,600,000	\$11,400,000	Combine Park Av/Bay-Porter and State Park/Park Av. Aux Lanes projects. Shift some funds from FY20/21 to earlier in order to advance environmental review and design.
3	Highway 1 Bicycle/Pedestrian Overcrossing at Mar Vista Drive <sup>(3)</sup>	Address restricted bike/ped access by building new overcrossing with lighting, traffic calming, and moderate aesthetic treatments	\$0			---	\$2,000,000		\$2,000,000	Funds shifted to FY21/22 based on current schedule.
4	Cruz 511	Ongoing system & demand management (TDM), includes Cruz511.org traveler information, carpool and other TDM programs	\$50,000	\$150,000	\$150,000	\$200,000	\$200,000	\$200,000	\$950,000	Funds for FY22/23 added
5	Safe on 17	Ongoing system management program, involves increased CHP enforcement on Highway 17	\$25,000	\$25,000	\$25,000	\$35,000	\$35,000	\$35,000	\$180,000	Funds for FY22/23 added
6	Freeway Service Patrol <sup>(4)</sup>	Ongoing system management and congestion reducing program. Roving tow trucks removing incidents and obstructions during peak travel periods on Hwy 1 and Hwy 17	\$100,000	\$250,000	\$250,000	\$250,000	\$250,000	\$250,000	\$1,350,000	Funds for FY22/23 added
Estimated Annual Measure D Expenditures			\$175,000	\$4,525,000	\$1,425,000	\$12,985,000	\$5,685,000	\$5,085,000	\$29,880,000	
Carry over to next fiscal year			\$4,847,242	\$5,176,418	\$8,668,699	\$664,904	\$25,865	\$52,423		

**Notes:**

- Hwy 1 41st/Soquel Aux Lane and Chanticleer B/P Overcrossing:** Previously programmed funds used for preliminary and final engineering design and right-of-way phases over the period FY 17/18-through-FY 19/20. Measure D funds for capital construction and construction management activities. RTC will seek grants or could decide at future date whether to utilize bond or other financing that may be required in years 4 and 5 to accelerate delivery.
- Hwy 1 Auxiliary Lanes from State Park Drive to Bay/Porter Project:** Measure D funding for preliminary design and environmental documentation (FY 18/19 - FY 20/21) and final engineering and right-of-way acquisition (FY 21/22).
- Mar Vista Bicycle/Pedestrian Overcrossing:** Previously programmed funds used for project development and construction activities. Measure D funds (\$2M) to supplement construction and construction support activities as needed.
- Freeway Service Patrol:** through the State budget more Caltrans funds may become available reducing the need for Measure D fund:

Category: Active Transportation/MBSST-Rail Trail (17% of Measure D Revenues)

## Measure D Revenues

## Estimated Annual Measure D Allocations

Prior	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23	Total Measure D
\$3,415,114	\$3,300,840	\$3,343,751	\$3,387,220	\$3,431,253	\$3,475,860	\$20,354,038

	Rail Trail Project/Program	Description	Original Estimate FY17/18	Updated Estimate FY17/18	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23	Total Measure D	2018 Updates
1	Segment 7: Natural Bridges to Wharf, City of Santa Cruz (SC)	Construction of trail. Includes \$1.1M to city of SC for construction and funds for RTC oversight and technical assistance	\$1,130,000	\$100,000	\$1,205,000	\$40,000	\$0	\$0	\$0	\$1,345,000	Shift capital funds from FY17/18 to FY18/19, based on updated schedule. Update RTC and technical assistance costs
2	Seg 8: San Lorenzo River parkway trestle widening, City of Santa Cruz	Widening of existing walkway on the existing railroad bridge over San Lorenzo River near Boardwalk to serve multi-use purposes. \$500k to city of SC for construction. Balance for RTC project management and technical assistance/services.	\$500,000	\$5,000	\$505,000	\$5,000	\$0	\$0	\$0	\$515,000	Shift funds from FY17/18 to FY18/19, based on updated schedule. Incorporate RTC oversight and technical assistance costs.
3	Seg 8/9: San Lorenzo River parkway to 17th Ave., City of SC lead	\$2M set aside to serve as match for construction grants. Joint project with County. Balance for RTC project management, oversight, outreach, and technical assistance.	\$0	\$0	\$0	\$40,000	\$2,040,000	\$40,000	\$0	\$2,120,000	Incorporate RTC oversight and technical assistance costs.
4	City of SC trail maintenance and operations	Ongoing maintenance of sections of trail once constructed. Includes restriping, sweeping, vegetation management and periodic repaving.	\$0	\$0	\$0	\$25,000	\$50,000	\$50,000	\$50,000	\$175,000	City of Santa Cruz requests to increase from \$20K to \$50k/yr based on required enviro monitoring, plus adds FY22/23 funds
5	Segment 18: Lee Rd-Walker, Watsonville	\$150k to City of Watsonville for trail construction. Balance for RTC project management and technical assistance/services	\$170,000	\$20,000	\$225,000	\$20,000	\$0	\$0	\$0	\$265,000	Shift funds from FY17/18 to FY18/19 based on updated schedule. Incorporate RTC oversight and technical assistance costs.
6	City of Watsonville trail maintenance and operations	Ongoing maintenance of sections of trail once constructed. Includes restriping, sweeping, vegetation management and periodic repaving.	\$0	\$0	\$0	Postponed	\$10,000	\$11,000	\$11,000	\$32,000	Add funds for FY22/23 maintenance
7	North Coast Segment 5	Environmental review and design of north coast section of trail, tehcnical assistance, and project management	\$500,000	\$380,000	\$410,000	\$160,000	\$160,000	\$0	\$0	\$1,110,000	Shift funds to match current schedule and incorporate RTC oversight and technical assistance costs.

	Rail Trail Project/Program	Description	Original Estimate FY17/18	Updated Estimate FY17/18	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23	Total Measure D	2018 Updates
8	North Coast trail maintenance and operations	Ongoing maintenance of sections of trail once constructed. Includes restriping, sweeping, vegetation management and periodic repaving.	\$0	\$0	\$0	---	\$50,000	\$100,000	\$125,000	\$275,000	Add funds for FY22/23 maintenance. Shift maintenance funds FY20/21 and later years.
9	North Coast: Reserve to match future grants for unfunded 2 mi.	Funds set aside to service as match when seeking future grant application.	\$173,415	---	---	---	\$3,690,245	-	-	\$3,690,245	Shift funds to FY20/21, based on updated schedule.
10	<u>Segment 10: Preliminary Analysis of 17th Ave-47th Ave/Jade St. Park</u>	\$200k for preliminary design, right of way research/surveys, assessments, and grant writing services. \$20k/yr for RTC oversight and technical assistance. Likely a joint County DPW-Capitolola project.	\$0	\$0	\$0	\$0	\$220,000	\$20,000	\$20,000	\$260,000	NEW: Add funds to begin work on this section in high density area. Includes RTC oversight and technical assistance.
11	<u>Capitolola Trail: City Hall to Monterey Ave</u>	RTC oversight and technical assistance.	\$0	\$0	\$15,000	\$15,000	\$0	\$0	\$0	\$30,000	NEW: Add funds for oversight and assistance on this section.
12	Ongoing RTC oversight, coordination, and assistance, including on development of future trail sections.	Surveys, Environmental Health (EHS), attorneys, coordination, response to public comments, and development of future grant applications	\$200,000	\$100,000	\$165,000	\$235,000	\$260,000	\$260,000	\$260,000	\$1,280,000	Some costs shifted into specific sections of trail. Add funds for ongoing work in FY22/23
13	Corridor encroachments & maintenance	Ongoing corridor maintenance, including vegetation, tree removal, trash, graffiti, drainage, encroachments, storm damage repairs outside of what is required for railroad operations	\$519,250	\$519,250	\$519,250	\$519,250	\$519,250	\$519,250	\$519,250	\$3,115,500	Add funds for ongoing work in FY22/23
<b>Estimated Annual Measure D Expenditures</b>			<b>\$3,192,665</b>	<b>\$1,124,250</b>	<b>\$3,044,250</b>	<b>\$1,059,250</b>	<b>\$6,999,495</b>	<b>\$1,000,250</b>	<b>\$985,250</b>	<b>\$14,212,745</b>	
<b>RESERVE: Unappropriated balances to be programmed in future 5-year plans (after 2018)</b>										<b>\$6,141,293</b>	Unappropriated amount increased by \$4M.

2018 Update: For all projects capital, project management, oversight, and technical assistance costs were combined

**DRAFT- June 2018****Measure D: 5-Year Program of Projects (FY18/19-FY22/23)****Category: Rail Corridor (8% of Measure D Revenues)****Measure D Revenues**

	Prior	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23
Estimated Annual Measure D Allocations	\$1,607,117	\$1,553,336	\$1,573,530	\$1,593,986	\$1,614,707	\$1,635,699

Planned Use										
	Name/Road/Limits	Description	Prior Years	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23	Total Measure D	2018 Updates
1	Unified Corridor Investment Study	Analysis of rail corridor usage options	\$300,000	\$325,000					\$625,000	Carryover FY17/18 unspent funds to FY18/19.
2	2017 Storm Damage Repair & Cleanup	Repair and cleanup of damage resulting from the 2017 winter storms including one washout, minor slides and various downed or compromised trees	\$150,000	\$250,000					\$400,000	Carryover FY17/18 unspent funds to FY18/19.
3	Railroad Bridge analysis	Updated engineering analysis of railroad bridges to complete bridge rehabilitation work outlined in engineering analysis completed in 2012	\$150,000	\$300,000					\$450,000	Add \$100k, carryover unspent FY17/18 funds to FY18/19
4	Railroad Bridge Rehabilitation	Rehabilitation of railroad bridges consistent with analysis completed in 2012 and updated analysis proposed for 2018		\$1,200,000	\$800,000	\$250,000			\$2,250,000	Carryover unspent FY17/18 funds and respread to reflect current schedule
5	Track infrastructure and signage maintenance and repairs	On-going maintenance, repair and rehabilitation of railroad track infrastructure and signage	\$60,500	\$262,587	\$164,700	\$166,841	\$169,010	\$171,207	\$994,845	Carryover unspent FY17/18 funds and add FY22/23 estimates
6	Rail Transit or Other Projects Environmental Document	Preparation of environmental documents for projects on rail corridor resulting from the Unified Corridor Study		\$0	\$750,000	\$1,150,000	\$1,450,000	\$650,000	\$4,000,000	Update to reflect current schedule and cost estimate.
Estimated Annual Measure D Expenditures			\$660,500	\$2,337,587	\$1,714,700	\$1,566,841	\$1,619,010	\$821,207	\$8,719,845	
Carry over to next fiscal year			\$946,617	\$162,367	\$21,196	\$48,341	\$44,038	\$858,530		



**DRAFT - June 2018****Measure D: 5-Year Program of Projects (FY18/19-FY22/23)****Category: Neighborhood Projects: San Lorenzo Valley (SLV)/Highway 9 Corridor (\$10 million over 30 years)****Measure D Revenues**

Prior	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23
\$347,222	\$333,333	\$333,333	\$333,333	\$333,333	\$333,333

**Planned Use**

	Name/Road/Limits	Description	Prior Years	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23	Total Measure D	2018 Updates
1	San Lorenzo Valley (SLV) Safe Routes to Schools sidewalk (SR 9: SLVHS entrance south to at least Fall Creek Rd)- Preconstruction & ATP grant match	Bike/Ped Pathway & transit stop	\$0	<u>\$250,000</u>		<u>\$500,000</u>			<b>\$750,000</b>	<i>Shift FY17/18 funds to FY18/19. Add funds to match grant requests</i>
2	Hwy 9/SLV Corridor Plan - Completion of plan	Identify priorities for limited funds	\$0	<u>\$50,000</u>					<b>\$50,000</b>	<i>Additional work needed to reflect public input</i>
3	<b>Estimated Annual Measure D Expenditures</b>		<b>\$0</b>	<b>\$300,000</b>	<b>\$0</b>	<b>\$500,000</b>	<b>\$0</b>	<b>\$0</b>		
4	<b>Reserve for priorities in Highway 9/SLV Corridor Plan</b>	<i>Exact projects TBD. Use as match to leverage grants</i>	\$347,222	\$380,556	\$713,889	\$547,222	\$880,556	\$1,213,889		

**DRAFT - June 2018**

**Measure D: 5-Year Program of Projects (FY18/19-FY22/23)**

Exhibit E

**Category: Highway 17 Wildlife Corridor (\$5 million over 30 years)**

**Measure D Revenues**

	Prior	FY18/19	FY19/20	FY20/21	FY21/22	FY22/23	Future
Estimated Annual Measure D Allocations	\$166,667	\$166,667	\$166,667	\$166,667	\$166,667	\$166,667	\$4,000,000

			Planned Use								
	Name/Road/Limits	Description	Prior Years	FY18/19	FY19/20	FY20/21	FY21/22*	FY22/23*	Future Debt Service*	Total Measure D	2018 Updates
1	Highway 17 Wildlife Crossing near Laurel Curve	Construct wildlife undercrossing to connect habitat on either side of the highway. Creates a wildlife corridor enabling animals to safely cross the highway.	\$0	\$15,000	\$15,000	\$15,000	\$2,015,000	\$2,015,000	\$925,000	\$5,000,000	Include placeholder for financing to advance project delivery. Includes \$15,000 per year for RTC staff time associated with oversight, agreements, financing, coordination, and public engagement.
Estimated Annual Measure D Expenditures			\$0	\$15,000	\$15,000	\$15,000	\$2,015,000	\$2,015,000			
Carryover Balance - RESERVE for construction			\$166,667	\$318,333	\$470,000	\$621,667	(\$1,226,667)	(\$3,075,000)			

\*At a future date, RTC will evaluate financing options to advance delivery, since full \$5M committed in Measure D will not be available until end of 30 year measure.

\*\*Total cost includes pre-construction funded through Caltrans SHOPP - \$3.1M. Land Trust has committed private funds for some of construction costs. Actual cost will depend on final design, bids and any claims.

**Summary Measure D Regional Categories 5-Year Plans**

*Measure D is a 30 year transportation sales tax approved by voters in 2016. The 5-Year Programs of Projects provide an opportunity for the community to provide input on how recipient agencies plan to utilize Measure D revenues in the near term, consistent with the voter-approved Measure D Expenditure Plan.*

**Highway Corridors**

25% of net Measure D revenues are designated for the Highway Corridors category, which includes Highway 1 auxiliary lanes, bicycle and pedestrian overcrossings, traveler information, transportation demand management (TDM), highway safety and congestion reduction programs. **The five year program of projects (Attachment 1, Exhibit A) proposes to use some Measure D funds in the first few years on pre-construction phases of capital projects – including auxiliary lanes from 41<sup>st</sup> Avenue to Soquel Avenue and State Park Drive to Bay/Porter interchanges and replacement of the Capitola Avenue overcrossing - and ongoing traveler information and safety programs; as well as construction of auxiliary lanes and bicycle/pedestrian crossings.** The RTC is working to accelerate environmental review and design of Highway 1 auxiliary lane projects and increase opportunities to secure other state and federal grant funds to supplement Measure D revenues allocated to these projects, including new grant programs which are funded by Senate Bill 1 gas tax revenues. If grants are not available, staff will return to the RTC to consider financing options to construct projects as quickly as possible.

In summary, deliverables for the Highway Corridor category for the next five years include:

- Highway 1 - 41<sup>st</sup> Ave/Soquel Auxiliary Lanes: Completion of environmental documents by Caltrans, final design, right of way and utility work, then begin construction in 2021.
- Highway 1 – Pedestrian/Bicycle Bridge at Chanticleer: Completion of environmental documents by Caltrans, final design, right of way and utility work, and begin construction in 2021.
- Highway 1 – Pedestrian/Bicycle Bridge at Mar Vista: Begin construction following completion of environmental documents, final design, right of way and utility work.
- Highway 1 – Park Ave/Bay-Porter Auxiliary Lanes: Environmental review, final design, right of way and utility work.
- Highway 1 – State Park Drive/Park Ave Auxiliary Lanes: Environmental review, final design, right of way and utility work.
- Cruz511, Safe on 17, Freeway Service Patrol: Provide ongoing services

**Coastal Rail Trail Program**

17% of net Measure D revenues are designated for the “Active Transportation” category to fund the Monterey Bay Sanctuary Scenic Trail Network (MBSST), also known as the Coastal Rail Trail. As noted in the Measure D Expenditure Plan, this

funding will be used for bicycle and pedestrian trail construction; maintenance, management and drainage for the rail and trail corridor; and possible installation of conduit for internet and electrical services.

The draft FY18/19-22/23 plan for revenues allocated to the Active Transportation category (Attachment 1 Exhibit B) focuses Measure D funds to trail projects currently under development, ongoing trail development and maintenance of the corridor. **New this year, staff recommends adding \$200,000 for preliminary work on Segment 10 from 17<sup>th</sup> Avenue in Live Oak to 47<sup>th</sup> Avenue/Jade Street Park in Capitola.** At its April 9, 2018 meeting, **the Bicycle Advisory supported the recommendation to allocate funding towards Segment 10**, as requested by the County of Santa Cruz and City of Capitola. Funds would go towards preliminary design and right of way research. The draft plan also includes **additional funds for maintenance requested by the City of Santa Cruz. The balance of funds will be held in reserve.** Reserve funds are expected to serve as match to leverage Senate Bill 1 - Active Transportation Program, Congested Corridors, and Local Partnership Program grants. It is anticipated that some of the reserve funds may be used on the North Coast trail.

Measure D funds are also used for work tasks related to trail project implementation and program management, including design, preliminary engineering, environmental permitting, as well as public engagement on projects currently under way and for development of the network as a whole. Specific tasks include coordination with local public works departments, stakeholders, State Parks, rail operators, California Public Utilities Commission, Environmental Health Services, consultants and technical experts; right-of-way surveying, encroachments and property rights certifications; grant applications for future projects and complete funding for current projects; studies related to trail implementation or corridor uses; management of all funding sources and contracts; partnership building; presentations to community organizations; event planning and participation. Program management and technical work anticipated for specific segments has been incorporated into each of those lines.

The Active Transportation Measure D category also provides funds for corridor right-of-way encroachments and maintenance, including items such as vegetation abatement, trash and graffiti removal, drainage, storm damage repairs, etc.

In summary, deliverables for the Active Transportation category for the next five years include:

- Segment 5 North Coast rail trail: With the Federal Highway Administration's Central Federal Lands as the lead agency, funding will provide for construction and maintenance/operation of trail following completion of environmental analysis (per California law), completion of design, management and technical assistance.
- Segment 7 City of Santa Cruz rail trail (Natural Bridges to Wharf): With the City of Santa Cruz as the lead agency, complete construction of trail and conduct maintenance/operations activities with supplemental funding. Phase 1 from Natural Bridges to Bay St/California Ave is expected to be under

construction later this summer through Spring 2019. Phase 2 (from California Ave to Pacific Ave/Wharf) is expected to begin construction in early 2019.

- San Lorenzo River Parkway Phase II (Segment 8 bicycle/pedestrian trestle bridge widening): With the City of Santa Cruz as the lead agency, funds will be used to widen the existing walkway for multi-use purposes on the existing railroad bridge. Pending review by the Coastal Commission, the City's objective is to start and complete construction prior to the 2019 summer tourist season.
- Segment 8/9 City and County of Santa Cruz rail trail (Pacific Ave/Wharf to 17<sup>th</sup> Ave): Environmental review and design is funded by ATP and Land Trust grants. Measure D funds will be utilized to leverage future grants for construction work.
- Segment 10 in Live Oak (17<sup>th</sup> to 47<sup>th</sup> Avenues/Jade Street Park): Preliminary design, right of way research/surveys, assessments, and grant writing services.
- Segment 18 City of Watsonville rail trail (Lee Rd-Walker): With the City of Watsonville as the lead agency, funding will be used for construction of the trail and conduct maintenance/operations activities. Construction is scheduled to begin in 2019.
- Maintenance of rail corridor: Ongoing activity to address vegetation, trash, graffiti, drainage, encroachments, etc.
- Ongoing oversight, technical work, coordination with stakeholders and other applicable entities, responding to public comments and questions, and advancing future trail segments, including pursuing additional grants.

#### Rail Corridor

8% of net Measure D revenues are designated for the Rail Corridor category for infrastructure preservation and analysis (including environmental and economic analysis) to answer community questions about possible future transit and other transportation uses of the rail corridor. **Slight modifications from the plan approved by the RTC in April as part of the FY18/19 budget adoption are included in the new the 5-year plan.**

As stated in the Measure D Expenditure Plan, projects under this category include "maintaining and repairing the publicly-owned Santa Cruz Branch Rail Line." Therefore, the 5-year program of projects includes track maintenance and repairs, storm damage repair and clean up, and bridge analysis and rehabilitation to continue work began in 2012. Measure D does not include funding for any new train or rail service.

In summary, deliverables for the Rail Corridor category for the next five years include:

- Unified Corridor Investment Study: Complete three-route analysis including transportation options on the rail corridor
- Rail Bridges: Updated analysis of bridge conditions and rehabilitation needs to prioritize work; and rehabilitation of bridges based on results of analysis
- Track infrastructure: Conduct ongoing repair, maintenance and improvement, including storm damage repairs

- Corridor Use: Complete required environmental documents based on results of the Unified Corridor Study

#### San Lorenzo Valley (SLV) Highway 9 Corridor Improvements

Measure D includes a total of \$10 million for transportation projects along the Highway 9 corridor through San Lorenzo Valley (approximately \$333,000 per year over 30 years). **The 5-year plan includes \$750,000 to serve as a match to a grant application to improve pedestrian access to the SLV school complex,** which has consistently been identified as one of the highest priority projects in SLV. The RTC is working with a consultant team, Caltrans, County Public Works and Planning, Santa Cruz Metropolitan Transit District (METRO), and the public to develop a comprehensive plan for the Highway 9 corridor. The corridor plan identifies, evaluates and prioritizes multimodal investments along the corridor. **The updated 5-year plan allocates \$50,000 in Measure D funds to complete the corridor plan.** Additional funds are needed to address comments received on the draft implementation plan. The balance of Measure D revenues are reserved for projects identified in Highway 9/SLV corridor plan. Through development of the plan, hundreds of project ideas, challenge areas and concepts have been identified. The project team is currently working with the community to prioritize approximately 30 projects for implementation in the short, mid and long term. Measure D funds are insufficient to address all of the priority transportation projects and will need to be focused on the very highest priority projects in San Lorenzo Valley and to leverage other funds.

In summary, deliverables for the SLV/Highway 9 project for the next five years include:

- SLV School Complex Access: Funds to serve as match to leverage Active Transportation Program grant funds to construct pathways and other improvements.
- Through the SLV/Highway 9 corridor planning effort (scheduled for completion in 2018), determine near-term and longer term priority investments of Measure D and other funds.

#### Highway 17 Wildlife Crossing

Measure D includes a total of \$5 million for the Highway 17 Wildlife Crossing (approximately \$167,000 per year over 30 years). Caltrans and the Land Trust of Santa Cruz County secured over \$3 million in State Highway Operation and Protection Program (SHOPP) funds from the California Transportation Commission (CTC) to initiate environmental review and design of a wildlife crossing on Highway 17 near Laurel Curve. The Land Trust has secured right-of-way adjacent to the highway and is raising private funds to complete the project. While Caltrans currently anticipates that pre-construction work will be done and the project ready for construction in FY20/21, Measure D revenues apportioned to the project will not have had time to accumulate to a level sufficient to fully fund the project by FY20/21. RTC staff is investigating financing and other options to accelerate delivery of this project, which may include loans from other entities or other Measure D categories, or bond financing. **The update 5-year plan assumes some of the Measure D funds may be needed for debt financing and**

**ongoing RTC staff time associated with project implementation, coordination and outreach.**

Community Bridges Measure D Projects

Twenty percent (20%) of net Measure D revenues are designated for the Transit for Seniors and People with Disabilities category. Four percent (4%) of net Measure D revenues are allocated to the Consolidated Transportation Services Agency (CTSA) for Santa Cruz County for paratransit service. Community Bridges-Lift Line serves as the CTSA for Santa Cruz County. The remaining 16% of net Measure D revenues under the Transit for Seniors and People with Disabilities investment category are allocated to the Santa Cruz Metropolitan Transit District. As noted in the Measure D Expenditure Plan, paratransit services work with social service agencies to increase transportation options for seniors, individuals living with disabilities, and persons with low incomes.

The Measure D five-year funding projection (FY18-FY23) for direct allocation to Community Bridges-Lift Line is estimated to be approximately \$3.98 million for the five-year period with an average annual distribution of \$797,000. The draft five-year plan (Attachment 1: Exhibit F) distributes the direct allocation as follows:

- 31% for two driver positions to provide door-to-door service for elderly and/or physically or mentally limited and/or ill passengers. Drivers operate the vehicles, keep simple records, and maintain the van in a clean, safe, and orderly condition and perform other related work as required;
- 9% for a new van driver trainer who will supervise van drivers and provide all phases of training for drivers, including preparing van driver trainees for promotion to van driver and ongoing retraining;
- 8% for an administrative assistant and dispatcher to provide administrative support including clerical support, receiving and screening telephone calls, maintaining records, preparing statistics reports, correspondence and coordination of tasks and scheduling when needed to provide support to the van drivers;
- 1% for outreach and publicity to provide materials and videos to promote paratransit ride availability;
- 47% for operations and facility reserve to purchase property and construct an operations facility that will house the entire Lift Line operations in one location or for increases in facility leases, including relocation if needed; and,
- 4% for vehicle and equipment reserve to purchase two new fleet vehicles, including on one 24-passenger replacement bus and one electric vehicle, and replace aging and worn shop improvement or upgrades to the maintenance and operations facility.

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**TO:** Bicycle Advisory Committee

**FROM:** Anais Schenk, Transportation Planner

**RE:** Green Hills / Glen Canyon Paving Project

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## **RECOMMENDATIONS**

Staff recommends the Committee review information regarding the Green Hills / Glen Canyon Paving project and provide any recommendations to City of Scotts Valley staff no later than June 18<sup>th</sup>.

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## **BACKGROUND**

The Green Hills / Glen Canyon Paving project was brought to the Bicycle Advisory Committee (Committee) for review as part of the 2016 Surface Transportation Block Grant Program (STBG) Recommendations. At that point in time the Committee recommended the project for funding. It is not clear if plans were reviewed by the Committee after initial approval of the project. The City of Scotts Valley is now moving forward with construction of the project which is scheduled to begin on June 18<sup>th</sup>, 2018. Committee member Rick Hyman requested the Committee review the construction plans ([Attachment 1](#)).

The City of Scotts Valley will be developing a new Active Transportation Plan and is working on a Complete Streets to School project which will allow for future opportunities for the Committee to provide additional recommendations on improving bicycle facilities, connections and access.

## **DISCUSSION**

The plans show a number of pavement and striping improvements to Green Hills Road between South Navarro Drive and Glen Canyon Road as well as on Glen Canyon Road between Green Hills Road and Graham Hill Road. Specific features of the striping improvements include a new Class II facility (bicycle lane) that transitions into a Class III facility (bicycle route), green bike lane markings at the intersection of Green Hills Road / Glen Canyon Road and sharrows on Glen Canyon Road. The plans also note four potential add-ons to the project as follows:

1. Seal cracks and construct ¼" slurry seal at various locations within the project area.
2. Full depth reclamation of Green Hills Road.
3. Remove gate barrier at South Navarro Drive and Green Hills Road and replace with bollards at 8 ft intervals. Resurface concrete on path that connects South Navarro and Green Hills Road.
4. Add sharrows and parking striping on Green Hills Road between South Navarro Drive and 1300/1350 Green Hills Road.

Scotts Valley staff has indicated that Alternative 2 will not be included in the project due to funding restrictions but that the other Alternatives will be included. Staff also noted that the fire department has raised concerns with the plan to remove the gate in place of bollards (Alternative 3) for access reasons.

Committee members and staff note a few points for discussion:

- Improving the Green Hills and South Navarro connection is positive. However, it appears that the fire department may have issues with changing the barrier type.
- Glen Canyon continues to have a gap in bike lanes due to the right of way constraints from the Highway 17 overpass. Consider possible future recommendations as part of the upcoming Active Transportation Plan project in Scotts Valley.
- The plans also call for installing four signs at the approaches to the Glen Canyon and Mount Hermon Road intersection "Bike Route: Use Crosswalk to Cross Road." Concerns about this signage include the message this sign may send motorists and the non-standard verbiage used.
- The new green bike lanes at the intersection of Green Hills Road and Glen Canyon is a positive improvement, however other options exist for this right turn. Consider the bicycle only right turn at Soquel and Capitola Road as an example. Additionally, it appears the striping plans are non standard. The green bike lane marking would typically have gaps leading up to and at vehicle crossing points.
- There is a lack of signage on Green Hills Road including "Bike Lane Ends" or "Bikes May Use Full Lane."

## **SUMMARY**

The improvements on Green Hills Road are a positive move for improving bicycle access and connectivity in Scotts Valley. There are some points of concern raised by Committee members. Recommendations by the Committee should be communicated to City of Scotts Valley staff as soon as possible since the project is scheduled to begin June 18<sup>th</sup>.

### Attachments:

1. Construction plans for the Green Hills / Glen Canyon Paving Project

# REVISED IMPROVEMENT PLANS

## GLEN CANYON AND GREEN HILLS ROAD BIKE LANES PROJECT

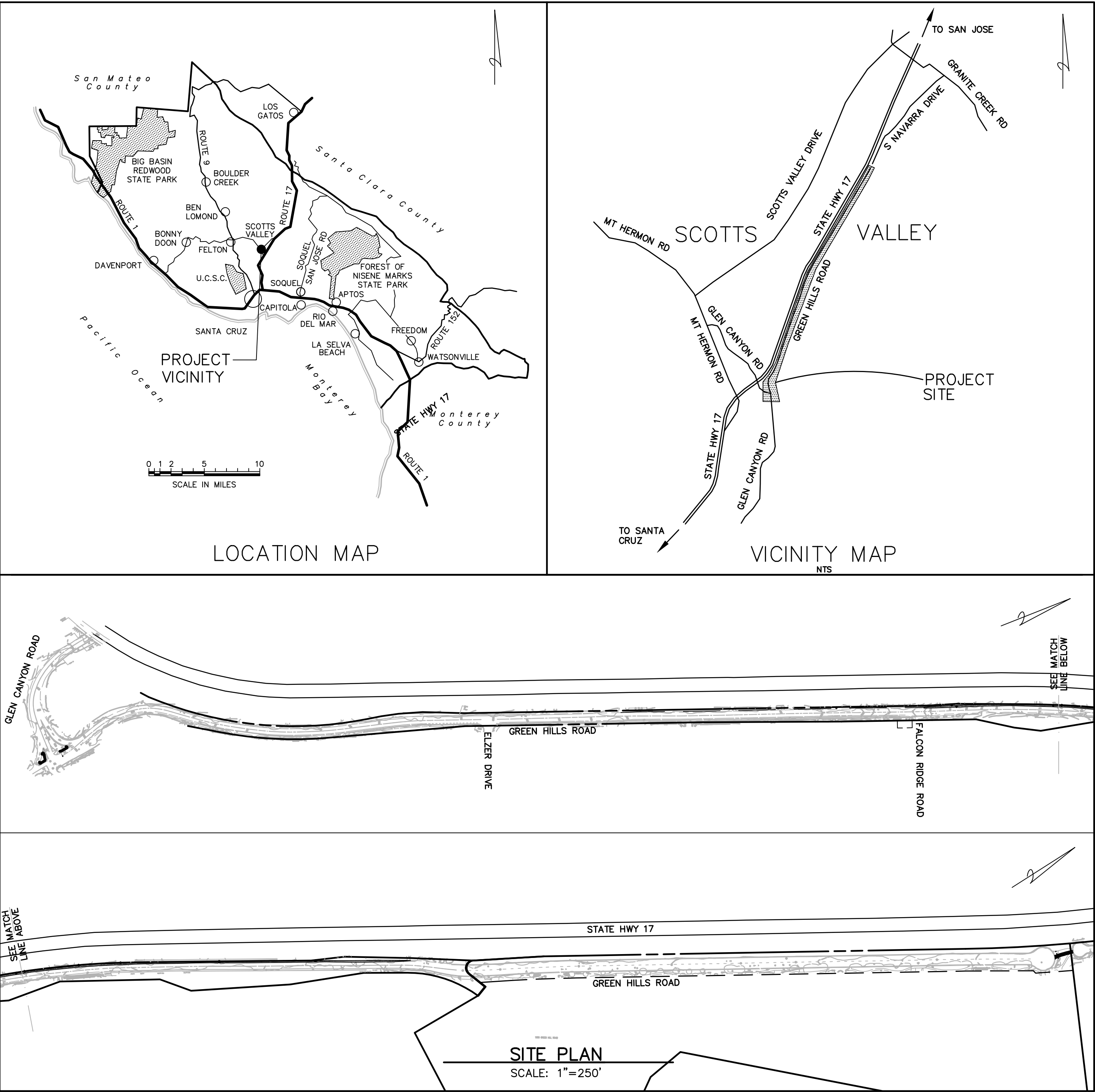
### SCOTTS VALLEY, CALIFORNIA

SHEET INDEX

C0.0	COVER SHEET
C0.1	DETAILS
C1.0	GLEN CANYON AND GREEN HILLS ROAD PAVEMENT IMPROVEMENTS STA 0+00 TO 6+40.65
C1.1	GREEN HILLS ROAD PAVEMENT IMPROVEMENTS STA 6+40.65 TO 32+32.49
C1.2	GREEN HILLS ROAD PAVEMENT IMPROVEMENTS STA 32+32.49 TO 58+00
C1.3	GREEN HILLS ROAD PAVEMENT IMPROVEMENTS STA 58+00 TO STA 69+18.95
C2.0	GLEN CANYON AND GREEN HILLS GRADING AND SECTIONS STA 0+00 TO STA 19+00
C2.1	GREEN HILLS ROAD GRADING AND SECTIONS STA 19+00 TO STA 32+00
C2.2	GREEN HILLS ROAD GRADING AND SECTIONS STA 32+00 TO STA 45+00
C2.3	GREEN HILLS ROAD GRADING AND SECTIONS STA 45+00 TO STA 69+18.95
C3.0	GLEN CANYON AND GREEN HILLS ROAD EROSION CONTROL PLANS STA 0+00 TO 6+40.65
C3.1	GREEN HILLS ROAD EROSION CONTROL PLAN STA 6+40.65 TO 32+32.49
C3.2	GREEN HILLS ROAD EROSION CONTROL PLAN STA 32+32.49 TO 58+00
C3.3	GREEN HILLS ROAD EROSION CONTROL PLAN STA 58+00 TO STA 69+18.95
C4.0	GLEN CANYON AND GREEN HILLS ROAD STRIPING AND SIGNAGE STA 0+00 TO 6+40.65
C4.1	GREEN HILLS ROAD STRIPING IMPROVEMENTS STA 6+40.65 TO 32+32.49
C4.2	GREEN HILLS ROAD STRIPING IMPROVEMENTS STA 32+32.49 TO 58+00
C4.3	GREEN HILLS ROAD STRIPING IMPROVEMENTS STA 58+00 TO STA 69+18.95 AND S. NAVARRA DRIVE PAVEMENT MARKINGS
C4.4	S. NAVARRA DRIVE PAVEMENT MARKINGS

CIVIL ABBREVIATIONS

AC	ASPHALT CONCRETE
ADA	AMERICANS WITH DISABILITIES ACT
APN	ASSESSORS PARCEL NUMBER
C	CURB
CAMUTCD	CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
CB	CATCH BASIN
CDPR	COLD CENTRAL PLANT RECYCLING
CONC	CONCRETE
DWY	DRIVEWAY
(E), EX	EXISTING
EG	EXISTING GRADE
ELEC	ELECTRIC
EP	EDGE OF PAVEMENT
FC	FACE OF CURB
FG	FINISHED GRADE
FL	FLOWLINE
FH	FIRE HYDRANT
G	GUTTER
GR	GRATE
GB	GRADE BREAK
HMA	HOT MIX ASPHALT
INV	INVERT
IP	IRON PIPE
MAG	MAGNETIC
(N)	NEW
±	PLUS OR MINUS
(P)	PROPOSED
PG&E	PACIFIC GAS & ELECTRIC
PT	POINT OF CURVE-TANGENT INTERSECTION
PVMT	PAVEMENT
RCE	REGISTERED CIVIL ENGINEER
RW	REDWOOD
SDCB	STORM DRAIN CATCH BASIN
SQ	SQUARE
STD	STANDARD
S/W	SIDEWALK
TC	TOP OF CURB
TELE	TELEPHONE
WM	WATER METER



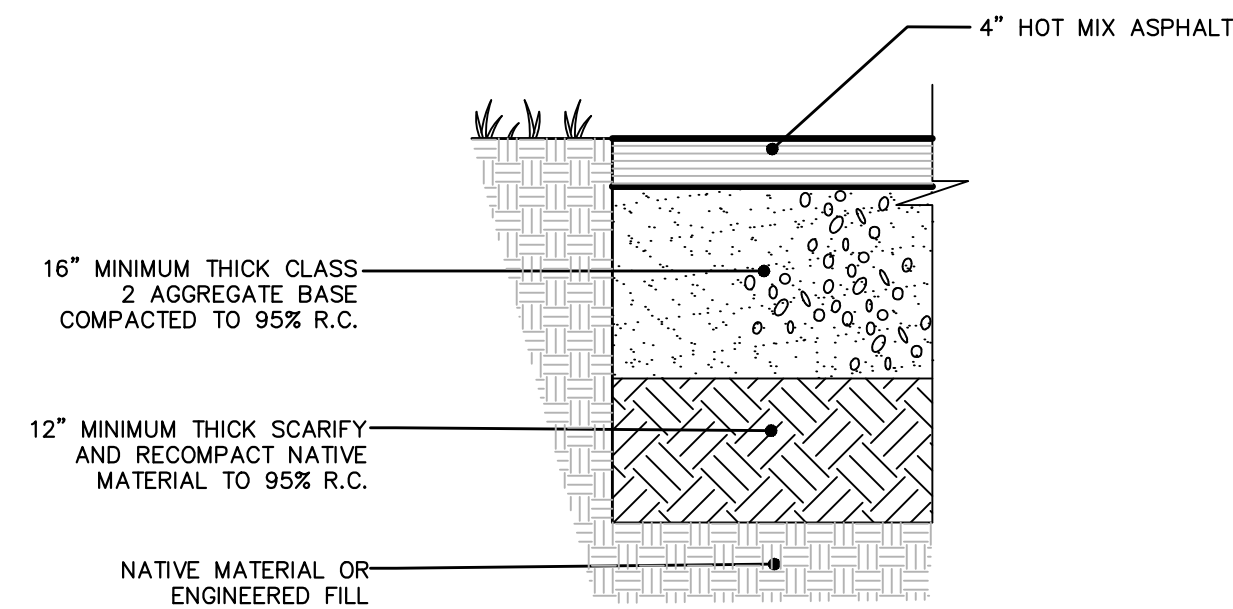
LEGEND

	ADD ALTERNATE 1: ADDITIONAL COST FOR CRACK SEALING AND 1/4" SLURRY OVERLAY
	BASE BID: NEW ASPHALT CONCRETE PAVEMENT
	BASE BID: MILLING AND HOT MIX ASPHALT
	BASE BID: MILLING AND HOT MIX ASPHALT AND IF CHOSEN ADD ALTERNATE 2: ADDITIONAL COST FOR FULL DEPTH RECLAMATION
	BASE BID: NEW ASPHALT CONCRETE PAVEMENT AND IF CHOSEN ADD ALTERNATE 2A: ADDITIONAL COST FOR EXTENSION OF FULL DEPTH RECLAMATION AND HOT MIX ASPHALT TO NON-PAVED AREA IN LEFT OF NEW ASPHALT CONCRETE PAVEMENT
	FENCELINE
	PROPERTY LINE
	CONCRETE CURB

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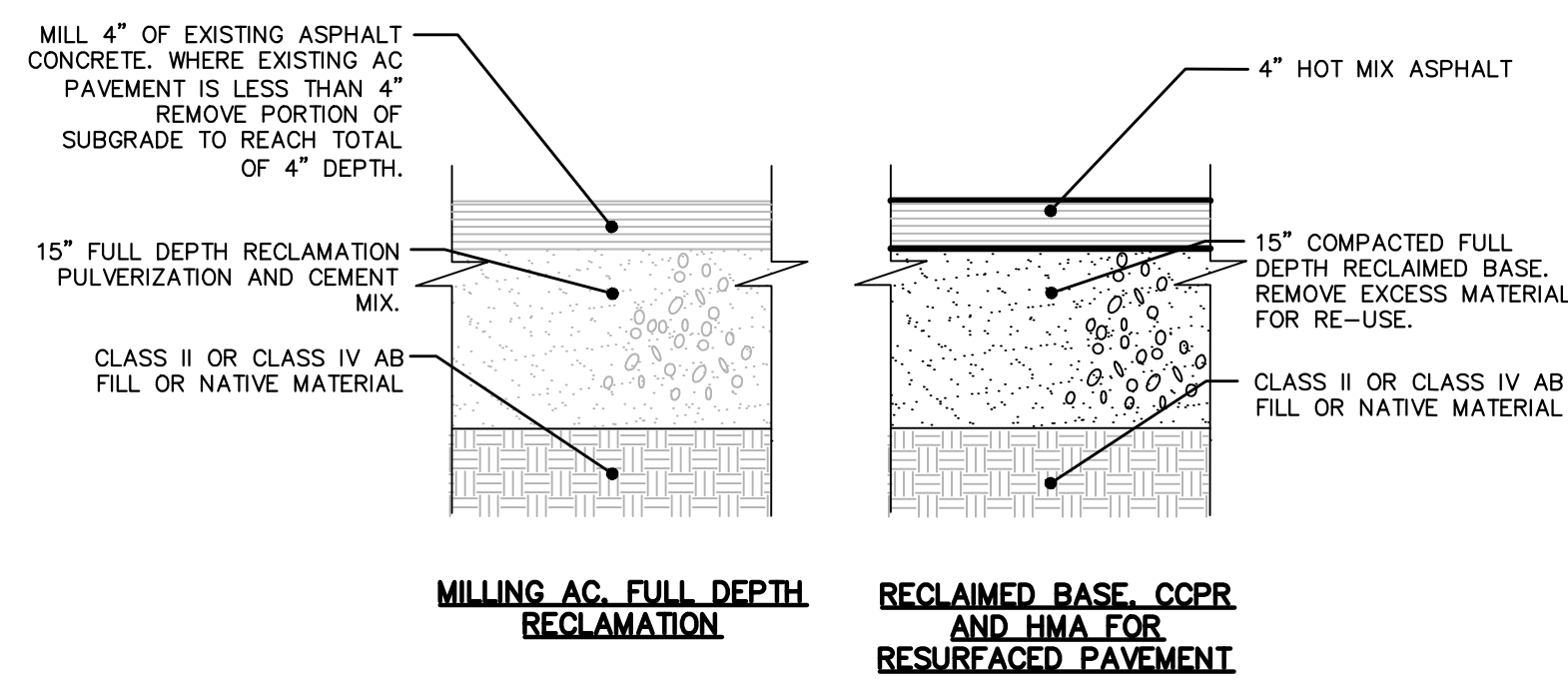
REVISED			
BOWMAN & WILLIAMS CONSULTING CIVIL ENGINEERS AND LAND SURVEYORS 1011 CEDAR STREET SANTA CRUZ, CA 95060 (831) 426-3560 www.bowmanandwilliams.com		COVER SHEET BIKE LANES PROJECT GLEN CANYON ROAD AND GREEN HILLS ROAD CITY OF SCOTTS VALLEY, DPW 701 LUNDY LANE, SCOTTS VALLEY, CA 95066	
SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63	SHEET
DATE APRIL 6, 2018	CHECKED JFR	INDEX	C0.0
DESIGN KAB	DWG NAME 25471.63	FILE NO.	OF



### NEW ASPHALT PAVEMENT SECTION

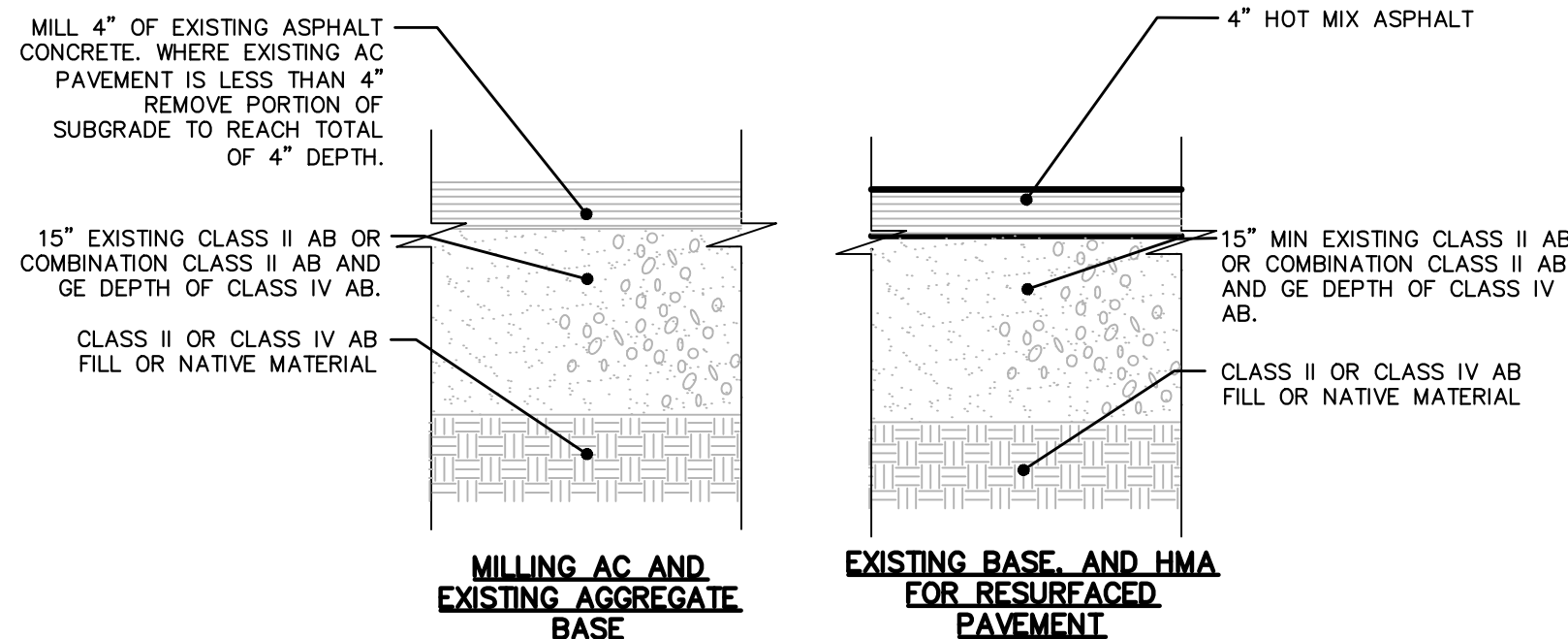
1  
C0.1  
SCALE: 3/4" = 1'-0"

2  
C0.1  
SCALE: 3/4" = 1'-0"



### TYPICAL MILLING AND HOT MIX ASPHALT AND FULL DEPTH RECLAMATION SECTION

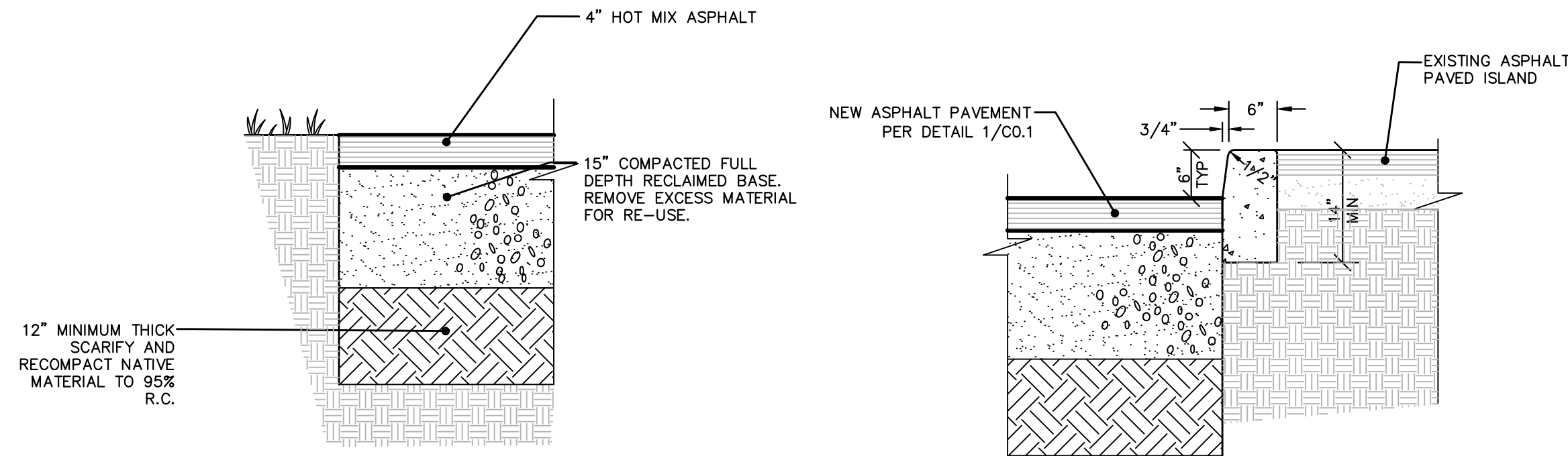
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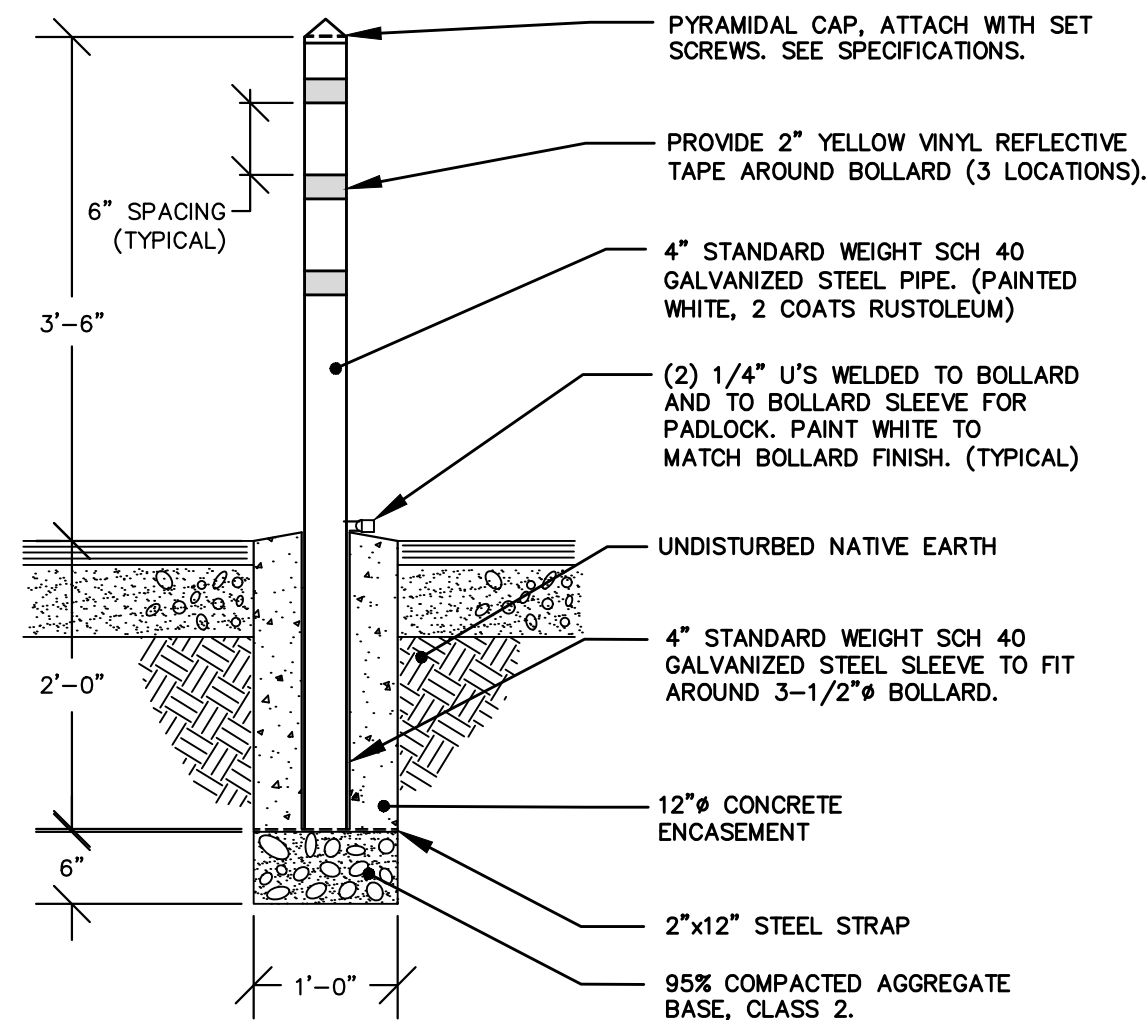
### TYPICAL MILLING AND HOT MIX ASPHALT SECTION

4  
C0.1  
SCALE: 3/4" = 1'-0"

5  
C0.1  
SCALE: 3/4" = 1'-0"



6  
C0.1  
SCALE: 3/4" = 1'-0"



### REMOVABLE BOLLARD

7  
C0.1  
SCALE: 3/4"=1'-0"

### UNDERGROUND NOTES

- ALL SURVEYED EXISTING UTILITY LINES ARE SHOWN FOR INFORMATION ONLY. CONTRACTOR SHALL EXERCISE ALL NECESSARY CAUTION TO AVOID DAMAGE TO ANY EXISTING UTILITIES OR FACILITIES TO REMAIN IN PLACE, WHETHER OR NOT SUCH LINES OR FACILITIES ARE SHOWN ON THESE PLANS, AND SHALL BEAR FULL RESPONSIBILITY FOR ANY DAMAGE THERE TO. CONTRACTOR IS ADVISED TO CONTACT A UNDERGROUND SERVICE LOCATOR FOR MARKING UNDERGROUND LINES PRIOR TO BEGINNING WORK. THE PROJECT INSPECTOR SHALL BE PRESENT WHILE THE UNDERGROUND SERVICE LOCATOR IS ONSITE.

### EARTHWORK AND GRADING NOTES:

- WORK SHALL CONSIST OF ALL CLEARING, GRUBBING, STRIPPING, PREPARATION OF LAND TO BE FILLED, EXCAVATION, SPREADING, COMPACTION AND CONTROL OF FILL, AND ALL SUBSIDIARY WORK NECESSARY TO COMPLETE THE GRADING TO CONFORM TO THE LINES, GRADES, AND SLOPES, AS SHOWN ON THE APPROVED PLANS.
- ALL GRADING OPERATIONS SHALL BE DONE IN ACCORDANCE WITH THE "GEOTECHNICAL INVESTIGATION DESIGN PHASE FOR PROPOSED ROAD IMPROVEMENTS, GREEN HILLS ROAD, SCOTTS VALLEY, CA" DATED APRIL 19, 2017, "ADDENDUM INVESTIGATION PROPOSED ROAD IMPROVEMENTS GREEN HILLS ROAD SCOTTS VALLEY, CALIFORNIA" LETTER DATED DECEMBER 13, 2017, AND "GEOTECHNICAL PLAN REVIEW PROPOSED ROAD IMPROVEMENTS GREEN HILLS ROAD SCOTTS VALLEY, CALIFORNIA" LETTER DATED DECEMBER 14, 2017, ALL PREPARED BY BUTANO GEOTECHNICAL ENGINEERING, INC., PROJECT NO. 17-111-SC, AND 17-111-M.
- THE CONTRACTOR SHALL GRADE TO THE LINE AND ELEVATIONS SHOWN ON THE PLAN AND SHALL SECURE THE SERVICES OF A LICENSED LAND SURVEYOR OR REGISTERED CIVIL ENGINEER TO PROVIDE STAKES FOR LINE AND GRADE.
- MILLINGS AND THE EXCESS SOIL FROM GRADING OPERATIONS SHALL BE HAULED OFF SITE AND DISPOSED AT THE LOCAL LANDFILL, OR TAKEN TO A SITE WITH AN ACTIVE GRADING PERMIT. A HAUL ROUTE SHALL BE PROVIDED TO THE CITY PRIOR TO COMMENCEMENT OF WORK.

### DISCLAIMER

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BOWMAN & WILLIAMS CONSULTING CIVIL ENGINEERS AND LAND SURVEYORS 1011 CEDAR STREET SANTA CRUZ, CA 95060 (831) 426-3560 www.bowmanandwilliams.com		DETAILS AND PROJECT SPECIFICATIONS BIKE LANES PROJECT GLEN CANYON ROAD AND GREEN HILLS ROAD CITY OF SCOTTS VALLEY, DPW 701 LUNDY LANE, SCOTTS VALLEY, CA 95066	
SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63	SHEET C0.1
DATE APRIL 6, 2018	CHECKED JFR	INDEX	OF
DESIGN KAB	DWG NAME 25471.63	FILE NO.	



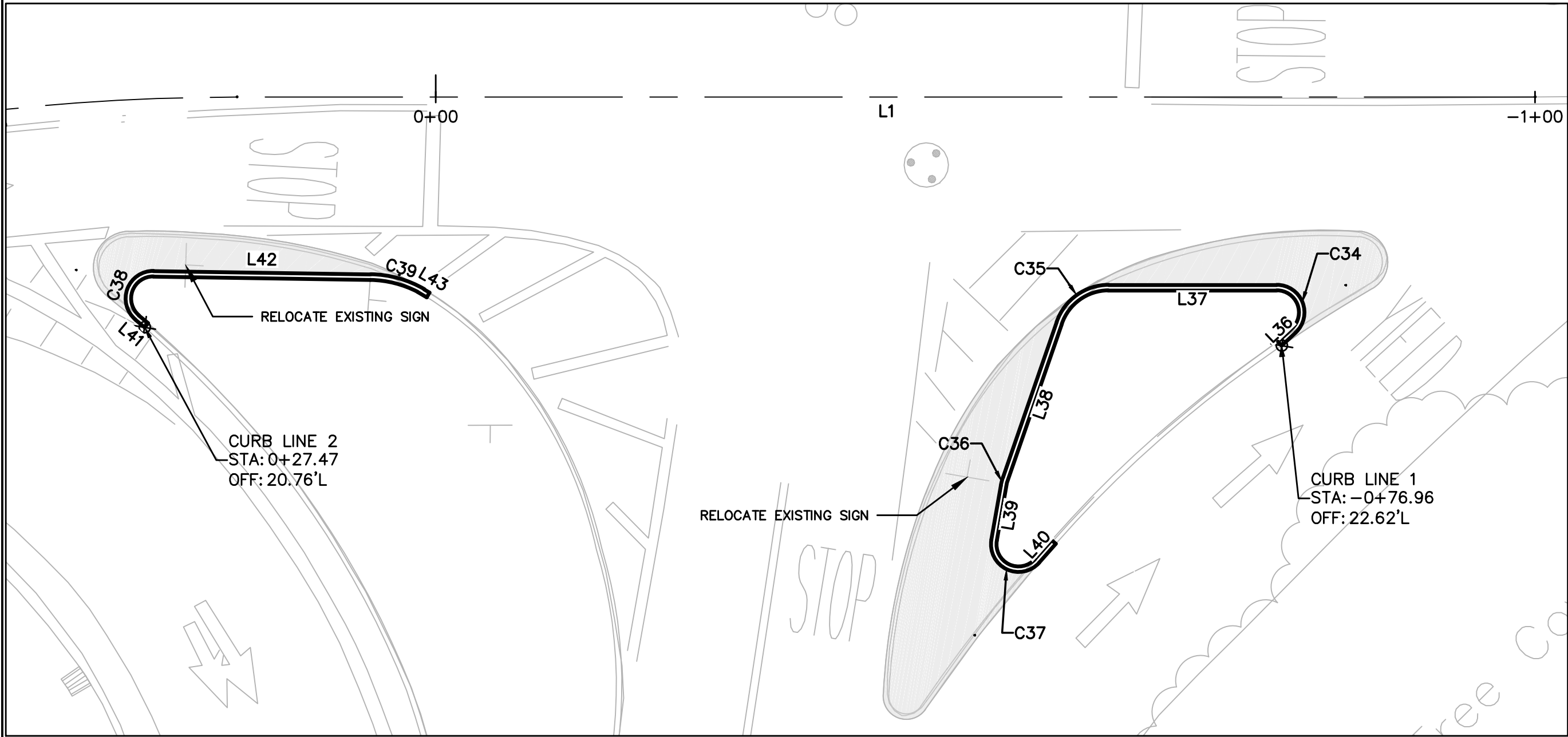
CURB LINE 1 STATIONING

LINE AND CURVE TABLE BY STATION				
NUMBER	STATION	BEARING / Δ (DELTA)	RADIUS	LENGTH
L36	0+00.00	S 52° 45' 19" E		1.73 '
C34	0+01.73	Δ 136° 37' 51"	2.50 '	5.96 '
L37	0+07.69	N 9° 23' 10" W		15.29 '
C35	0+22.98	Δ 70° 58' 51"	5.00 '	6.19 '
L38	0+29.18	N 80° 22' 01" W		14.95 '
C36	0+44.13	Δ 9° 10' 03"	5.00 '	0.80 '
L39	0+44.93	N 89° 32' 04" W		4.98 '
C37	0+49.91	Δ 148° 21' 23"	2.50 '	6.47 '
L40	0+56.38	S 57° 53' 28" E		2.29 '

CURB LINE 2 STATIONING

LINE AND CURVE TABLE BY STATION				
NUMBER	STATION	BEARING / Δ (DELTA)	RADIUS	LENGTH
L41	0+00.00	N 30° 37' 53" E		1.10 '
C38	0+01.10	Δ 140° 48' 46"	2.50 '	6.14 '
L42	0+07.24	S 8° 33' 21" E		19.75 '
C39	0+26.99	Δ 29° 22' 01"	10.00 '	5.13 '
L43	0+32.12	S 20° 48' 40" W		0.56 '

MATCHLINE A (SEE SHEET C1.1)



CURB PLAN

SCALE 1" = 10'

GREEN HILLS ROAD CENTERLINE STATIONING

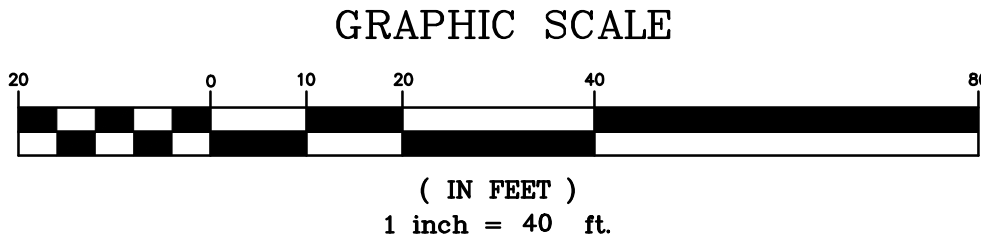
LINE AND CURVE TABLE BY STATION				
NUMBER	STATION	BEARING / Δ (DELTA)	RADIUS	LENGTH
L1	-1+00.00	N 9° 25' 35" W		118.08 '
C1	0+18.08	Δ 12° 30' 28"	200.00 '	43.66 '
L2	0+61.74	N 21° 56' 03" W		23.59 '
C2	0+85.33	Δ 6° 00' 40"	200.00 '	20.98 '
L3	1+06.31	N 15° 55' 23" W		70.72 '
C3	1+77.03	Δ 10° 07' 58"	200.00 '	35.37 '
L4	2+12.40	N 5° 47' 24" W		13.95 '
C4	2+26.36	Δ 12° 21' 09"	200.00 '	43.12 '
L5	2+69.47	N 6° 33' 45" E		10.54 '
C5	2+80.01	Δ 31° 27' 40"	250.00 '	137.28 '
L6	4+17.29	N 38° 01' 25" E		88.42 '
C6	5+05.70	Δ 4° 35' 57"	600.00 '	48.16 '
L7	5+53.87	N 33° 25' 28" E		58.90 '
C7	6+12.77	Δ 5° 25' 53"	300.00 '	28.44 '
L8	6+41.21	N 27° 59' 35" E		79.19 '
C8	7+20.40	Δ 6° 02' 11"	1000.00 '	105.35 '
L9	8+25.75	N 21° 57' 24" E		28.19 '
C9	8+53.94	Δ 7° 37' 35"	1000.00 '	133.10 '
L10	9+87.05	N 14° 19' 49" E		179.45 '
C10	11+66.49	Δ 6° 16' 05"	1000.00 '	109.40 '

LINE AND CURVE TABLE BY STATION				
NUMBER	STATION	BEARING / Δ (DELTA)	RADIUS	LENGTH
L11	12+75.89	N 20° 35' 54" E		618.68 '
C11	18+94.57	Δ 1° 37' 29"	1000.00 '	28.36 '
L12	19+22.93	N 18° 58' 25" E		56.90 '
C12	19+79.83	Δ 2° 19' 40"	1000.00 '	40.63 '
L13	20+20.45	N 21° 18' 05" E		120.89 '
C13	21+41.35	Δ 0° 54' 18"	1500.00 '	23.69 '
L14	21+65.04	N 20° 23' 47" E		661.16 '
C14	28+26.20	Δ 1° 00' 13"	1500.00 '	26.27 '
L15	28+52.47	N 21° 24' 00" E		51.49 '
C15	29+03.96	Δ 0° 36' 24"	800.00 '	8.47 '
L16	29+12.43	N 20° 47' 36" E		40.39 '
C16	29+52.82	Δ 2° 48' 53"	800.00 '	39.30 '
L17	29+92.12	N 17° 58' 42" E		7.61 '
C17	29+99.73	Δ 1° 39' 55"	200.00 '	5.81 '
L18	30+05.55	N 19° 38' 38" E		184.36 '
C18	31+89.90	Δ 2° 14' 13"	800.00 '	31.23 '
L19	32+21.14	N 21° 52' 50" E		105.30 '
C19	33+26.43	Δ 4° 26' 44"	2500.00 '	193.97 '
L20	35+20.40	N 26° 19' 34" E		107.16 '
C20	36+27.56	Δ 3° 24' 04"	800.00 '	47.49 '

LINE AND CURVE TABLE BY STATION				
NUMBER	STATION	BEARING / Δ (DELTA)	RADIUS	LENGTH
L21	36+75.05	N 29° 43' 38" E		37.84 '
C21	37+12.89	Δ 1° 37' 07"	800.00 '	22.60 '
L22	37+35.49	N 31° 20' 45" E		220.15 '
C22	39+55.64	Δ 0° 47' 05"	2000.00 '	27.40 '
L23	39+83.03	N 32° 07' 50" E		415.57 '
C23	43+98.61	Δ 0° 09' 32"	10000.00 '	27.71 '
L24	44+26.32	N 31° 58' 19" E		239.65 '
C24	46+65.97	Δ 6° 57' 08"	1000.00 '	121.34 '
L25	47+87.31	N 38° 55' 27" E		78.37 '
C25	48+65.68	Δ 9° 35' 18"	800.00 '	133.88 '
L26	49+99.56	N 29° 20' 09" E		182.55 '
C26	51+82.11	Δ 2° 11' 43"	2000.00 '	76.63 '
L27	52+58.75	N 31° 31' 52" E		1308.85 '
C27	65+67.59	Δ 0° 36' 23"	5000.00 '	52.91 '
L28	66+20.50	N 32° 08' 15" E		208.29 '
C28	68+28.79	Δ 0° 22' 22"	2000.00 '	13.01 '
L29	68+41.80	N 32° 30' 37" E		76.75 '

REPAVING PLAN

SCALE 1" = 40'



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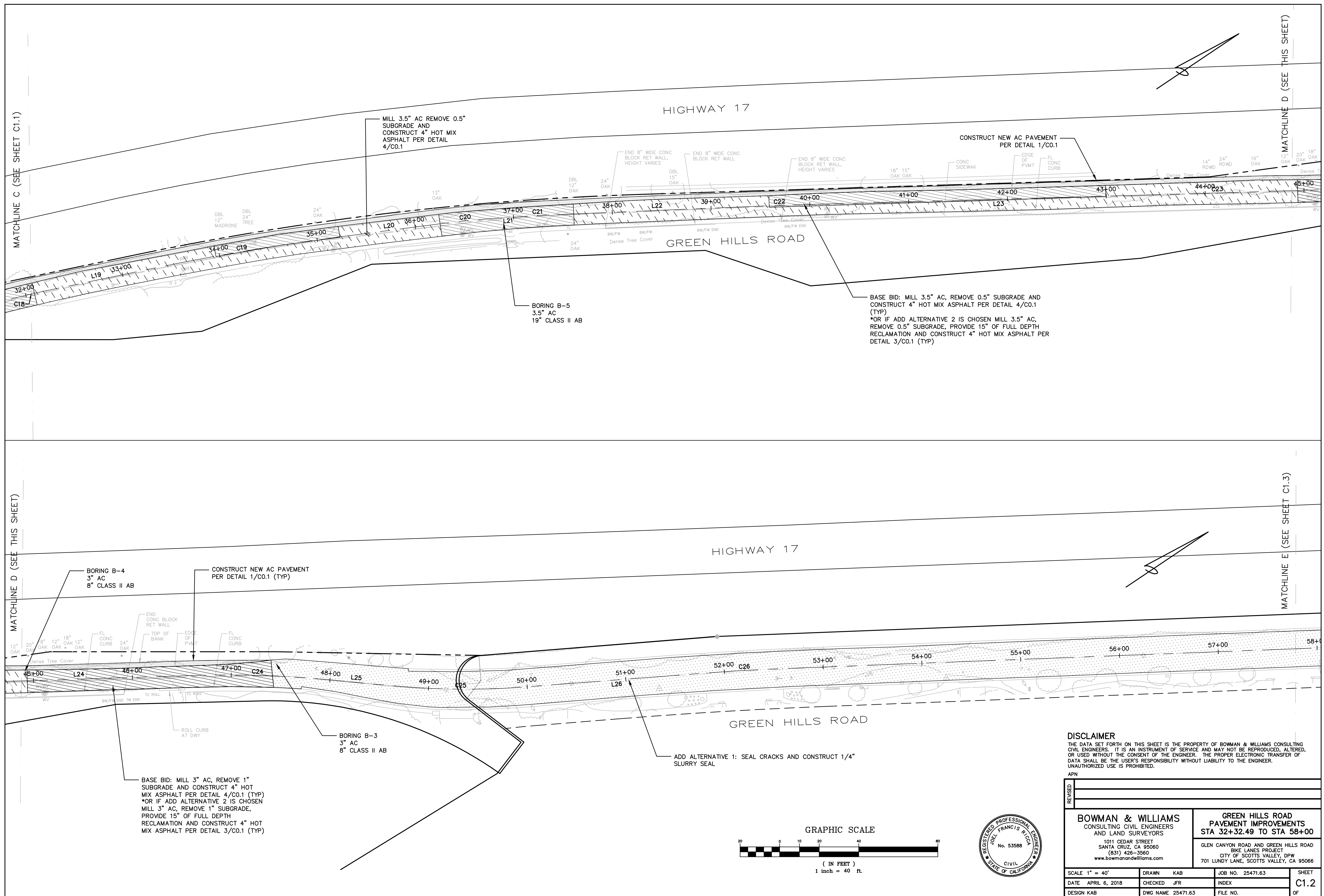
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<b>BOWMAN &amp; WILLIAMS</b> CONSULTING CIVIL ENGINEERS AND LAND SURVEYORS 1011 CEDAR STREET SANTA CRUZ, CA 95060 (831) 426-3560 www.bowmanandwilliams.com			<b>GLEN CANYON AND GREEN HILLS ROAD PAVEMENT IMPROVEMENTS STA 0+00 TO STA 6+40.65</b>		
			GLEN CANYON ROAD AND GREEN HILLS ROAD BIKE LANES PROJECT CITY OF SCOTT'S VALLEY, DPW 701 LUNDY LANE, SCOTT'S VALLEY, CA 95066		
SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63		SHEET	
DATE APRIL 6, 2018	CHECKED JFR	INDEX		C1.0	
DESIGN KAB	DWG NAME 25471.63	FILE NO.		OF	

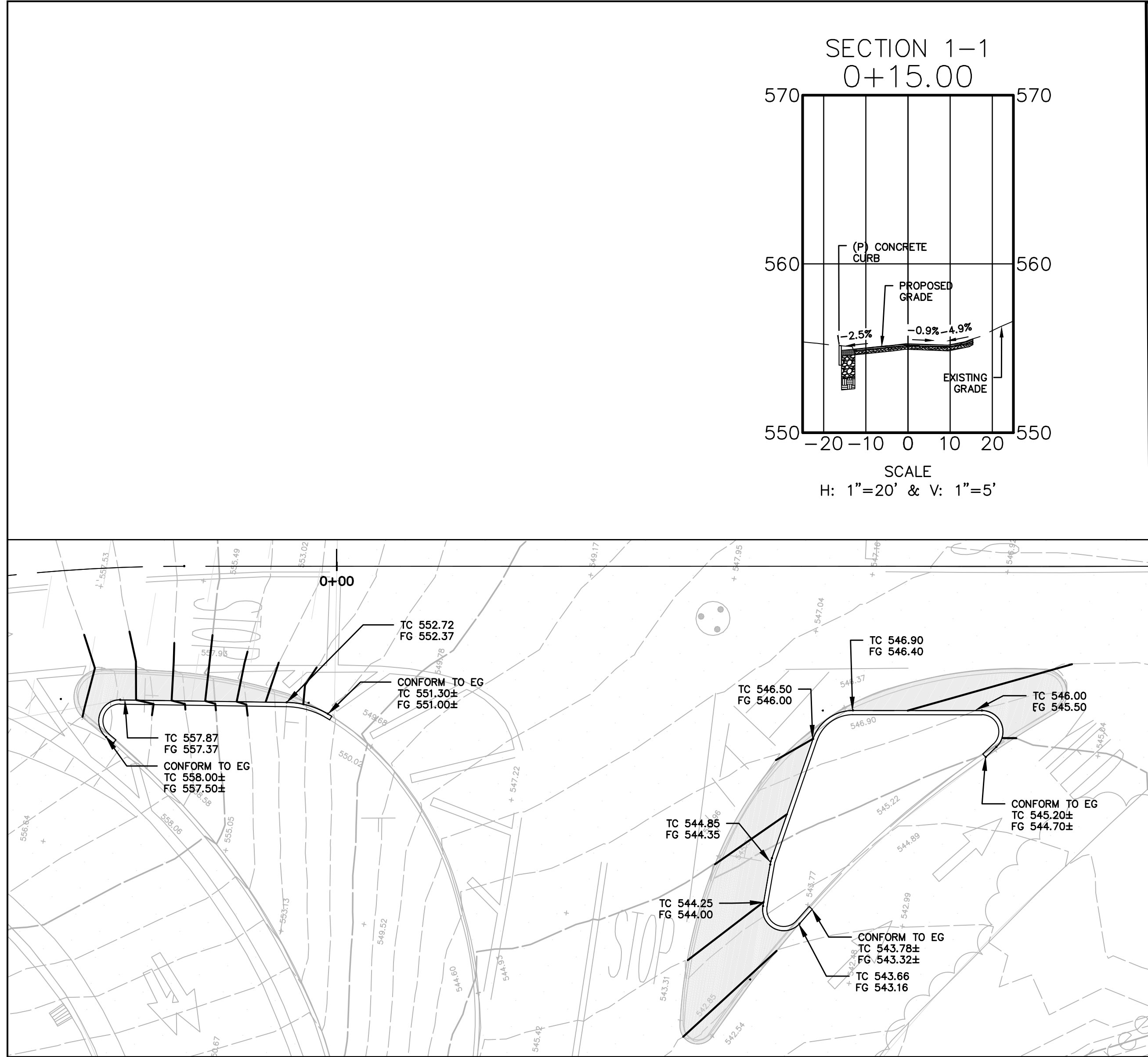




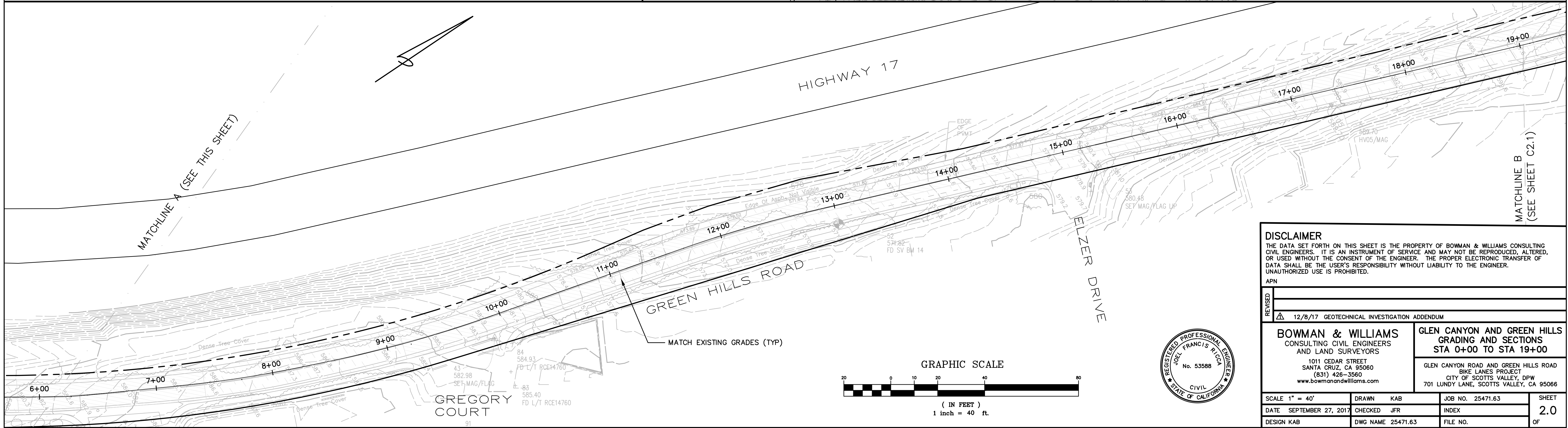
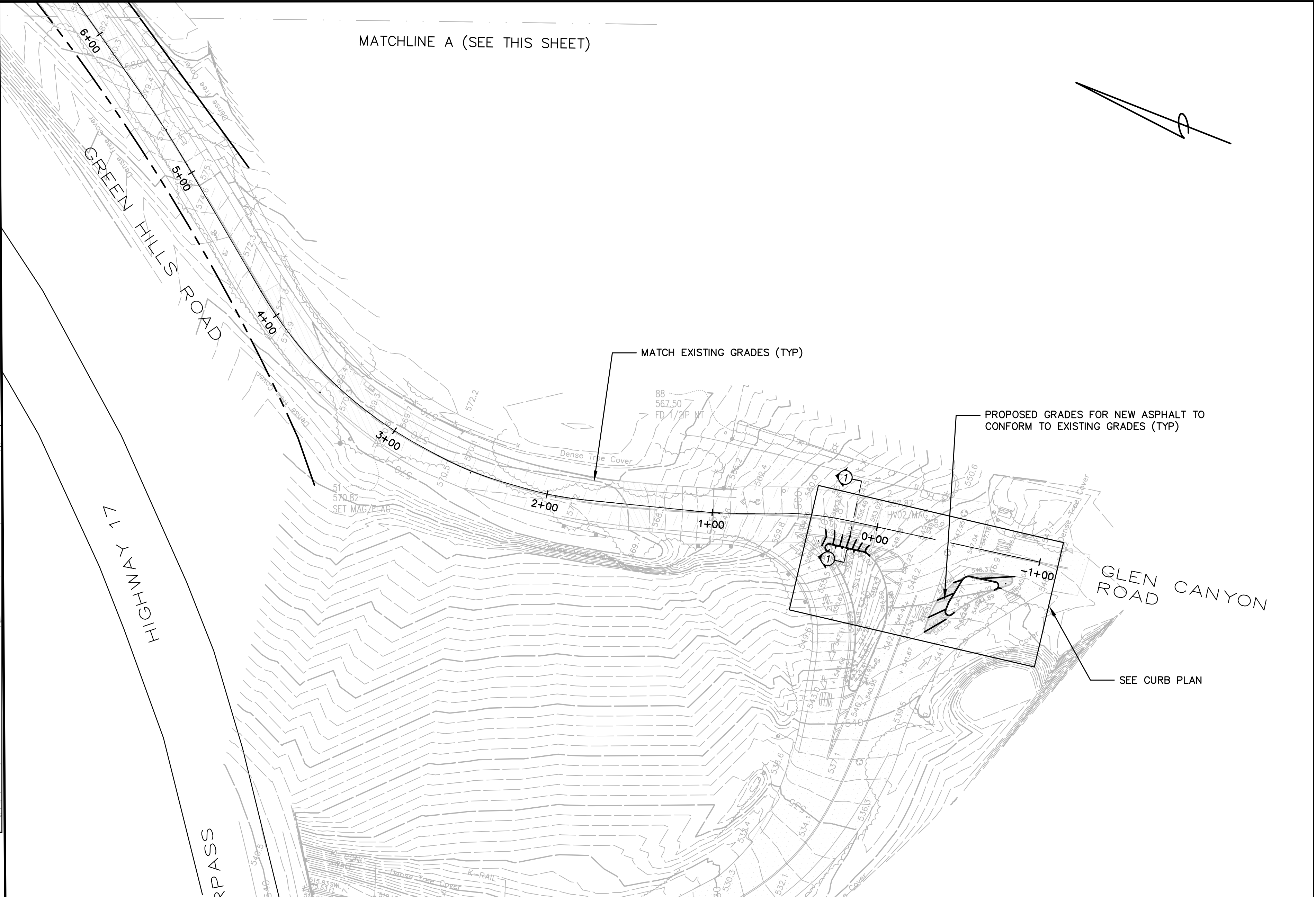






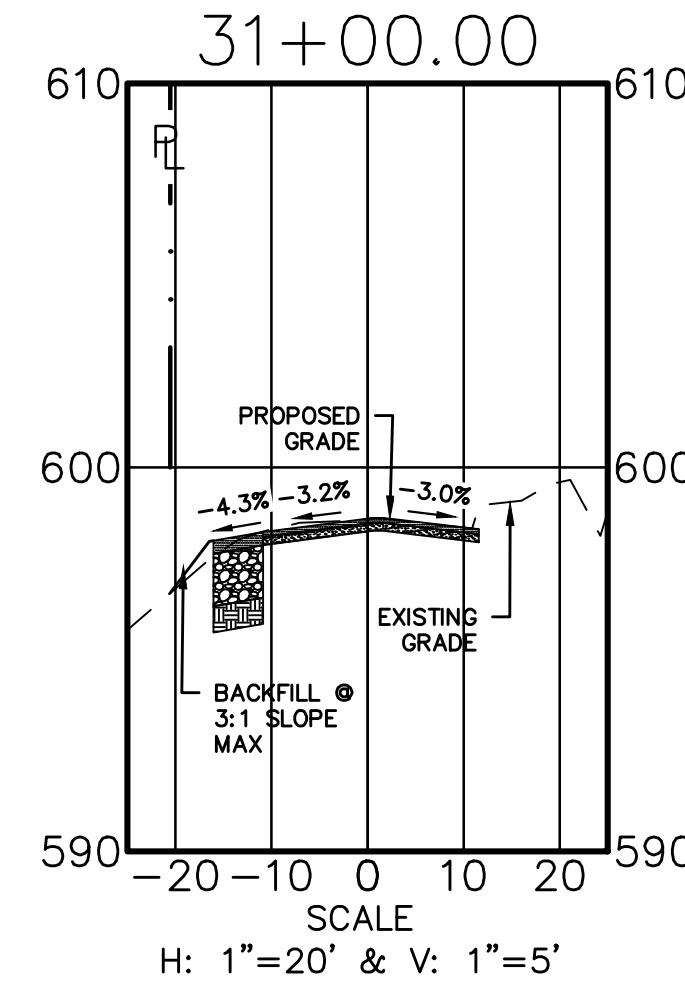
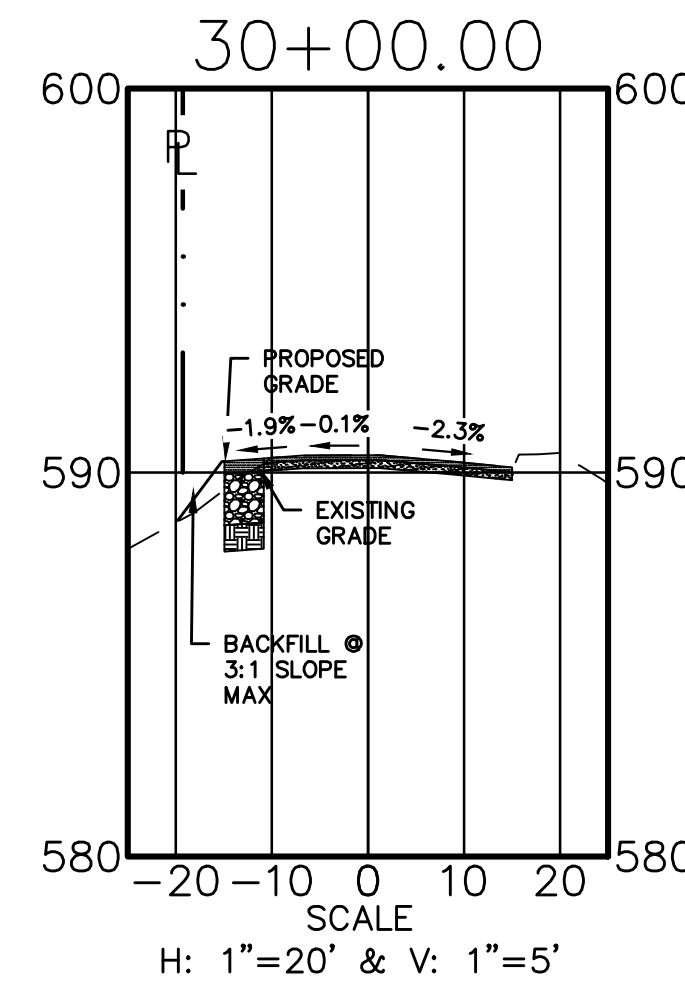
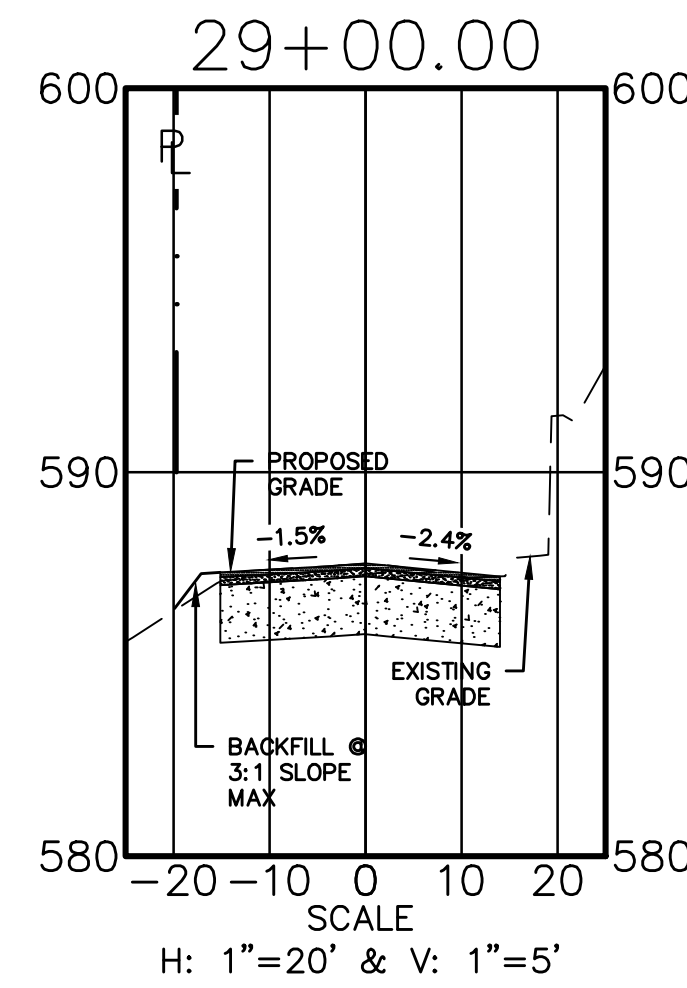
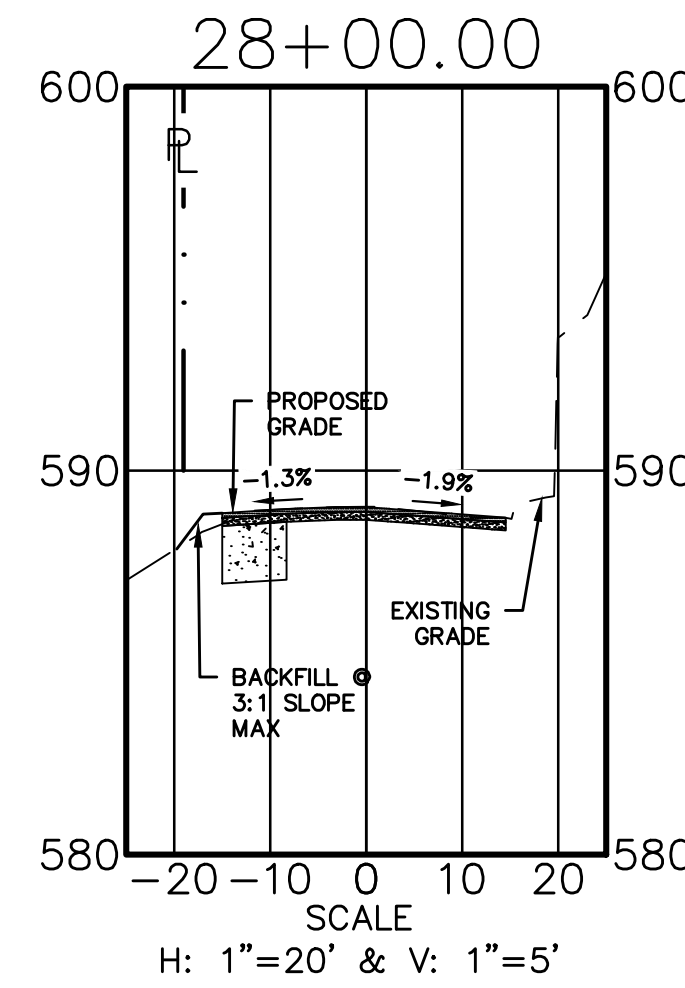
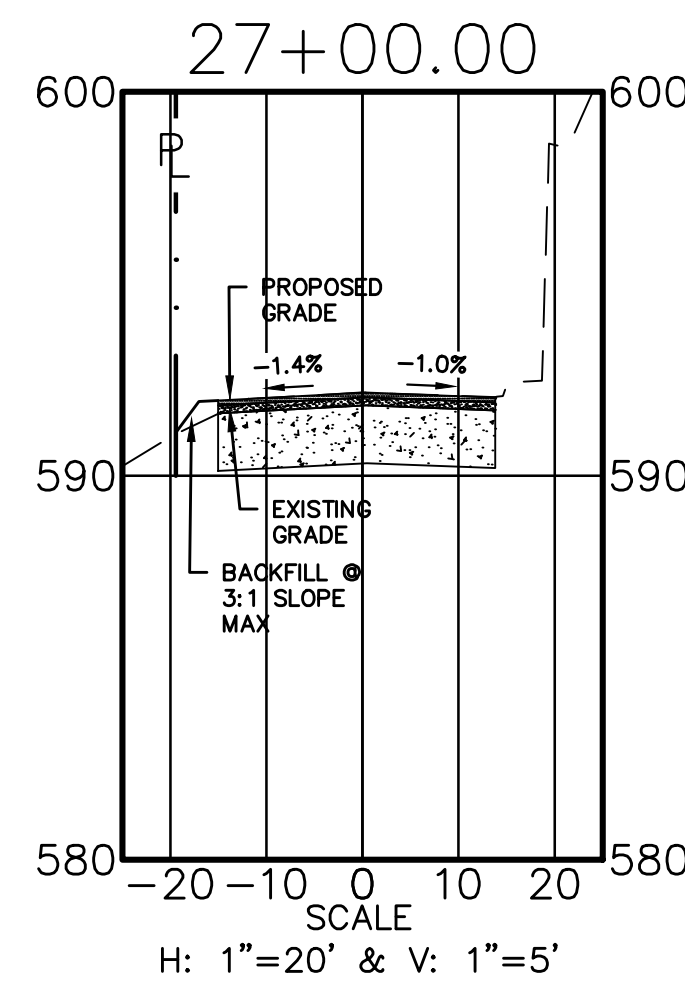
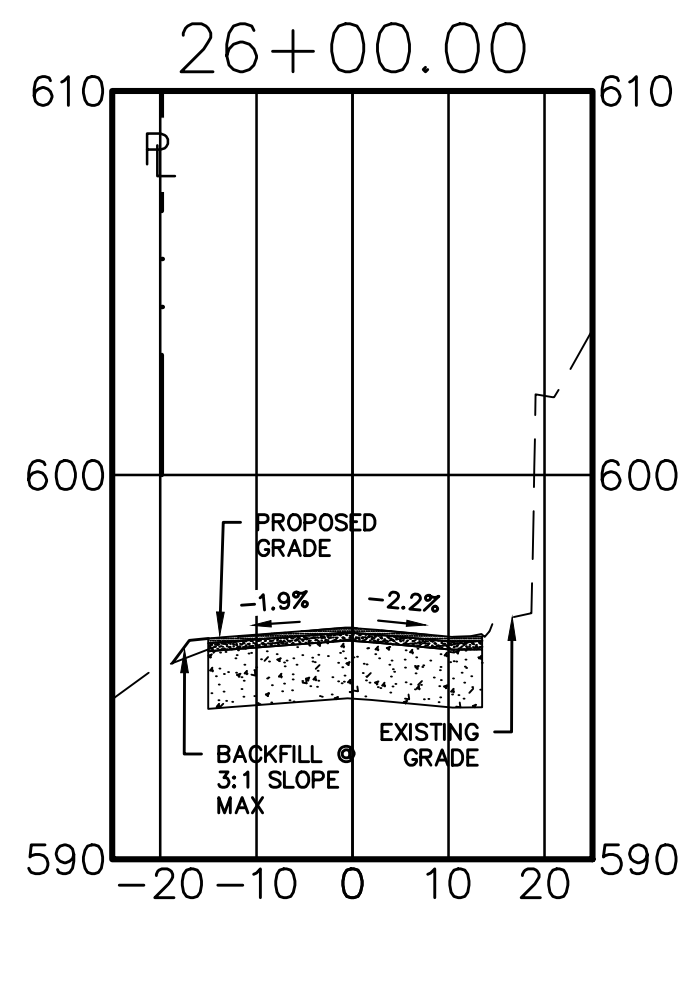
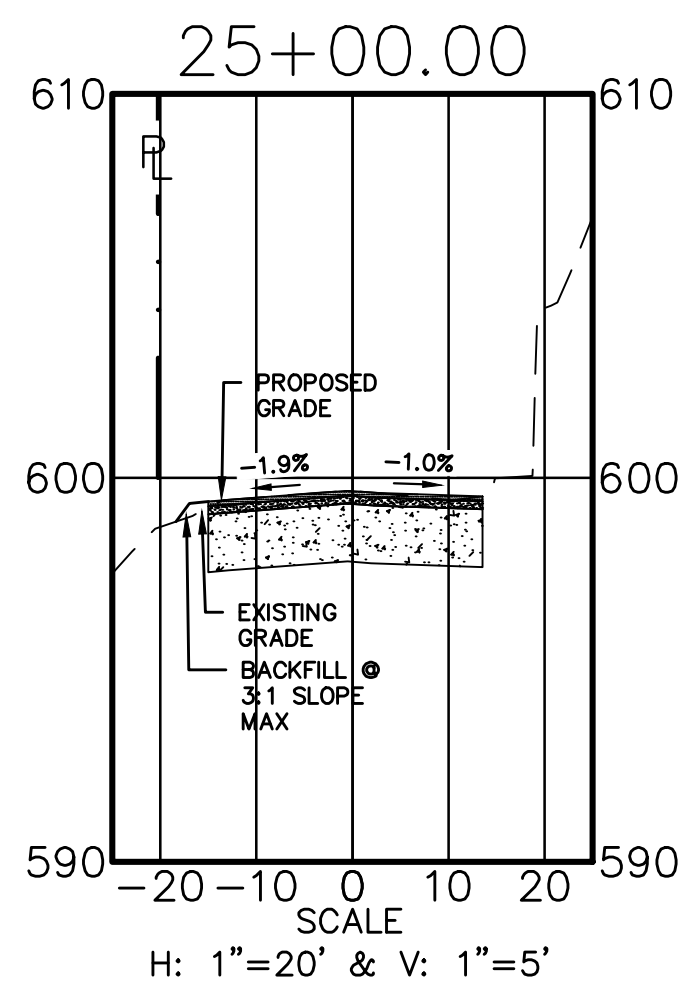
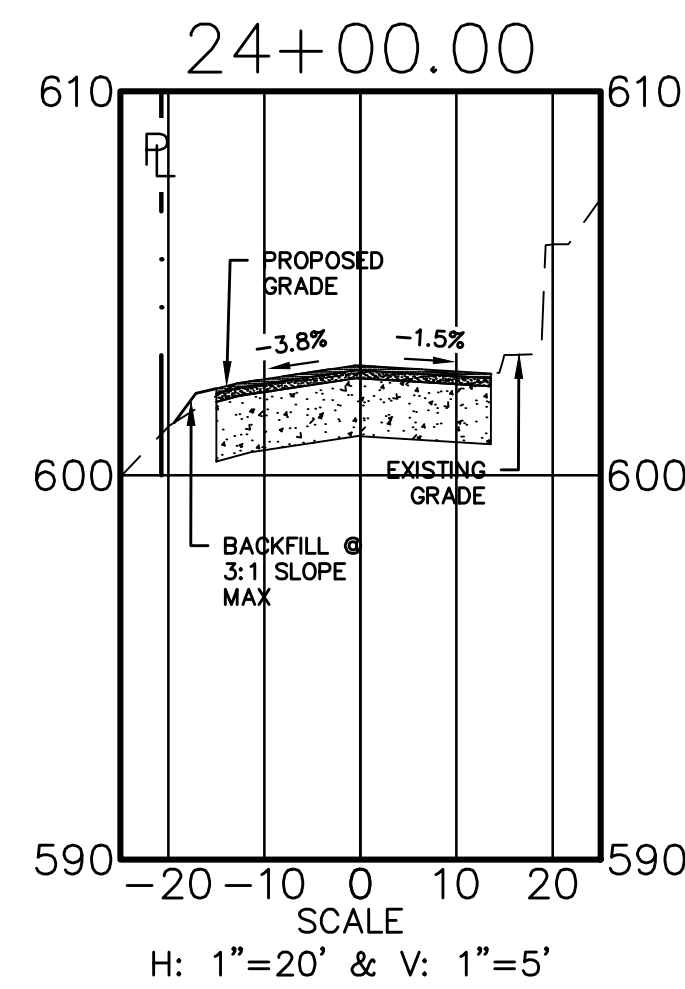
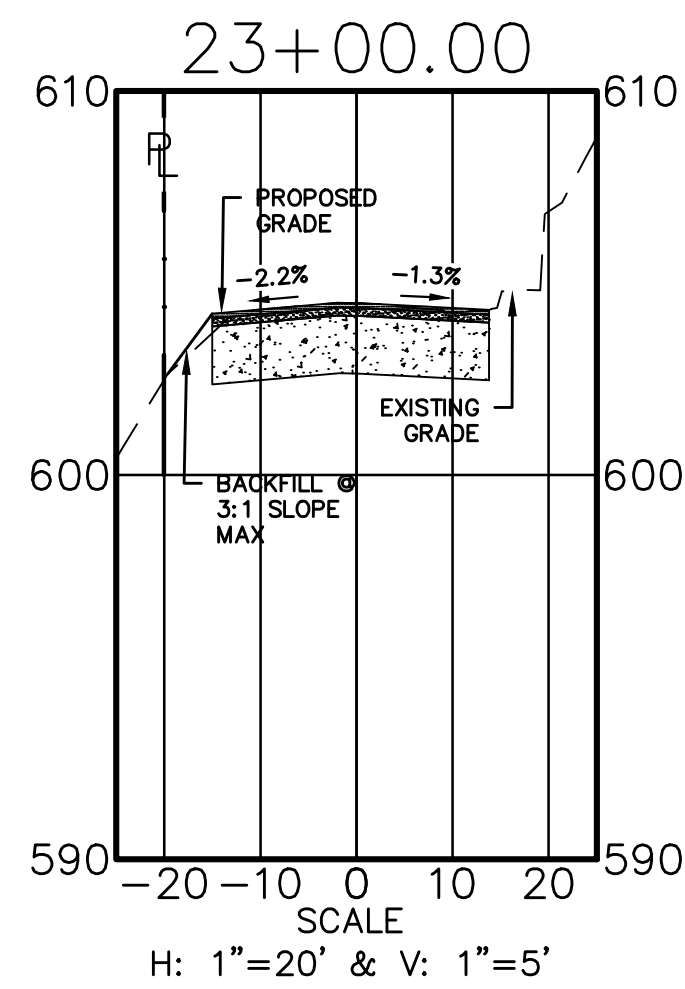
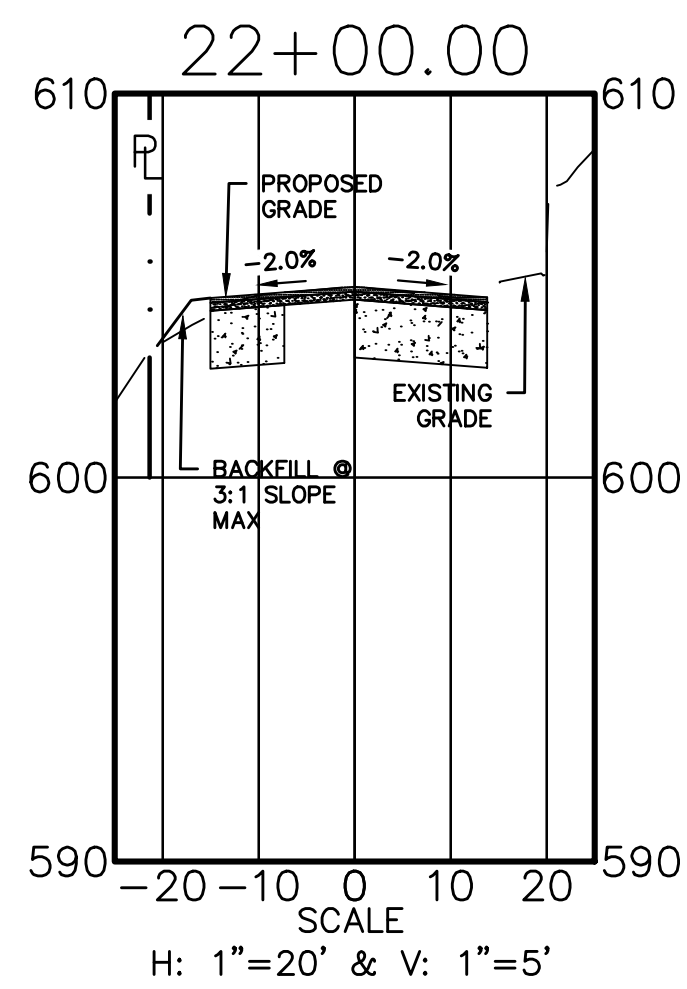
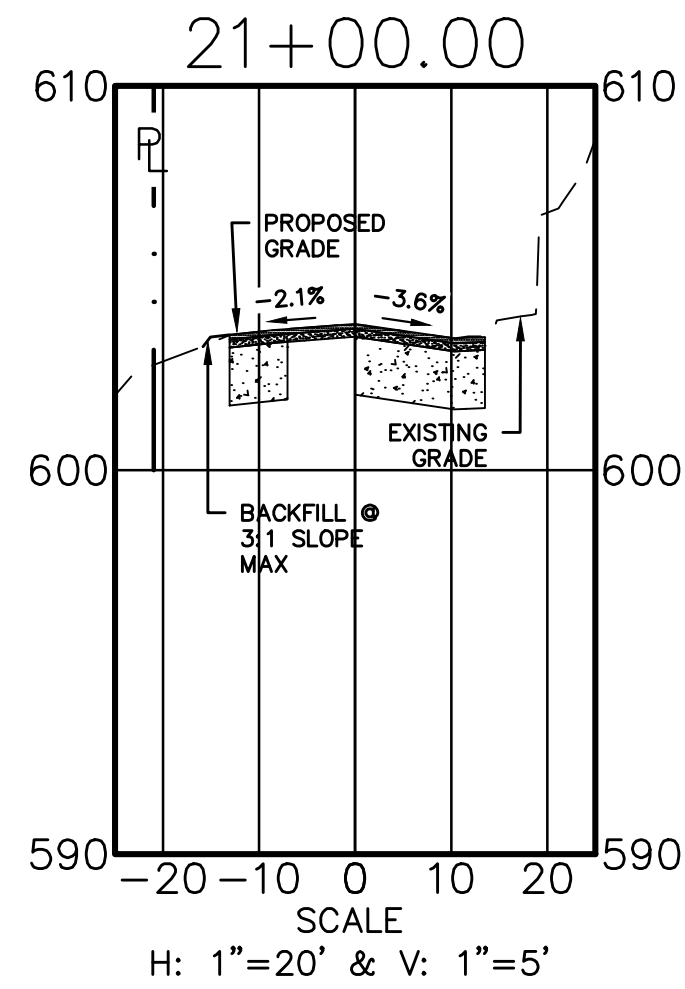
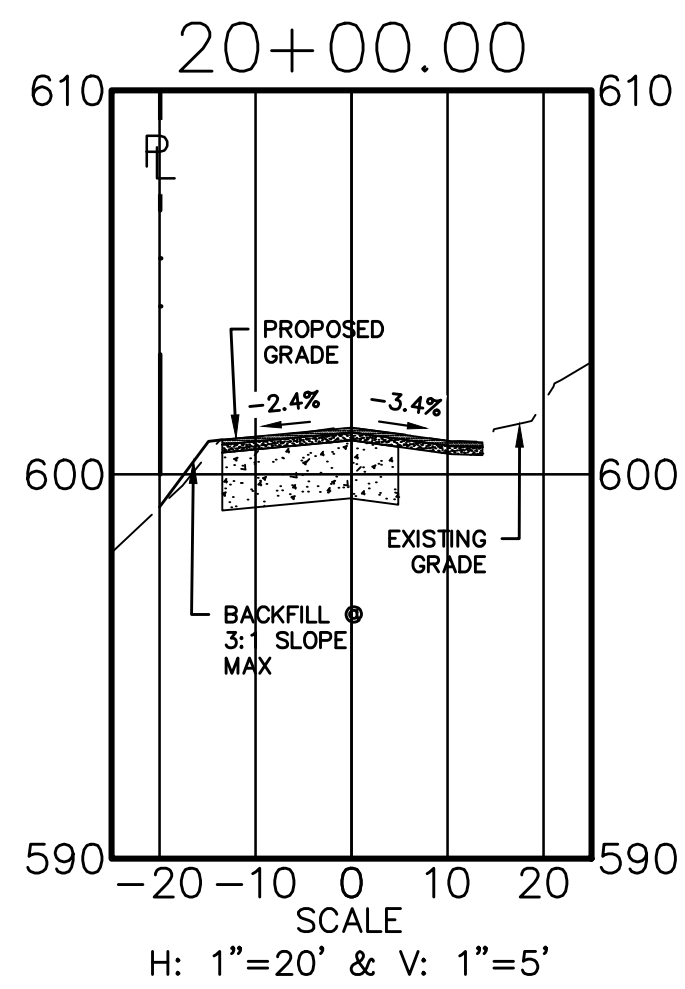
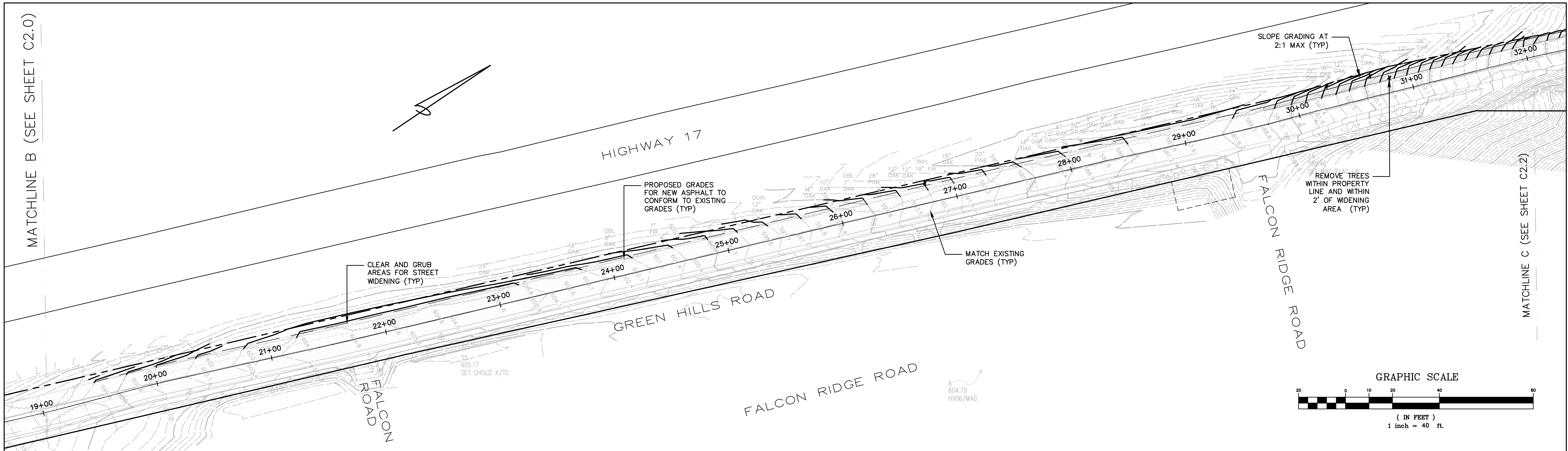


CURB PLAN  
SCALE 1" = 10'



MATCHLINE B (SEE SHEET C2.0)

MATCHLINE C (SEE SHEET C2.2)



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REVISIONS			
12/8/17 GEOTECHNICAL INVESTIGATION ADDENDUM			
<b>BOWMAN &amp; WILLIAMS</b> CONSULTING CIVIL ENGINEERS AND LAND SURVEYORS 1011 CEDAR STREET SANTA CRUZ, CA 95060 (831) 426-3560 www.bowmanandwilliams.com		<b>GREEN HILLS ROAD</b> GRADING AND SECTIONS STA 19+00 TO 32+00 BIKE LANES PROJECT GLEN CANYON ROAD AND GREEN HILLS ROAD CITY OF SCOTT'S VALLEY, DPW 701 LUNDY LANE, SCOTT'S VALLEY, CA 95066	
SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63	SHEET
DATE SEPTEMBER 27, 2017	CHECKED JFR	INDEX	C2.1
DESIGN KAB	DWG NAME 25471.63	FILE NO.	OF

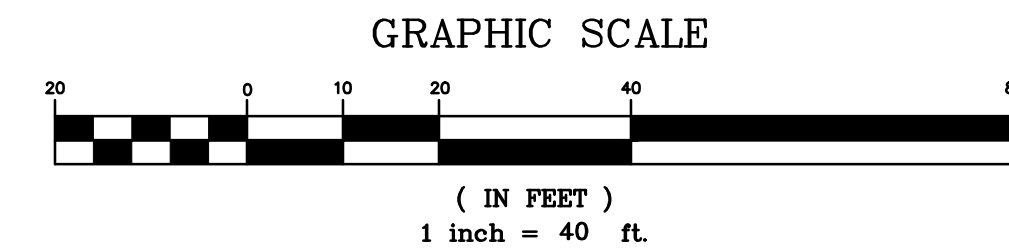
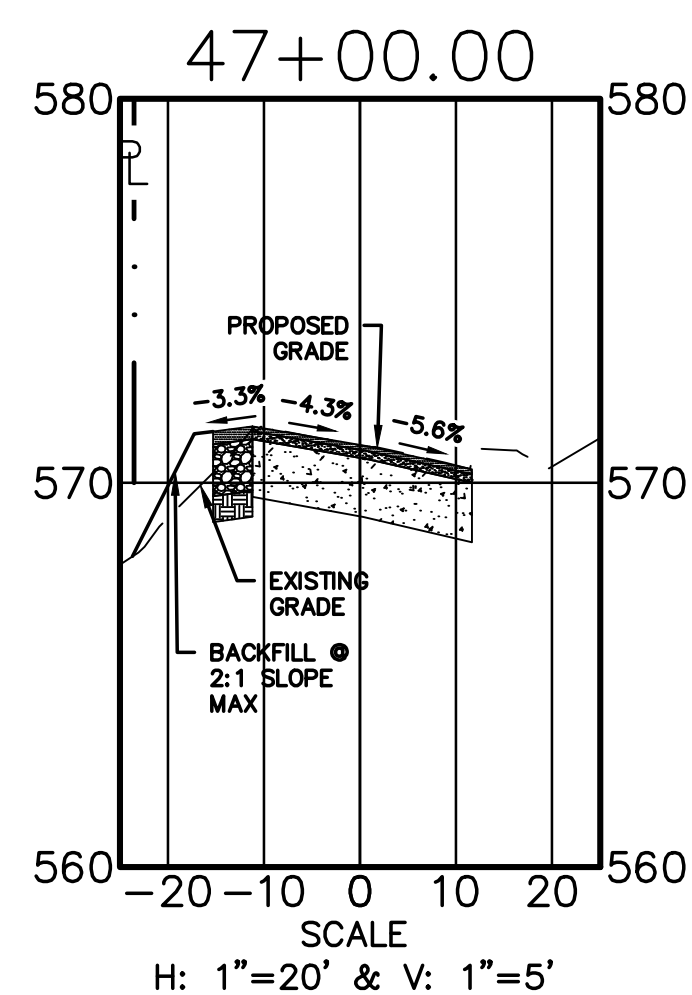
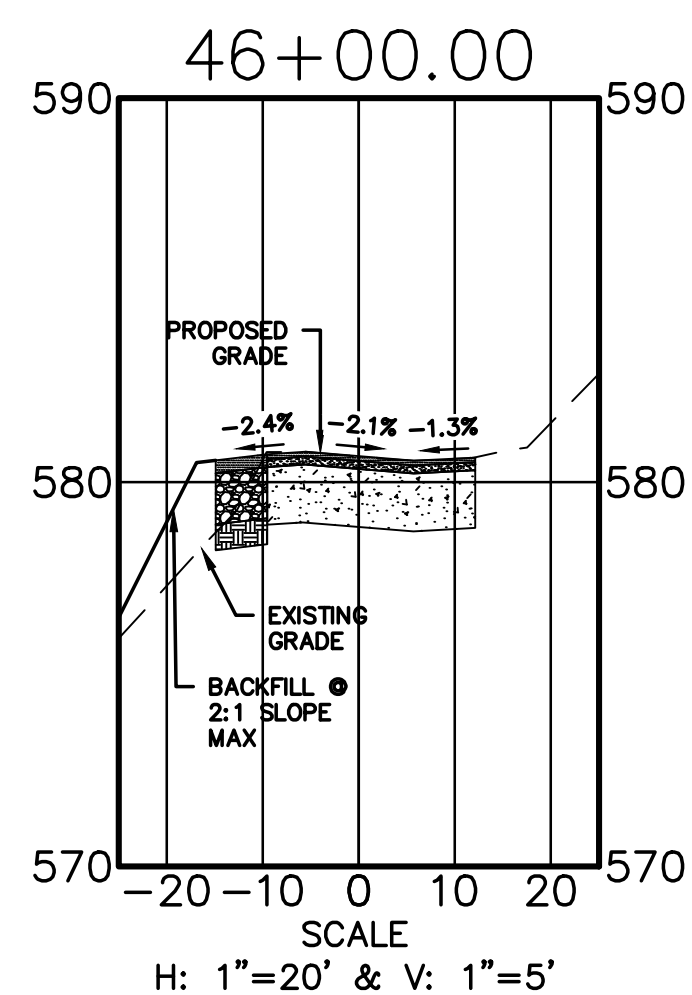
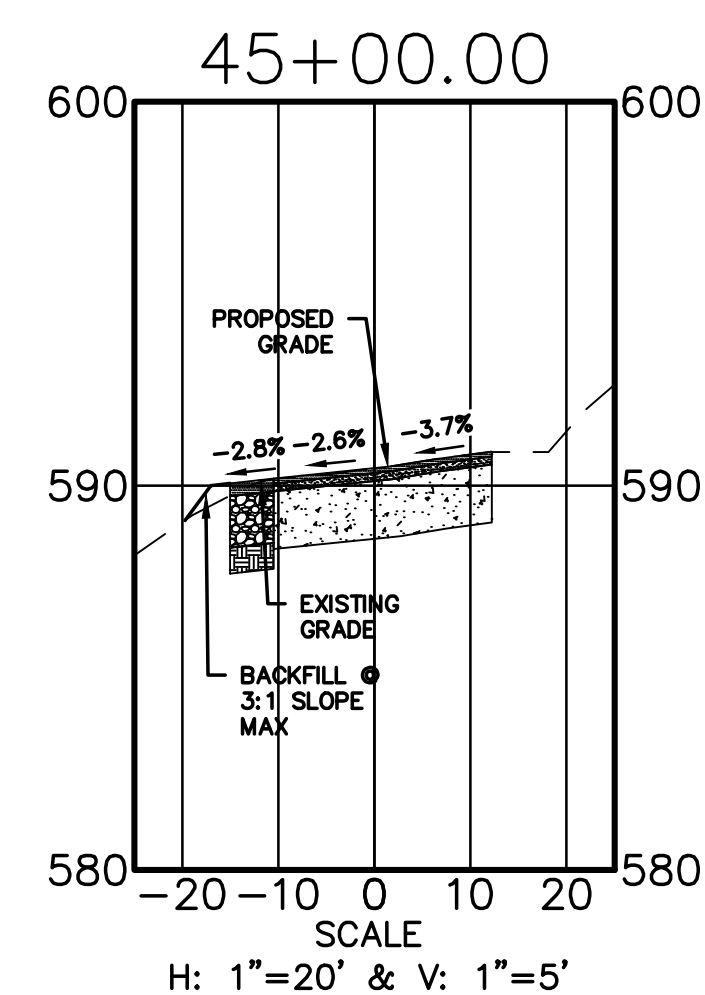
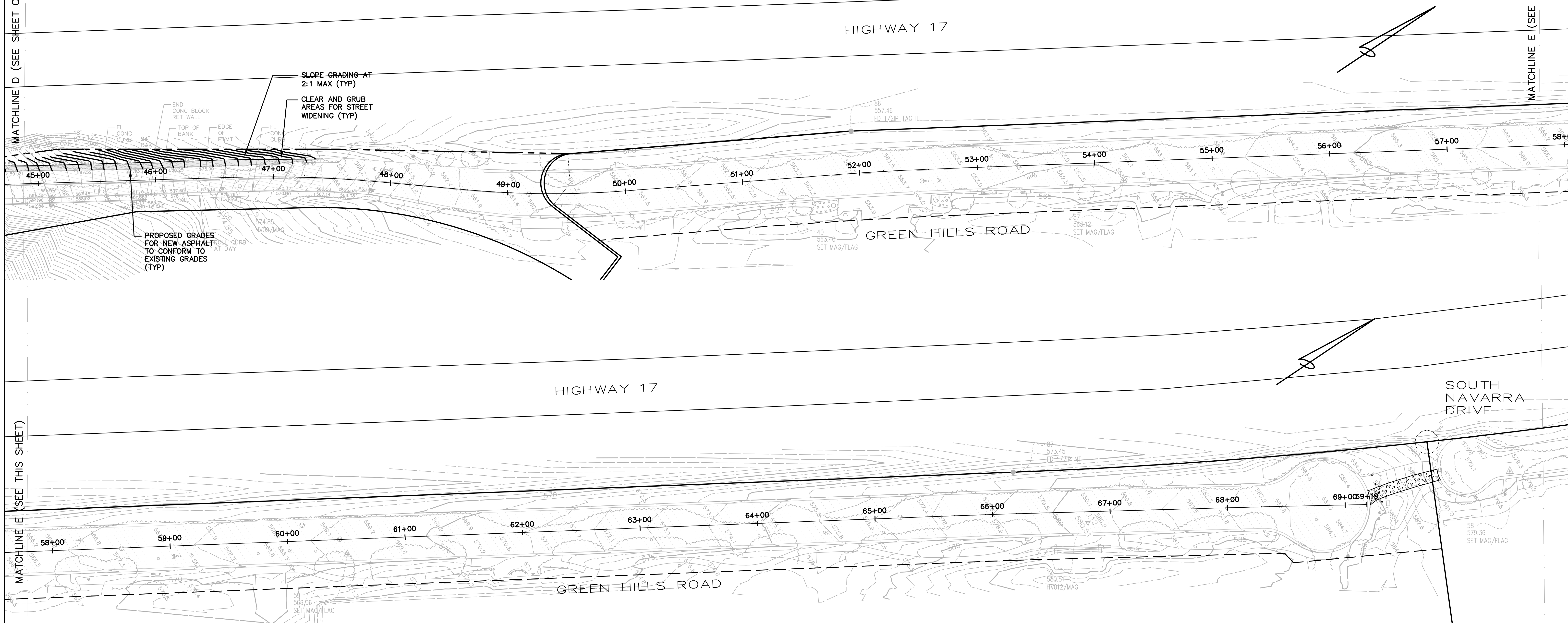




MATCHLINE D (SEE SHEET C2.2)

MATCHLINE E (SEE THIS SHEET)

MATCHLINE E (SEE THIS SHEET)



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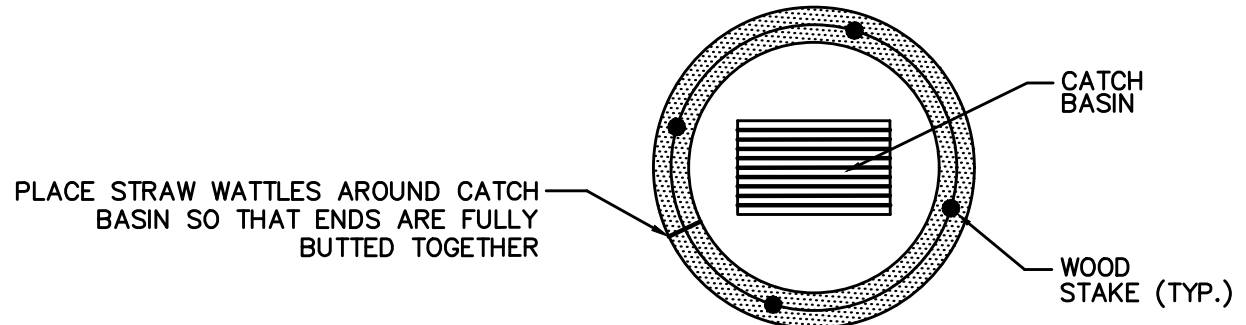
REVISION	12/8/17	GEOTECHNICAL INVESTIGATION ADDENDUM
APN		
BOWMAN & WILLIAMS CONSULTING CIVIL ENGINEERS AND LAND SURVEYORS 1011 CEDAR STREET SANTA CRUZ, CA 95060 (831) 426-3560 www.bowmanandwilliams.com		
GREEN HILLS ROAD GRADING AND SECTIONS STA 45+00 TO STA 69+18.95 BIKE LANES PROJECT CITY OF SCOTT'S VALLEY, DPW 701 LUNDY LANE, SCOTT'S VALLEY, CA 95066		
SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63
DATE SEPTEMBER 27, 2017	CHECKED JFR	INDEX
DESIGN KAB	DWG NAME 25471.63	FILE NO.
SHEET C2.3		
OF		



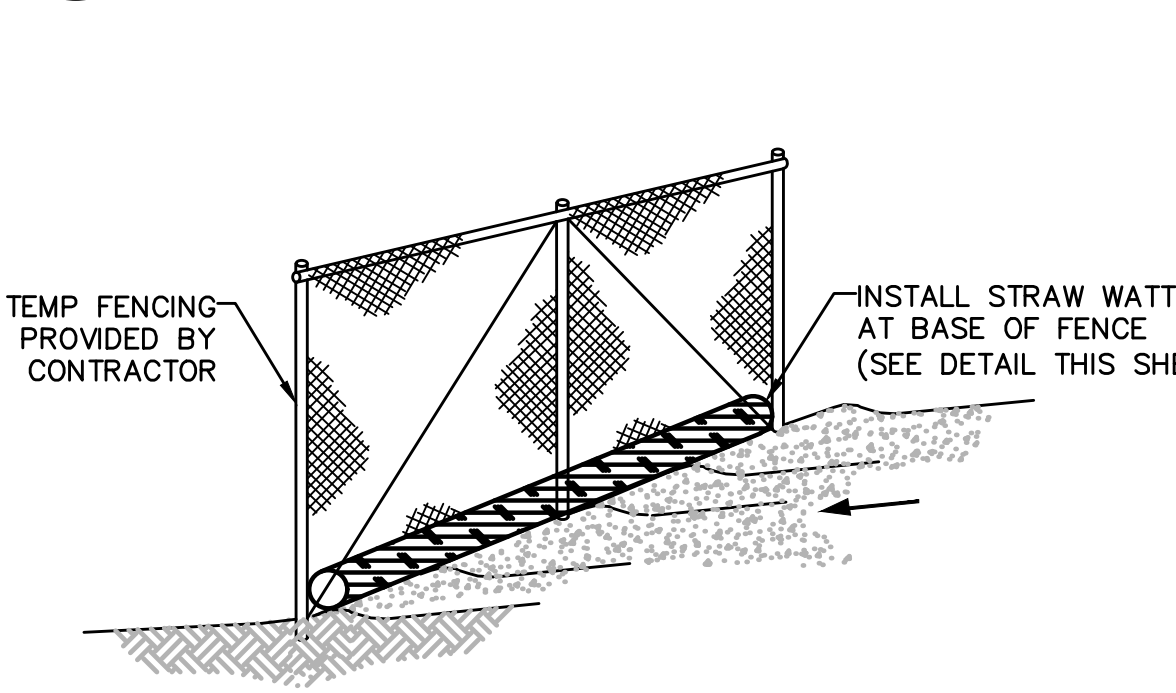


TEMPORARY EROSION CONTROL MEASURES

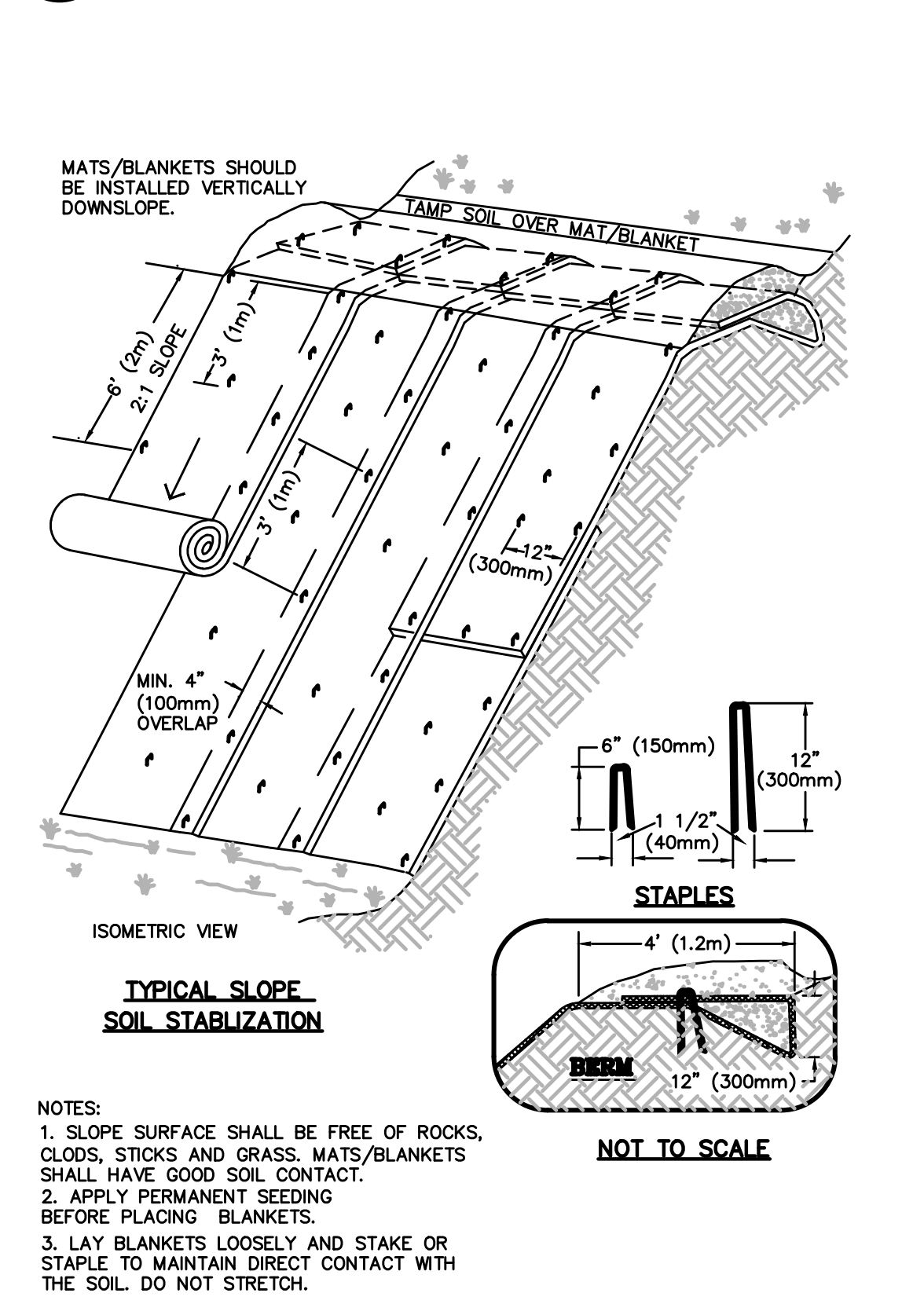
1. NO LAND CLEARING, GRADING, OR EXCAVATION SHALL BE DONE BETWEEN OCTOBER 15TH AND APRIL 15TH UNLESS ALLOWED BY THE CHIEF BUILDING OFFICIAL. THE DEVELOPER SHALL BE RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING SITE EROSION CONTROL AT ALL TIMES.
2. EROSION IS TO BE CONTROLLED AT ALL TIMES ALTHOUGH SPECIFIC MEASURES SHOWN ARE TO BE IMPLEMENTED AT A MINIMUM BETWEEN OCTOBER 15TH AND APRIL 15TH. BETWEEN OCTOBER 15TH AND APRIL 15TH HAY BALES, FILER BERMS, SILT FENCES OR OTHER MEANS SHALL BE EMPLOYED TO PREVENT SEDIMENT FROM LEAVING THE SITE OR ENTERING ANY WATERCOURSE.
3. RUNOFF FROM THE CONSTRUCTION SITE MUST NOT BE ALLOWED TO FLOW OVER ANY FILL SLOPES.
4. RUNOFF SHALL BE DIRECTED TOWARDS THE NEAREST CATCH BASIN OR SWALE WITH TEMPORARY EARTHEN BERMS.
5. SILT BARRIERS SHALL BE CONSTRUCTED ALONG THE BOTTOM OF ALL GRADED SLOPES.
6. CATCH BASINS SHALL BE ENCIRCLED BY A SILT BARRIER ON ALL FOUR SIDES. USE GRAVEL FILLED SANDBAGS OR STRAW BALE INLET BARRIER. PROVIDE FILTER FABRIC UNDER INLET GRATE.
7. ON ALL FINISH GRADED AREAS, ON- AND OFF-SITE, EXPOSED DURING CONSTRUCTION ACTIVITIES, IF NOT PERMANENTLY LANDSCAPED PER PLAN, SHALL BE PROTECTED BY MULCHING AND/OR PLANTING OF THE SCS APPROVED EROSION CONTROL MIX AT A RATE OF 35 LBS PER ACRE.
- ONCE SLOPES ARE SEEDED AND RAKED, COVER THEM WITH TWO INCHES (2") OF STRAW. IF AVAILABLE, USE NATIVE BUNCH GRASS STRAW (PREFERABLY BROMUS CARINATUS); IF NOT AVAILABLE, USE COMMERCIALY AVAILABLE TYPES (E.G. WHEAT, OAT, BARLEY, ETC.). EXPOSED SOIL ON SLOPES GREATER THAN 20 % SHALL BE SEEDED, COVERED WITH 2 INCHES OF STRAW, AND A NORTH AMERICAN GREEN SC150 EROSION CONTROL BLANKET, OR APPROVED EQUAL. THE EROSION CONTROL BLANKET SHALL BE STAKED IN PLACE.
- FERTILIZER REQUIRED FOR THIS SEED MIX IS AMMONIUM PHOSPHATE AT AN APPLICATION RATE OF 250 LB PER ACRE.
- SEEDING MAY BE OMITTED FOR AREAS THAT ARE TO RECEIVE PERMANENT LANDSCAPING PROVIDED SAID LANDSCAPING IS IN PLACE PRIOR TO OCTOBER 15TH.
8. UNNECESSARY GRADING AND DISTURBING OF SOIL SHALL BE AVOIDED.
9. DURING CONSTRUCTION, NO TURBID WATER SHALL BE PERMITTED TO ENTER THE CHANNEL OR STORM DRAIN SYSTEM. USE OF SILT AND GREASE TRAPS, FILTER BERMS, HAY BALES OR SILT FENCES SHALL BE USED TO PREVENT SUCH DISCHARGE.
10. ALL EXCAVATED MATERIAL NOT SUITABLE FOR FILL OR REUSE SHALL BE REMOVED TO AN APPROVED DISPOSAL SITE OR DISPOSED OF ON-SITE IN A MANNER THAT WILL NOT CAUSE EROSION.
11. ANY MATERIAL STOCKPILED ON-SITE SHALL BE COVERED WITH PLASTIC, ESPECIALLY DURING THE WINTER MONTHS OR DURING PERIODS OF RAIN.
12. IT IS THE DEVELOPER'S RESPONSIBILITY TO SEE THAT ADDITIONAL MEASURES, NECESSARY TO CONTROL SITE EROSION AND PREVENT SEDIMENT TRANSPORT OFF-SITE ARE IMPLEMENTED.
13. DUST CONTROL MEASURES CONSISTENT WITH MBUAPCD SHALL BE IMPLEMENTED PER PROJECT CONDITIONS OF APPROVAL ITEM NO. 20.
14. LIMIT GROUND DISTURBANCE AND VEGETATION REMOVAL AT ANY ONE TIME DURING CONSTRUCTION AND INSTALLATION OF DRAINAGE IMPROVEMENTS.
15. CONDUCT GRADING WORK PRIOR TO THE RAINY SEASON UNLESS OTHERWISE PERMITTED BY AND IN ACCORDANCE WITH THE CITY'S GRADING ORDINANCE. EXCEPT FOR INSTALLATION OF THE STORM DRAIN OUTLETS AS ADDRESSED BELOW, PROTECT DISTURBED AREAS DURING THE RAINY SEASON; AND CONTAIN AND/OR PROPERLY DE-WATER ACCUMULATED CONSTRUCTION-RELATED RUNOFF FROM DISTURBED AREAS OR EXCAVATED AREAS.
16. IMMEDIATELY REVEGETATE DISTURBED AREAS. APPLY WEED-FREE MULCH OR REVEGETATE ALL SOIL EXPOSED AS A RESULT OF THE PROPOSED GRADING BEFORE OCTOBER 15TH OF EACH YEAR.
17. INSPECT AND REPAIR/MAINTAIN ALL EROSION CONTROL MEASURES PRIOR TO AND AFTER ANY RAINFALL EVENT EXCEEDING 0.5 INCH, AT 24-HOUR INTERVALS DURING EXTENDED STORM EVENTS, AND A MINIMUM OF EVERY TWO WEEKS AFTER THE WINTER STORM SEASON.



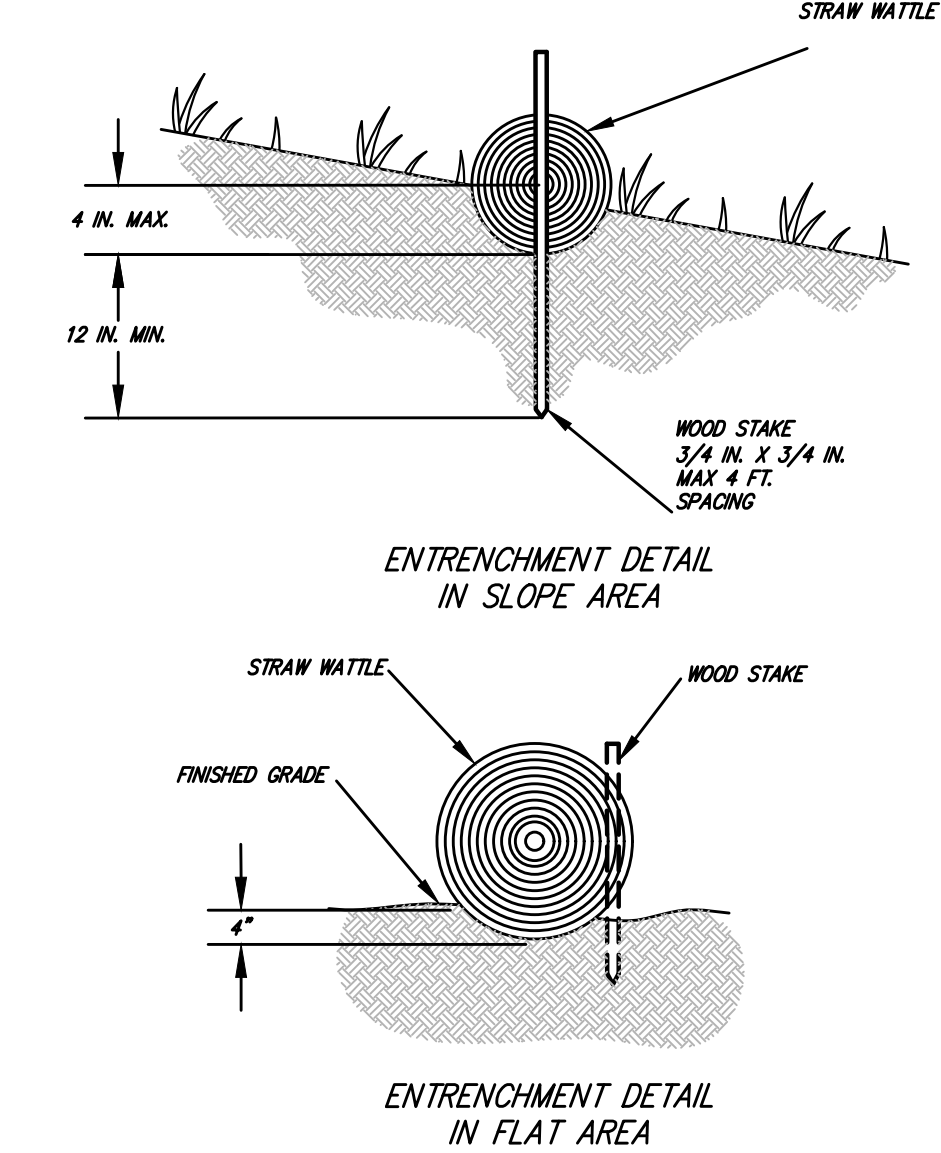
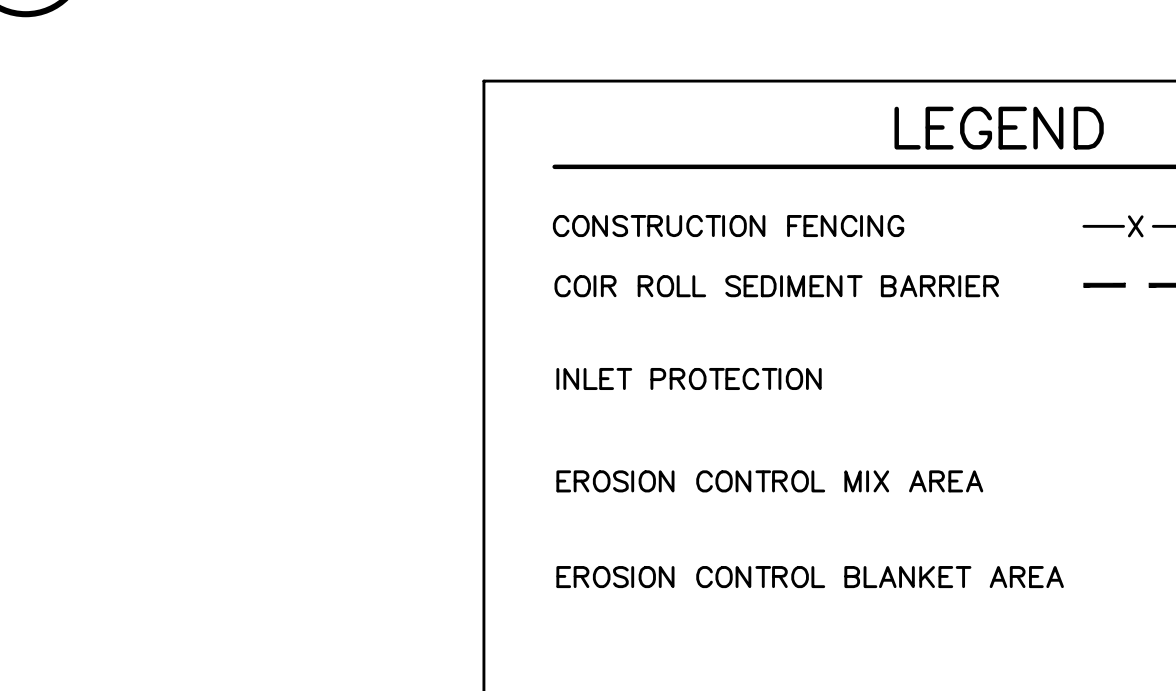
2 STRAW WATTLE INLET PROTECTION



3 TEMPORARY FENCE WITH SEDIMENT AND DEBRIS BARRIER

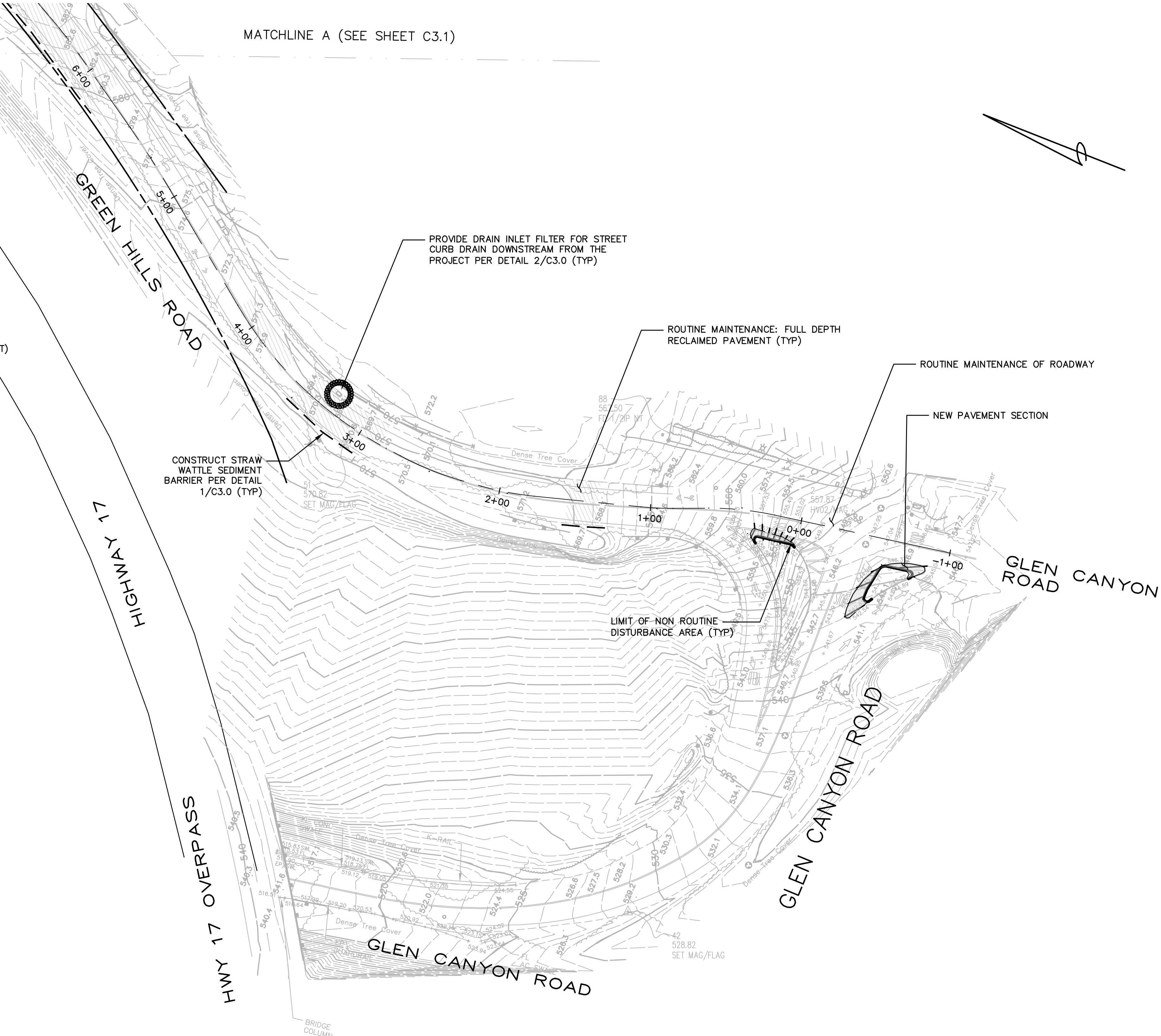


4 EROSION CONTROL BLANKETS

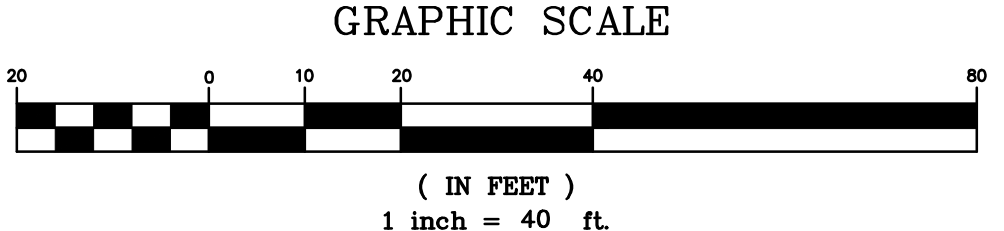


1 STRAW WATTLE SEDIMENT BARRIER

MATCHLINE A (SEE SHEET C3.1)

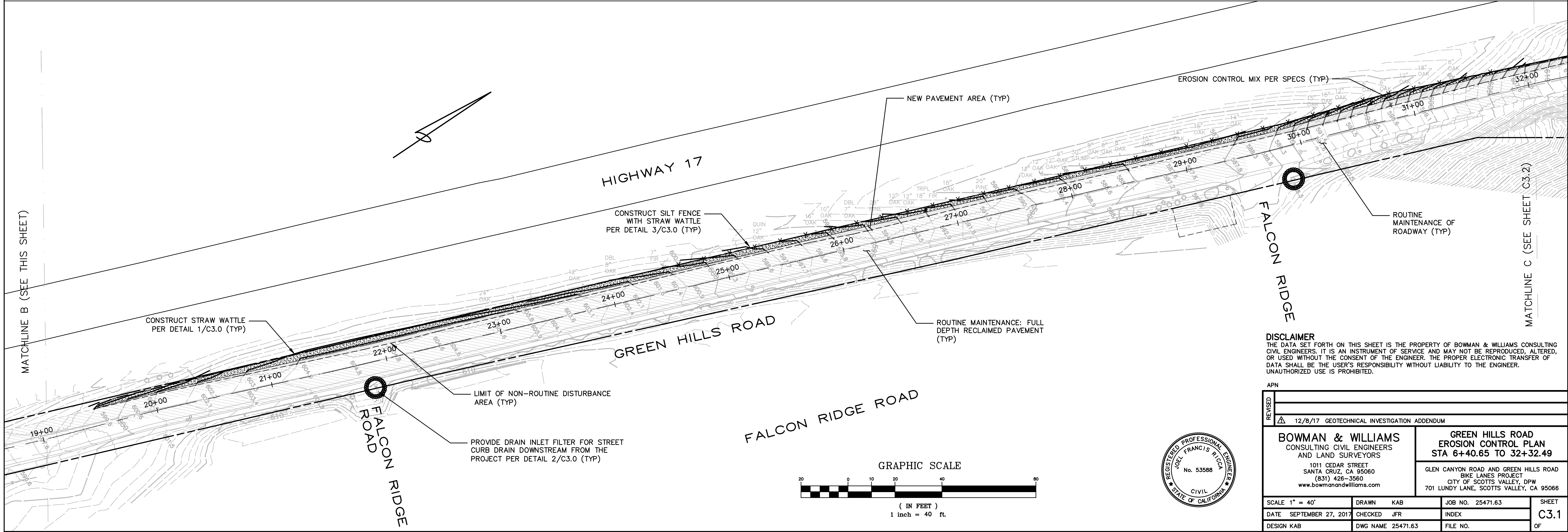
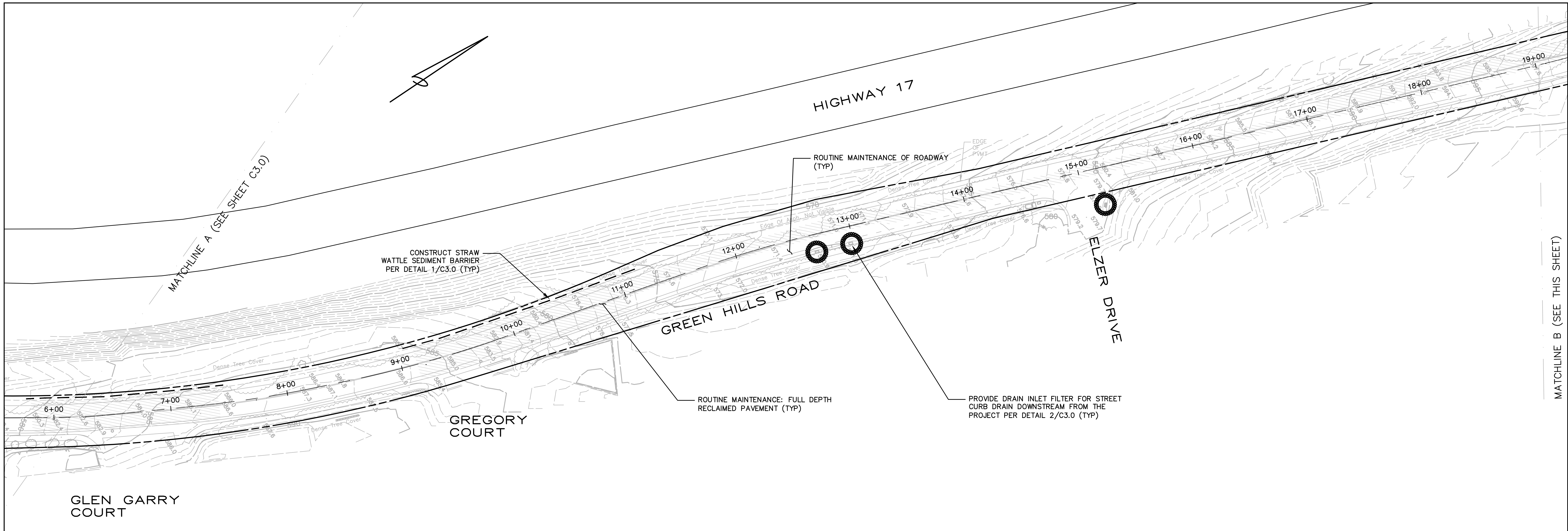


TOTAL AREA OF NON-ROUTINE DISTURBANCE = 20,659 SF (0.47 AC)



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APN			
REVISION			
12/8/17 GEOTECHNICAL INVESTIGATION ADDENDUM			
BOWMAN & WILLIAMS		GLEN CANYON AND GREEN HILLS	
CONSULTING CIVIL ENGINEERS		EROSION CONTROL PLAN	
AND LAND SURVEYORS		STA 0+00 TO STA 6+40.65	
1011 CEDAR STREET		BIKE LANES PROJECT	
SANTA CRUZ, CA 95060		CITY OF SCOTT'S VALLEY, DPW	
(831) 426-3560		701 LUNDY LANE, SCOTT'S VALLEY, CA 95066	
www.bowmanandwilliams.com			
SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63	SHEET
DATE SEPTEMBER 27, 2017	CHECKED JFR	INDEX	C3.0
DESIGN KAB	DWG NAME 25471.63	FILE NO.	OF

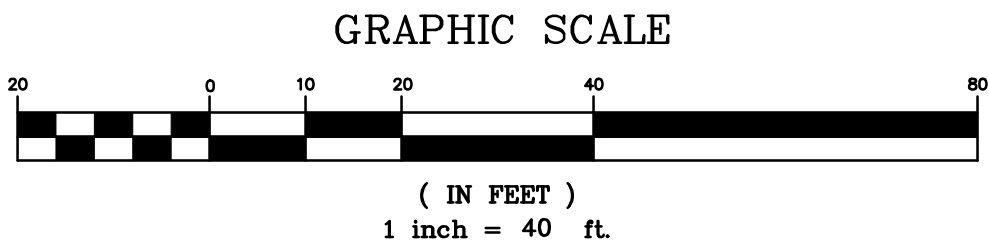




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SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63	SHEET
DATE SEPTEMBER 27, 2017	CHECKED JFR	INDEX	C3.1
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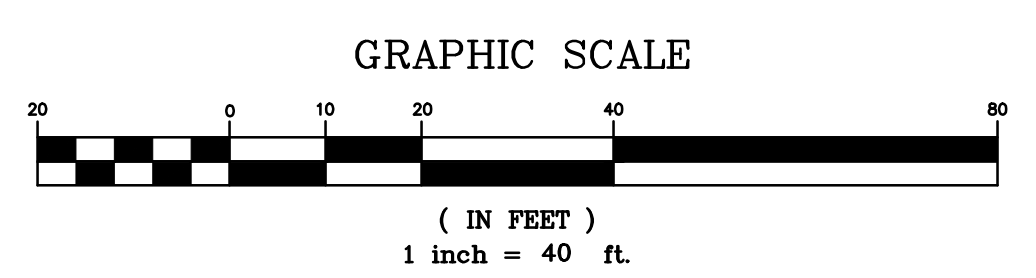
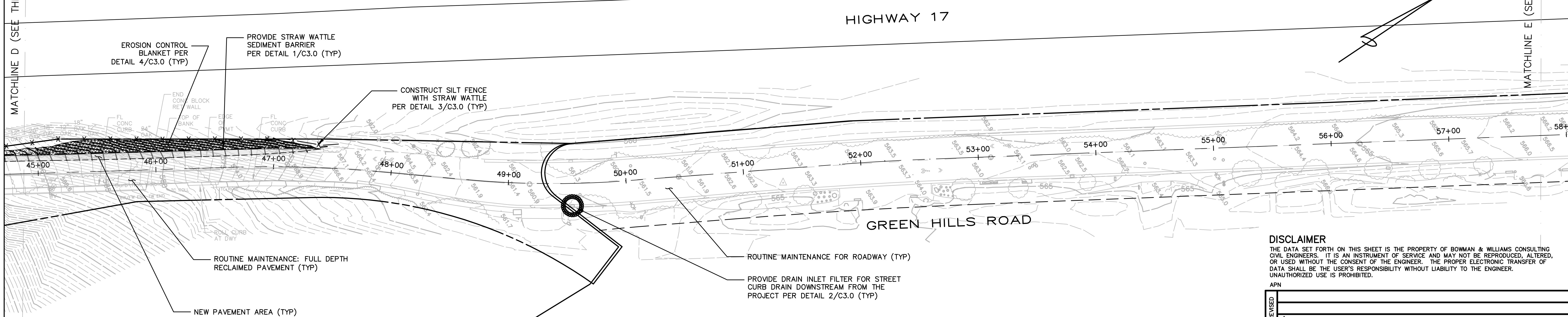
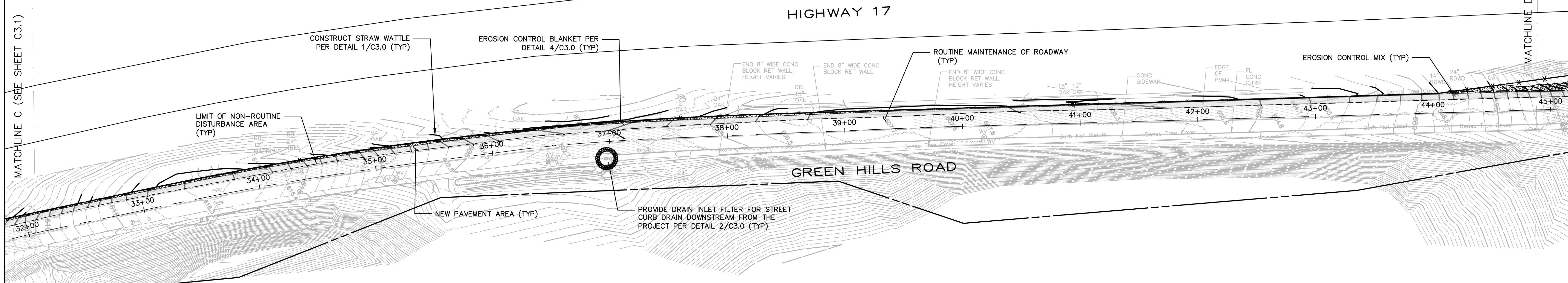


MATCHLINE C (SEE SHEET C3.1)

MATCHLINE D (SEE THIS SHEET)

MATCHLINE D (SEE THIS SHEET)

MATCHLINE E (SEE SHEET C3.3)



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REVISION	12/8/17	GEOTECHNICAL INVESTIGATION ADDENDUM
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SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63	SHEET
DATE SEPTEMBER 27, 2017	CHECKED JFR	INDEX	C3.2
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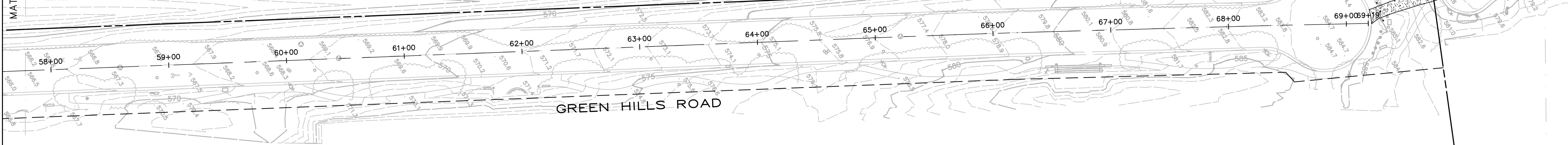


MATCHLINE E (SEE SHEET C3.2)

HIGHWAY 17

SOUTH  
NAVARRA  
DRIVE

GREEN HILLS ROAD



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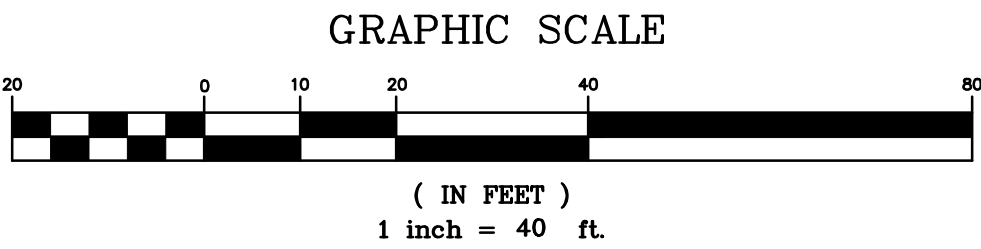
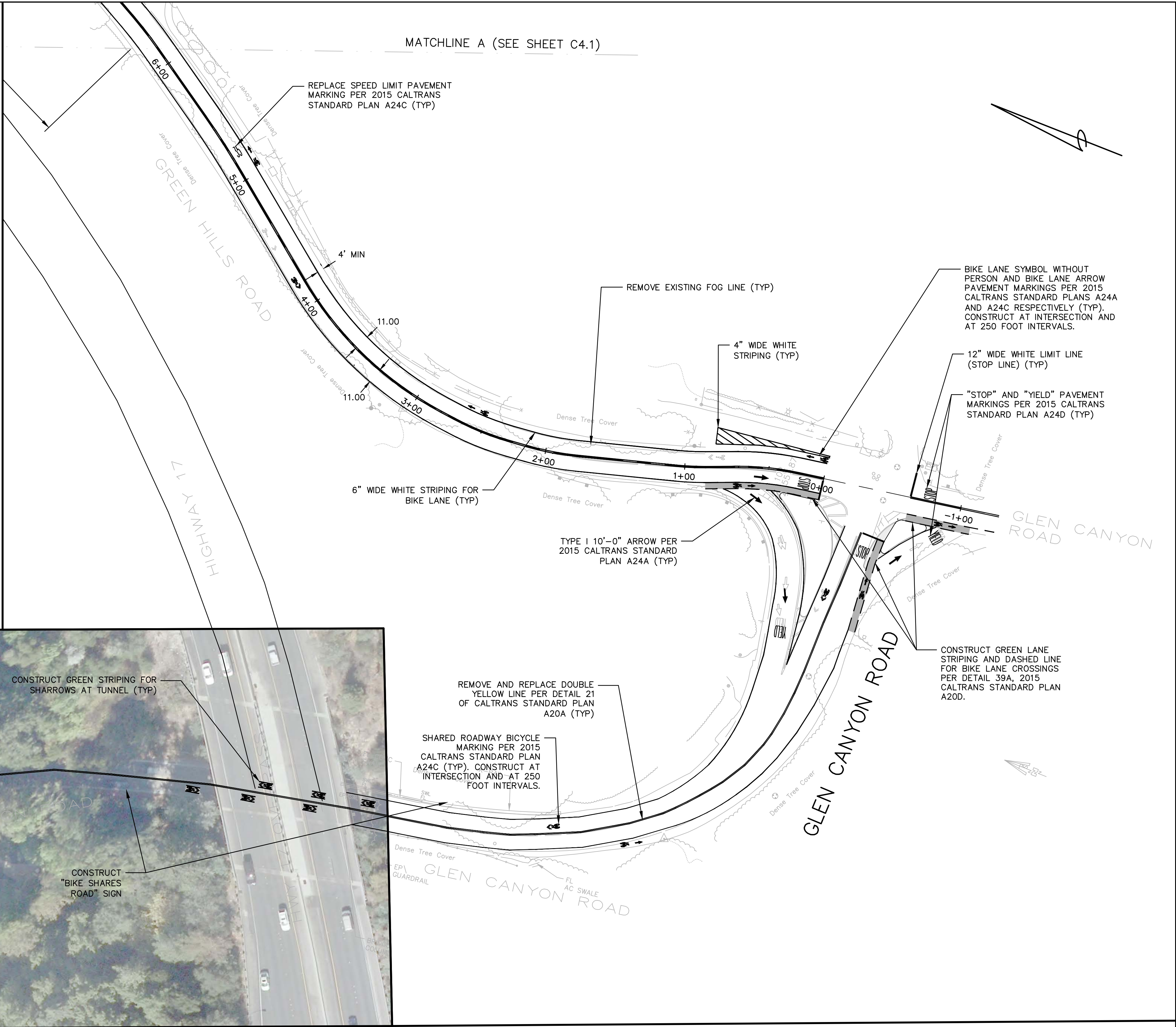
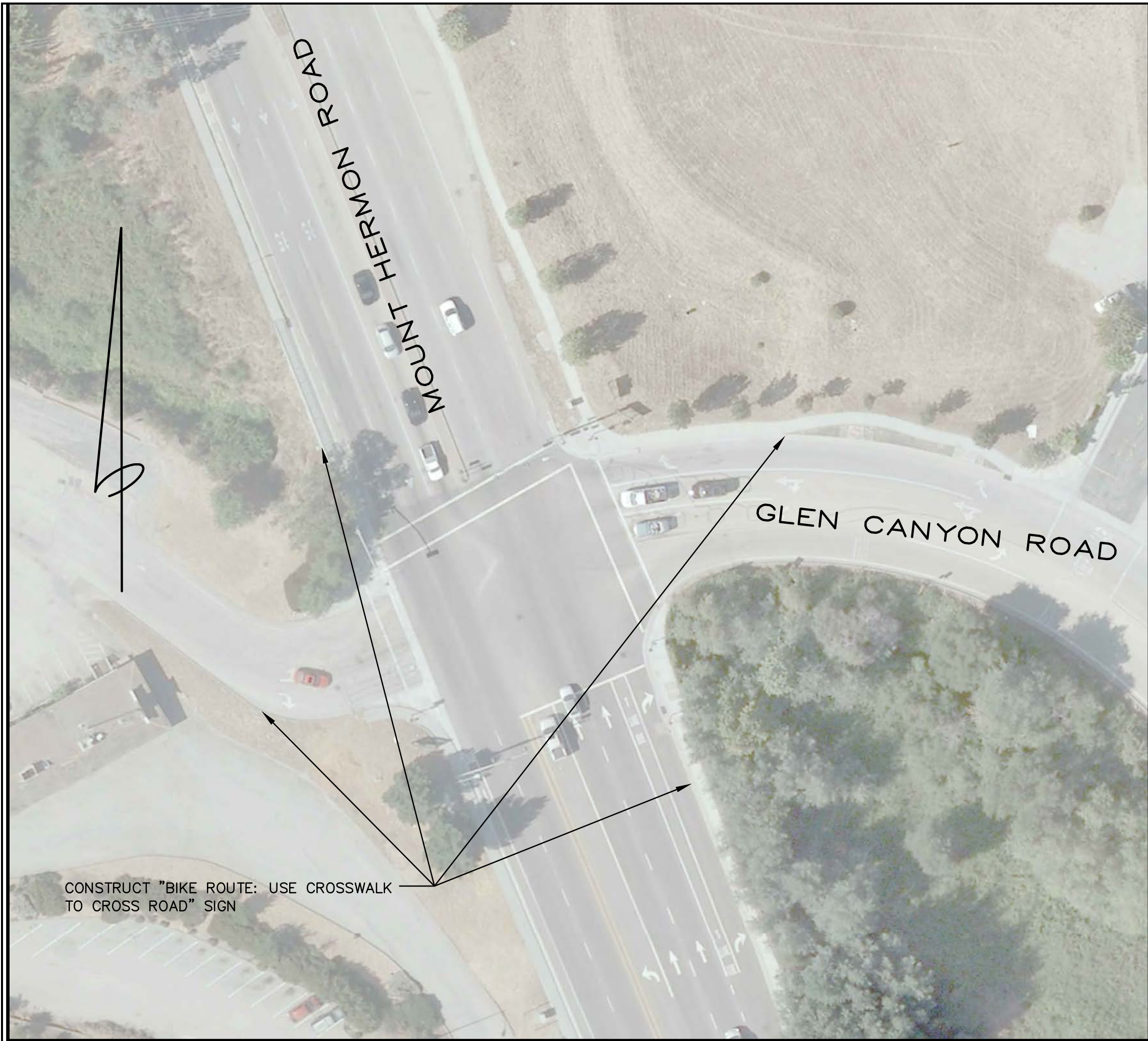
GRAPHIC SCALE



( IN FEET )  
1 inch = 40 ft.



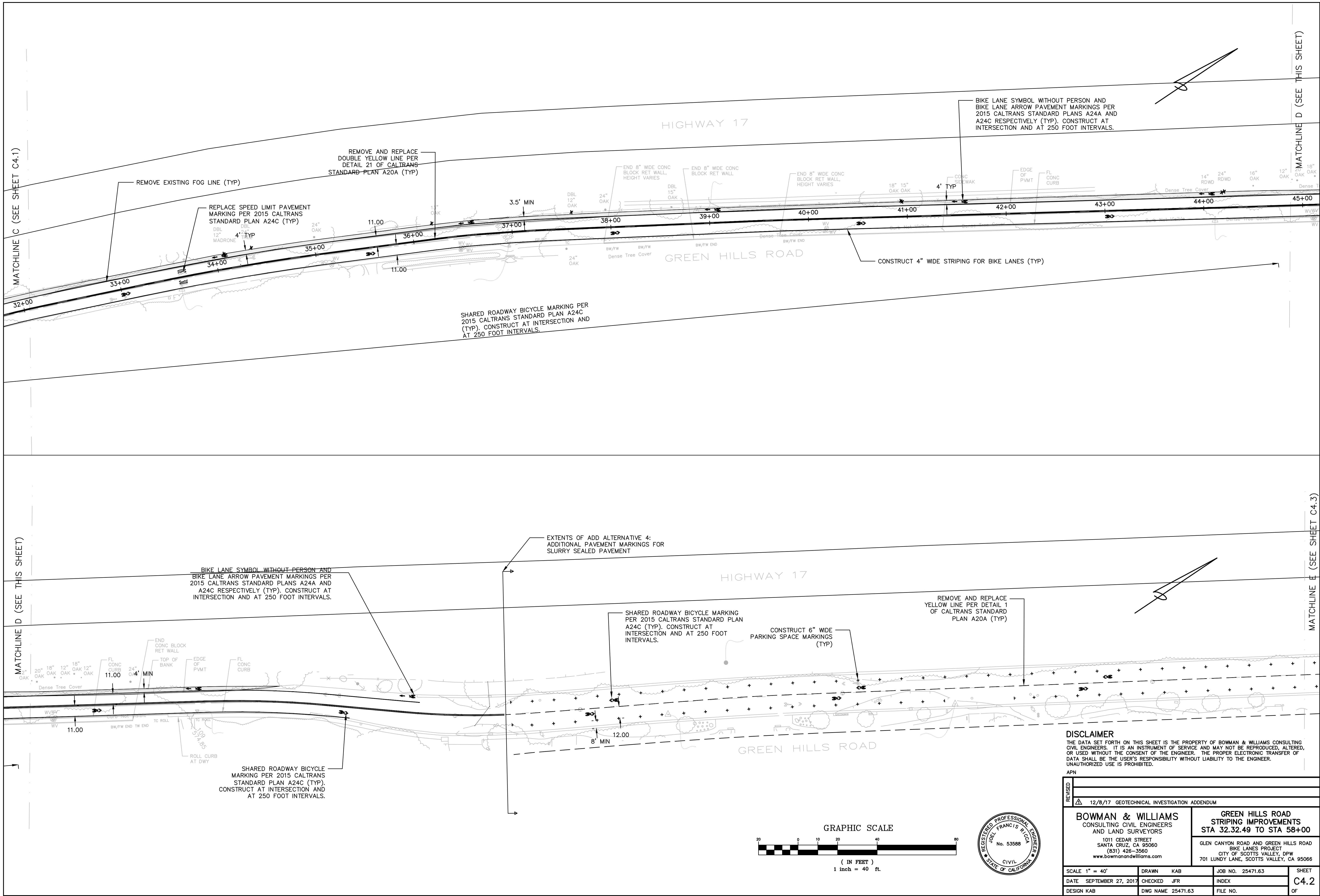




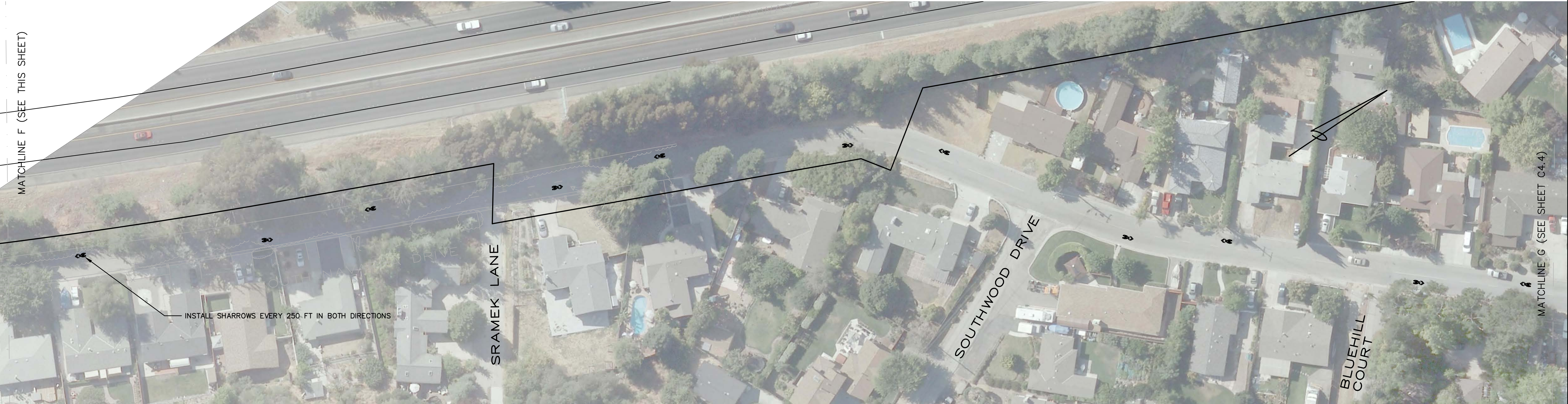
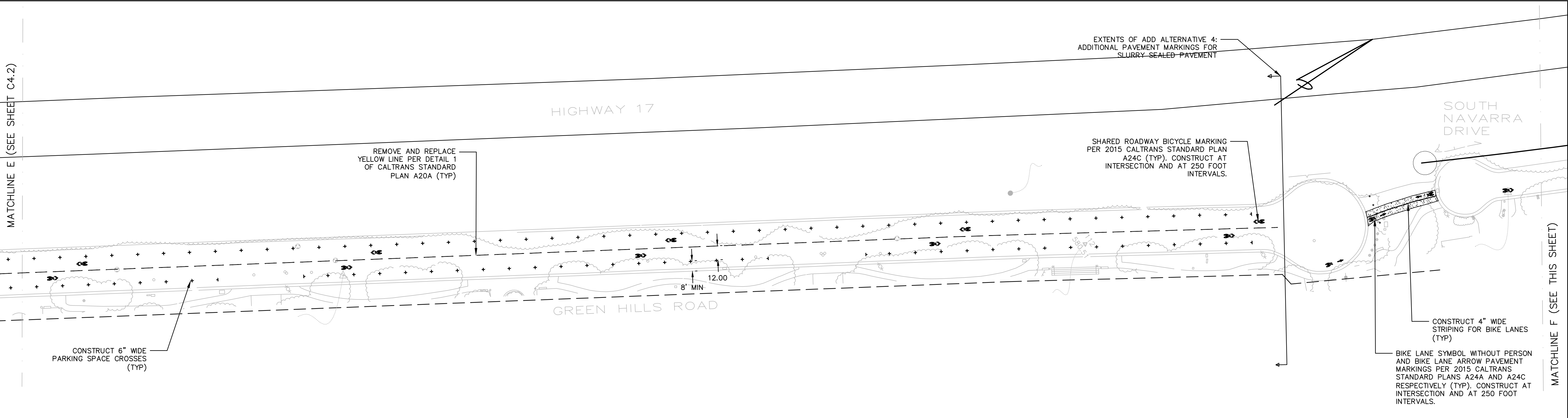
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SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63	SHEET
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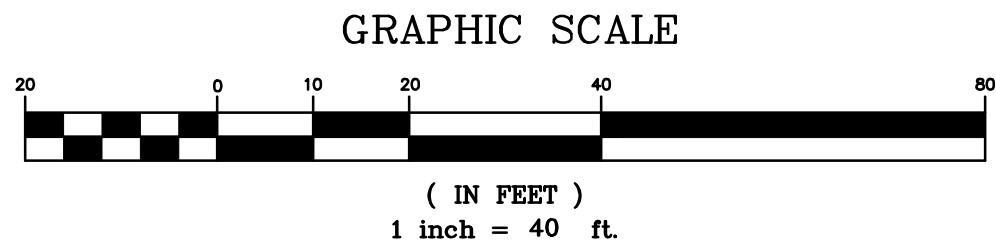




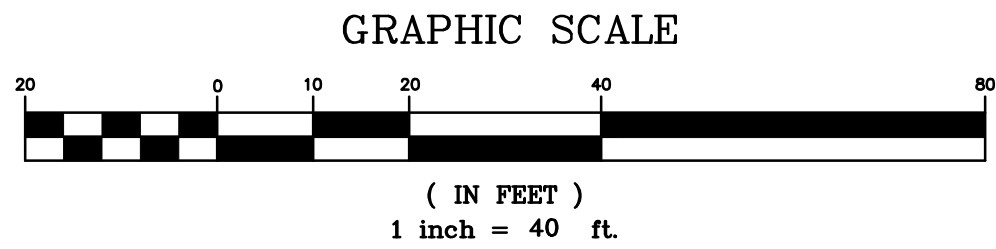
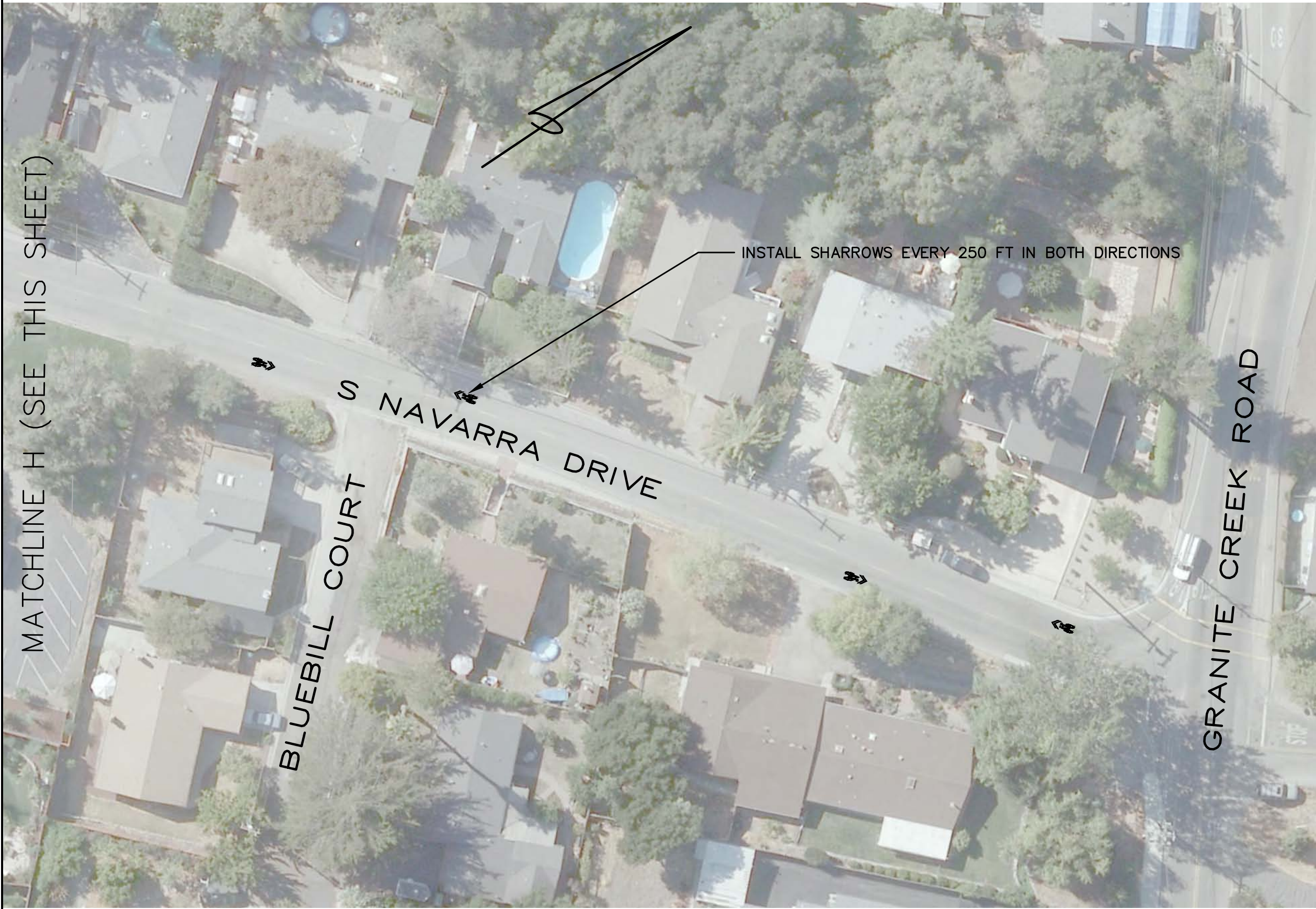
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REVISION	12/8/17	GEOTECHNICAL INVESTIGATION ADDENDUM
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<b>GREEN HILLS ROAD STRIPING IMPROVEMENTS STA 58+00 TO STA 69+18.95 AND S. NAVARRA DRIVE PAVEMENT MARKINGS</b>		
GLEN CANYON ROAD AND GREEN HILLS ROAD BIKE LANES PROJECT CITY OF SCOTT'S VALLEY, DPW 701 LUNDY LANE, SCOTT'S VALLEY, CA 95066		
SCALE 1" = 40'	DRAWN KAB	JOB NO. 25471.63
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12/8/17 GEOTECHNICAL INVESTIGATION ADDENDUM			
BOWMAN & WILLIAMS CONSULTING CIVIL ENGINEERS AND LAND SURVEYORS 1011 CEDAR STREET SANTA CRUZ, CA 95060 (831) 426-3560 www.bowmanandwilliams.com		S. NAVARRA DRIVE PAVEMENT MARKINGS	
		BIKE LANES PROJECT GLEN CANYON ROAD AND GREEN HILLS ROAD CITY OF SCOTT'S VALLEY, DPW 701 LUNDY LANE, SCOTT'S VALLEY, CA 95066	
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**TO:** Bicycle Advisory Committee

**FROM:** Anais Schenk, Transportation Planner

**RE:** Bicycle Advisory Committee Pending New Members Nominations

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## **RECOMMENDATIONS**

Staff recommends that the Bicycle Advisory Committee:

1. Receive the following report regarding nominations made by the Commissioners for existing Committee vacancies; and
  2. Recommend to the Regional Transportation Commission (RTC) approval of member nominations as follows:
    - a. Theresia Rogerson for the District 5 alternate seat;
    - b. Kelly Bond for the City of Santa Cruz alternate seat;
    - c. Michael Moore for the City of Capitola primary seat pending Capitola City Council approval;
    - d. Richard Masoner for the City of Scotts Valley primary seat pending nomination from Commissioner Johnson; and
    - e. Jo Fleming for the City of Scotts Valley alternate seat pending nomination from Commissioner Johnson.
- 

## **BACKGROUND**

Representation on the RTC's Bicycle Advisory Committee (Committee) corresponds to City and Supervisorial District seats on the RTC. Each Commissioner nominates individuals to represent their respective jurisdiction for Committee and RTC consideration. Two additional seats for Bike to Work and the Community Traffic Safety Coalition also exist. Appointments are made by the respective agency and accepted by the RTC. Seats for three-year terms on the Bicycle Committee expire on a rotating basis. In March 2018, new appointments were made for District 2 and reappointments were made for the City of Santa Cruz and Community Traffic Safety Coalition primary seats.

The Bicycle Advisory Committee's description, role and membership are in the 2017 RTC Rules and Regulations available on the RTC website: <https://sccrtc.org/meetings/bike-committee/>.

## **DISCUSSION**

Member seats on the Committee expired or became vacant in March 2018 for the City of Santa Cruz, Districts 2 and 4, and the Community Traffic Safety Coalition. The nominations as described below for these seats were approved by the Commission at its meeting on April 5, 2018:

- District 2: Shea Johnson was appointed as the voting member and Casey Beyer as the alternate member.
- City of Santa Cruz: Amelia Conlen was reappointed.
- Community Traffic Safety Coalition: Leo Jed was reappointed as the voting member and Jim Langelly as the alternate member.

The following seats on the Committee remain vacant:

- District 4: primary and alternate
- District 5: alternate
- City of Capitola: primary and alternate
- City of Santa Cruz: alternate
- City of Scotts Valley: primary and alternate
- City of Watsonville: alternate

Staff began an open recruitment process to fill these vacancies in March 2018. The recruitment process has consisted of advertising on social media, our website, on the radio and at outreach events. Due to the large number of vacancies, appointments will be made on a rolling basis. Applications were received for District 5, the City of Capitola, the City of Santa Cruz, and the City of Scotts Valley. Interested applicants were forwarded to the Commissioners and nominations were made as described below. Attachment 1 includes the applications for pending member nominations.

**District 5** – Commissioner Bruce McPherson nominated Theresia Rogerson for the alternate seat representing District 5. Ms. Rogerson is the staff lead for the Community Traffic Safety Coalition and has experience serving on an advisory committee and working with the RTC.

**City of Capitola** – The City of Capitola requires nominations to be approved by the City Council before finalizing a nomination. Commissioner Bertrand has asked that we proceed with the approval process for nominating Michael Moore. Mr. Moore has previously served on the Committee. He is also currently the General Manager for Bicycle Trip, Capitola. His application will be considered by the City Council at their meeting on June 28<sup>th</sup>.

**City of Santa Cruz** – Commissioner Brown nominated Kelly Bond for the alternate seat representing the City of Santa Cruz. Ms. Bond regularly volunteers for active transportation causes and has experience working on a board.

**City of Scotts Valley** – Two applications were received for the City of Scotts Valley. Richard Masoner has experience with the Silicon Valley Bicycle Coalition. Jo Fleming started the Scotts Valley Active Transportation Group. Both applicants are very familiar with current City of Scotts Valley projects and programs. Staff has reached out to Commissioner Johnson regarding nomination of these applicants but has not received a response.

## SUMMARY

There are a number of vacancies on the RTC Bicycle Advisory Committee. Vacancies affect the ability of the Committee to provide equal geographic representation so that all the jurisdictions have an opportunity to weigh in on projects and programs that affect cyclists in Santa Cruz



County. Applications have been received and nominations have been made for District 5 and the City of Santa Cruz. One nomination is also pending for the City of Capitola following approval of the City Council. The City of Scotts Valley may also have two nominees by the August RTC meeting. Staff recommends the Committee consider these nominations and recommend to the RTC appointment of Theresia Rogerson, Kelly Bond, Mike Moore pending Capitola City Council approval as well as Richard Masoner and Jo Fleming pending nomination from Commissioner Johnson.

Attachments:

1. Applications for pending Committee member nominations

# COMMITTEE APPOINTMENT APPLICATION

## Santa Cruz County Regional Transportation Commission (SCRTC) Bicycle Advisory Committee

The Bicycle Advisory Committee consists of eleven seats: one for each city within Santa Cruz County, one for each of the county districts, one representing the Bike to Work program and one representing the Community Traffic Safety Coalition. Each of the seats has an alternate member appointed in the case that the primary member is unable to attend a meeting.

Meetings are currently held the second Monday of even numbered months at 6:00 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1523 Pacific Avenue in downtown Santa Cruz. Please refer to the Committee description and bylaws for more information: <https://sccrtc.org/meetings/bike-committee/>.

If you are interested in serving on this committee, we recommend reviewing the by-laws and attending a meeting to familiarize yourself with the committee process. After doing so please complete this application and email a scanned signed copy to [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org) or return a signed copy to the RTC office.

Name: Theresia Rogerson  
 Home address: [REDACTED]  
 Mailing address (if different): [REDACTED]  
 Phone: (home) [REDACTED] (business/message/mobile) [REDACTED]  
 E-mail: [REDACTED]  
 Length of residence in Santa Cruz County: 12 years

I am applying to represent the following vacant position (circle one):

City of Capitola - Primary	City of Watsonville - Alternate
City of Capitola - Alternate	County District 4 - Primary
City of Scotts Valley - Primary	County District 4 - Alternate
City of Scotts Valley - Alternate	<u>County District 5 - Alternate</u>
City of Santa Cruz - Alternate	

I would consider a different position - either a different location or alternate versus primary (circle one) Yes No

**Previous experience on a government commission or committee:** (Please describe the committee/commission's purpose and your role.)

I helped form the first Bicycle and Pedestrian Advisory Committee for the City of Fresno. We worked with City staff to produce the first Bicycle Transportation Plan. This allowed the City to apply for Caltrans funding for bicycle projects.

**Other Relevant Work or Volunteer Experience**

Organization	Location	Position	Dates
County of Santa Cruz	Santa Cruz	Health Educator in safe & active transportation	2005 - current
Fresno County Bicycle Coalition	Fresno	Founding member & chair	2000 - 2005

**Statement of Qualifications:** Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

**Certification:** I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the



5/16/18  
Date

**Return Application to:**

SCCRTC Attn: Anais Schenk  
1523 Pacific Avenue  
Santa Cruz, CA 95060  
fax: (831) 460-3215 or email: [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org)

**Questions or Comments:**

Contact Anais Schenk at (831) 460-3209 or by email at [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org).

**Committee Appointment Application**  
**SCCRTC Bicycle Advisory Committee**

**Statement of Qualifications:**

My interest in serving on the BAC comes from my work in safe and active transportation at the County and from a desire to use my bicycle with my son to commute from our home in Ben Lomond to the SLVE campus and into Santa Cruz. As staff for the Community Traffic Safety Coalition (CTSC), I have worked with the RTC, their BAC, and its members on a variety of traffic safety programs. Our CTSC and Ride n' Stride programs receive TDA funding through RTC, so I have attended both BAC and other RTC meetings over the years. CTSC is also represented on the BAC and two of our members serve on the committee and share information between the two groups. As a resident for the past 7 years of the San Lorenzo Valley, I am also very pleased to see the Highway 9 corridor planning process unfolding. This is an area of our County in great need of complete streets.

# COMMITTEE APPOINTMENT APPLICATION

## Santa Cruz County Regional Transportation Commission (SCCRTC) Bicycle Advisory Committee

The Bicycle Advisory Committee consists of eleven seats: one for each city within Santa Cruz County, one for each of the county districts, one representing the Bike to Work program and one representing the Community Traffic Safety Coalition. Each of the seats has an alternate member appointed in the case that the primary member is unable to attend a meeting.

Meetings are currently held the second Monday of even numbered months at 6:00 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1523 Pacific Avenue in downtown Santa Cruz. Please refer to the Committee description and bylaws for more information: <https://sccrtc.org/meetings/bike-committee/>.

If you are interested in serving on this committee, we recommend reviewing the by-laws and attending a meeting to familiarize yourself with the committee process. After doing so please complete this application and email a scanned signed copy to [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org) or return a signed copy to the RTC office.

Name: Kelly Bond

Home address: [REDACTED]

Mailing address (if different):

Phone: (home) [REDACTED] (business/message/mobile)

E-mail: [REDACTED]

Length of residence in Santa Cruz County: Since October 2015 (about 2.5 years)

I am applying to represent the following vacant position (circle one):

City of Capitola - Primary

City of Watsonville - Alternate

City of Capitola - Alternate

County District 4 - Primary

City of Scotts Valley - Primary

County District 4 - Alternate

City of Scotts Valley - Alternate

County District 5 - Alternate

City of Santa Cruz - Alternate

I would consider a different position - either a different location or alternate versus primary (circle one): Yes / No

**Previous experience on a government commission or committee:** (Please describe the committee/commission's purpose and your role.)

While I have not previously been involved on a government commission/committee, I have been involved in committees through my work at the Tulane Prevention Research Center. Committees focused on a five year community health research project. I was a board member of Slow Food Santa Cruz, involved in chapter funding and leading events. Currently I am a member of a committee at my job (CCOF). The committee's purpose is to lead staff appreciation events, including hosting two major company-wide gatherings each year. As a member I work with budgets, staff input, and more, which I feel would all be applicable to the RTC's Bike Advisory Committee.

**Other Relevant Work or Volunteer Experience**

Organization	Location	Position	Dates
Ecology Action	Santa Cruz, CA	Volunteered at Bike to Work day and other bike trail advocacy events. Created "Why I bike to work" promotional video. Assisted in rail clean-up.	February 2016
Bike Santa Cruz County	Santa Cruz, CA	Volunteered at Bike to Work day, additional events as a bike valet, and at a night bike ride.	Fall 2017
Friends of the Lafitte Greenway	New Orleans, LA	Local Foods Intern leading community and partner outreach. Created Sustainability report, worked on grant funding, and more.	May-August 2015
Please see attached resume for additional experience			

**Statement of Qualifications:** Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

**Certification:** I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.

\_\_\_\_\_  
Signature

4/24/18

\_\_\_\_\_  
Date

**Return Application to:**

SCCRTC Attn: Anais Schenk  
1523 Pacific Avenue  
Santa Cruz, CA 95060  
fax: (831) 460-3215 or email: [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org)

**Questions or Comments:**

Contact Anais Schenk at (831) 460-3209 or by email at [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org).

Santa Cruz County Regional Transportation Commission

Bicycle Advisory Committee Application

Statement of Interest for Kelly Bond

*Why I'm interested in this committee:*

While living in New Orleans I was hit by a car while biking. I was doing everything right (wearing a helmet, hand signaling, etc.), but the car just didn't see me (or think to look for me). I want to make biking safer for everyone through improved infrastructure, better bike policy, and education for cyclists, pedestrians, and drivers. Roads should be safely shared and cities should support additional paths for solely cyclist and pedestrian commuting and leisure. I believe the RTC's Bike Advisory Committee would be a space for me to contribute my skills and knowledge, and make a difference in this county.

*Why I'm qualified for this position:*

My previous board member experience through Slow Food Santa Cruz, work with the Community Advisory Board at Tulane's Prevention Research Center, and current position on CCOF's staff appreciation committee, makes me fully qualified for this position.

I understand how boards function and what makes them work well. My studies and work in public health have provided me ample experience in leading meetings, engaging community members, and working with others on committees and projects.

In New Orleans I was an avid biker, participating in group rides, an urban bike farm tour, and won first place in the citywide Eat Local Footprint Challenge put on by local bike advocacy group Bike Easy.

In Santa Cruz I've continued to bike as much as possible and have participated in numerous bike events throughout the city. I've volunteered with Ecology Action and Bike Santa Cruz County, and was recently trained to be a Bike Ambassador for the city of Santa Cruz's upcoming bike share program. I am currently arranging to give a bike share demonstration with Delaware Avenue businesses. I understand the bike culture of Santa Cruz, as well as biking infrastructure, and organizations working to improve it all!

*Future endeavors I'm interested in:*

I'm interested in making connections with others in the community. My place of employment, CCOF, is full of biking advocates. I would be happy to communicate the RTC Bike Advisory Committee's message to my colleagues. I volunteer weekly at the downtown Farmers Market, and therefore might be able to speak to folks there as well. If chosen for a committee position, I would be interested in working on bike policy and any program development. Research is a passion of mine, and therefore I would be happy to conduct research on successful bike policies around the country if needed. I'd also be happy to perform community outreach, speaking to folks about their wants and concerns regarding biking.



# COMMITTEE APPOINTMENT APPLICATION

## Santa Cruz County Regional Transportation Commission (SCRTC) Bicycle Advisory Committee

The Bicycle Advisory Committee consists of eleven seats: one for each city within Santa Cruz County, one for each of the county districts, one representing the Bike to Work program and one representing the Community Traffic Safety Coalition. Each of the seats has an alternate member appointed in the case that the primary member is unable to attend a meeting.

Meetings are currently held the second Monday of even numbered months at 6:00 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1523 Pacific Avenue in downtown Santa Cruz. Please refer to the Committee description and bylaws for more information: <https://sccrtc.org/meetings/bike-committee/>.

If you are interested in serving on this committee, we recommend reviewing the by-laws and attending a meeting to familiarize yourself with the committee process. After doing so please complete this application and email a scanned signed copy to [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org) or return a signed copy to the RTC office.

Name:

Michael S. Moore

Home address:

[REDACTED]

Mailing address (if different):

\_\_\_\_\_

Phone: (home)

[REDACTED]

(business/message/mobile)

[REDACTED]

E-mail:

[REDACTED]

Length of residence in Santa Cruz County:

27

I am applying to represent the following vacant position (circle one):

City of Capitola - Primary

City of Watsonville - Alternate

City of Capitola - Alternate

County District 4 - Primary

City of Scotts Valley - Primary

County District 4 - Alternate

City of Scotts Valley - Alternate

County District 5 - Alternate

City of Santa Cruz - Alternate

I would consider a different position - either a different location or alternate versus primary (circle one): Yes / No



**Previous experience on a government commission or committee:** (Please describe the committee/commission's purpose and your role.)

Represented City of Capitola on Bike Committee previously;  
not certain of exact term; 90's & early 00's.

**Other Relevant Work or Volunteer Experience**

Organization	Location	Position	Dates
Bicycle Trip	1001 Soquel Ave, 95062 3555 Clares Str, 95010	General Manager	11/09 - current
Coffee Design	681 Beach Dr, La Selva 95026	Sales Manager	5/08 - 11/16
CX Magazine Pacific Cycle Design	Independent Contractor	Business Operations Sales Manager	6/16 - 3/17
Spokesman Bicycles	231 Cathcart Str.	Owner	10/98 - 5/04

**Statement of Qualifications:** Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

**Certification:** I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the



5/15/18

Date

**Return Application to:**

SCCRTC Attn: Anais Schenk  
1523 Pacific Avenue  
Santa Cruz, CA 95060  
fax: (831) 460-3215 or email: [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org)

**Questions or Comments:**

Contact Anais Schenk at (831) 460-3209 or by email at [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org).

Michael Sean Moore



## Statement of Qualifications; Bicycle Advisory Committee, Primary Representative-Capitola

I'm a dynamic, passionate, entrepreneurial professional that has spent his life in the bicycle industry. I began cycling seriously in the 7th grade - training for cycle touring trips in Oregon - and a 24 hour bike marathon raising funds for the American Red Cross. I grew up in the Claremont Village - a one car household - walking and riding everywhere. Early on, I learned to appreciate safe and supportive pedestrian and bicycle infrastructure and resources. I moved to Santa Cruz in 1991 - and began working at Spokesman Bicycles in 1992; I'd later purchase that store with a friend and business partner. Prior to leaving Spokesman in 2003, I founded the Santa Cruz Bicycle Industry Coalition, in partnership with Piet Canin and Ecology Action. So too did I serve on the Bicycle Advisory Committee as Primary Representative for Capitola. Nine years ago, I joined Bicycle Trip, charged with expanding our impact in the community, growing revenue and enhancing profitability. On the heels of a successful 2017, we purchased Bike Station in Capitola's Brown Ranch Marketplace - reopened the store this February as Bicycle Trip Capitola. As General Manager for Bicycle Trip Inc., I work in both store locations. My wife and I rented in the Capitola Village for years prior to purchasing a small cottage in the Riverview Terrace neighborhood in 2003. I commute to work by bicycle, often lengthening the trip with a boogie through the Santa Cruz Mountains. A dedicated recreational cyclist and retired bike racer, I train throughout our region, typically 10 hours a week. I'm personally and professionally dialed into the broad body of bicycle related issues in Santa Cruz County. I enjoy a profound connection to cyclists, roadway/bikeway conditions, bicycle and pedestrian plans and projects. As well, I'm keenly focused on contributing to and supporting the development and execution of true transportation solutions in our highly-impacted community. My views and values align with the crop of projects and policies driven by the RTC. I'm eager to re-join the Bicycle Advisory Committee as a civic-minded veteran bike commuter, recreational cyclist and bicycle industry insider.

**Santa Cruz County Regional Transportation Commission (SCCRTC)  
Bicycle Advisory Committee**

If you are interested in serving on this committee, we recommend reviewing the by-laws and attending a meeting to familiarize yourself with the committee process. After doing so please complete this application and email a scanned signed copy to [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org) or return a signed copy to the RTC office.

Length of residence in Santa Cruz County: 12 years

City of Capitola - Primary	City of Watsonville - Alternate
City of Capitola - Alternate	County District 4 - Primary
City of Scotts Valley - Primary	County District 4 - Alternate
City of Scotts Valley - Alternate	County District 5 - Alternate
City of Santa Cruz - Alternate	

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**Previous experience on a government commission or committee:** (Please describe the committee/commission's purpose and your role.)

Transportation Advisory Board, Longmont, Colorado, 2005-2006. This citizen advisory

committee consists of members appointed by city council to given citizen input on all

aspects of transportation planning and projects for the city of Longmont.

**Other Relevant Work or Volunteer Experience**

Organization	Location	Position	Dates
Silicon Valley Bicycle Coalition	Santa Clara County, CA	Caltrain Working Group	11/2008 - 3/2009
Silicon Valley Bicycle Coalition	San Jose, CA	Ride leader San Jose Bike Train / Lupe Bike Pool	2012 - present
Silicon Valley Bicycle Coalition	Santa Clara County, CA	Ride Scout / Ride Ambassador	2/2016 - present

**Statement of Qualifications:** Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

**Certification:** I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the



May 14 2018  
Date

**Return Application to:**

SCCRTC Attn: Anais Schenk  
1523 Pacific Avenue  
Santa Cruz, CA 95060  
fax: (831) 460-3215 or email: [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org)

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May 14, 2018

**Randy Johnson**

Santa Cruz County Regional Transportation Commission  
1523 Pacific Ave  
Santa Cruz, CA 95060

Dear Commissioner Johnson:

I am applying as the Scotts Valley citizen representative for Bicycle Advisory Committee.

I understand the role of advisory commissions, transportation funding in California, and the role of SCCRTC in funding projects in Santa Cruz County. I appreciate the challenges of funding and designing for a wide variety of transportation users in a county with a small tax base.

I am a long time cyclist who bikes many days across town, and often around the County. I like the small town character and safety of Scotts Valley, as well as the easy co-existence between those of all ages who drive, bike, walk, and use motorized wheelchairs in this town. Mobility for all road users is essential for our quality of life. I'm personally connected with most of those involved in recreational cycling in Scotts Valley, including Andrew Cavalletto (Scotts Valley Cycle Sport owner), Steve Simonovich (former coach, SVHS mountain bike team), and Mark Davidson (former director of Mountain Bikers of Santa Cruz who spearhead the Skypark Pump Track project).

Both of my children attended Scotts Valley schools; my son was active in Boy Scout troop 614. Professionally, I commute over the hill to Oracle Corporation in Santa Clara where I work as a software engineer.

Thank you for consideration.

Sincerely,



Richard Masoner

# COMMITTEE APPOINTMENT APPLICATION

## Santa Cruz County Regional Transportation Commission (SCCRTC) Bicycle Advisory Committee

The Bicycle Advisory Committee consists of eleven seats: one for each city within Santa Cruz County, one for each of the county districts, one representing the Bike to Work program and one representing the Community Traffic Safety Coalition. Each of the seats has an alternate member appointed in the case that the primary member is unable to attend a meeting.

Meetings are currently held the second Monday of even numbered months at 6:00 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1523 Pacific Avenue in downtown Santa Cruz. Please refer to the Committee description and bylaws for more information: <https://sccrtc.org/meetings/bike-committee/>.

If you are interested in serving on this committee, we recommend reviewing the by-laws and attending a meeting to familiarize yourself with the committee process. After doing so please complete this application and email a scanned signed copy to [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org) or return a signed copy to the RTC office.

Name: Jo Fleming

Home address: [REDACTED]

Mailing address (if different):

Phone: (home)  (business/message/mobile) [REDACTED]

E-mail: [REDACTED]

Length of residence in Santa Cruz County: ~20 years

I am applying to represent the following vacant position (circle one):

- |                                     |                                 |
|-------------------------------------|---------------------------------|
| City of Capitola - Primary          | City of Watsonville - Alternate |
| City of Capitola - Alternate        | County District 4 - Primary     |
| ✓ City of Scotts Valley - Primary   | County District 4 - Alternate   |
| ✓ City of Scotts Valley - Alternate | County District 5 - Alternate   |
| City of Santa Cruz - Alternate      |                                 |

I would consider a different position - either a different location or alternate versus primary (circle one): ☒ Yes / No

**Previous experience on a government commission or committee:** (Please describe the committee/commission's purpose and your role.)

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Previously, I worked for the County of Santa Cruz Public Works Dept. I also convene and chair an Active Transportation Committee in Scotts Valley. I served on the Board of Directors for Ecology Action for 11 years. I am a consultant for the City of Scotts Valley Public Works Department.

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**Other Relevant Work or Volunteer Experience**

Organization	Location	Position	Dates
See above.			

**Statement of Qualifications:** Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

**Certification:** I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.

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Signature

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4/26/18  
Date

**Return Application to:**

SCCRTC Attn: Anais Schenk  
1523 Pacific Avenue  
Santa Cruz, CA 95060  
fax: (831) 460-3215 or email: [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org)

**Questions or Comments:**

Contact Anais Schenk at (831) 460-3209 or by email at [aschenk@sccrtc.org](mailto:aschenk@sccrtc.org).



# Statement of Qualifications

## ENVIRONMENTAL INNOVATIONS

I operate an environmental consulting firm. Environmental Innovations (EI) provides environmental consulting services to businesses, local governments and learning institutions in the areas of Green Business, environmental awareness, public outreach, and Sustainable Management.

It would be an honor to serve on the Bicycle Committee representing Scotts Valley. I have lived in Scotts Valley since 2005 and in Santa Cruz County for approximately 20 years. I coached cross-country mountain biking for five years. I am an avid cyclist, both road and mountain. I have been keen to see active transportation grow in Scotts Valley with the influx of bike industry and a growing group of citizens who are also passionate. I started a volunteer group called the Scotts Valley Active Transportation Group. We run a "Rolling School Bus" once a month that safely takes kids to Vine Hill School. The effort involved the school, parents, the Police Department as well as volunteers to lead and usher kids to school by bike. I have proposed bike infrastructure and obtained funding for that infrastructure. I know the routes well and can speak to how they tie in with other County routes.

### Josephine Fleming, Principal Scientist and Owner

#### Skills Summary:

- Environmental Scientist with Multi-media experience in soil, wastewater, storm water, air, and solid waste resource management
- Experience with regulatory compliance, resource conservation, and sustainability
- Training and work ethic from the private sector with experience in the public sector
- Significant supervisory experience with a track record of only hiring and maintaining the best employees
- Rational, analytically-minded, mathematical thinker
- Strong collaboration abilities, never confrontational
- Ability to understand complicated industrial and corporate processes and recognize cost-saving pollution prevention opportunities
- Practice resource conservation and pollution prevention techniques at home and in life to afford a sustainable future to the next generation

#### Awards:

Western Regional Pollution Prevention Network Pollution Prevention Champion 2003.  
County of Santa Cruz "Gold" Employee of the Year Award 2004.  
California Water Environment Association Pretreatment, Pollution Prevention and Stormwater Person of the Year 2003  
Department of Toxic Substances Control Office of Pollution Prevention Recognition for exemplary work in promoting Pollution Prevention 2003 and 2006  
CWEA Certified Environmental Compliance Inspector

#### Education:

University of California, Riverside: Bachelor of Science in Environmental Science, Minor in Spanish Literature.  
Universidad de Granada, España: Educational year abroad, emphasis on ecology in Hispanic cultures.  
University of New England, Australia: First year toward a full scholarship Ph.D. in Resource Engineering.



**Experience:** Environmental Programs Coordinator, Extra Help: Amy Gross, County of Santa Cruz Department of Public Works. Perform public outreach duties as it pertains to the Santa Cruz County Sanitation District, present pollution prevention opportunities to businesses in the District and market businesses on behavioral changes toward sustainability. Begin and coordinate a Monterey Bay Area Green Business Program to provide an incentive for businesses to go above and beyond regulations. Additional duties were to supervise work necessary to achieve certification of governmental facilities. (March 2006 to April 2007)

Environmental Programs Coordinator: Russell Bateson, County of Santa Cruz Department of Public Works. This environmental regulatory position performed all of the above, as well as overseeing an Environmental Compliance Inspection Program and a Federally-mandated Industrial Wastewater Pretreatment Program. (May, 2002 to February 2006)

Senior Environmental Engineer: Cheryl McDougall, Seagate Technology LLC. Performed duties related to Environmental Health and Safety including composing, managing and administering the following programs: hazardous waste management, hazardous waste minimization, air emissions management, industrial wastewater management, chemical management, chemical hygiene, emergency response planning, hazardous energies management, and numerous health and safety programs. (January 2001 to April 2002)

Environmental Scientist: Amanda Aldersley, Earth Tech, Inc. Environmental Consulting involving Phase I, II, and III Environmental Site Assessments, on-site environmental consultant for construction/excavation projects, and environmental health and safety outsourcing. Provide EH&S compliance support to microelectronics companies in the Silicon Valley including Apple Computers, PE Biosystems, and Applied Materials. (April 1998 to September 2000)

Laboratory Research Assistant: Dr. David Crohn, Solid Waste Management, University of California, Riverside. Assist in constructing a mathematical computer model for the treatment of municipal wastewater through an artificial wetlands and its effects on the ecological system. Determined nutrient cycling patterns in water and benthos. Additional experiments also determined nutrient cycles in farm soils treated with biosolids and in composts. (October 1995 to August 1996)

Radiation Safety Technician: Environmental Health and Safety, University of California, Riverside. Radioactive and hazardous waste pickup, analysis, and disposal or proper storage to comply with EPA regulations. (August 1994 to October 1995)

Lab Assistant and Field Crew: USDA Soils and Pesticide Research, University of California, Riverside. Analysis of soil, water and atmospheric samples for pesticide volatility and mobility. Installation of field experiments, collection of samples. (October 1990 to June 1992, September 1992 to June 1993)

Lab Assistant: Borax España. Maintenance and operation of an Atomic Absorption Spectrometer to analyze Boron products. (June to September 1992)

#### **Leadership/Community Experience:**

Director: Ecology Action of Santa Cruz Board of Directors. As a board member, responsibilities were to oversee and govern the actions of the organization. Ecology Action is a nonprofit environmental consultancy delivering cutting edge education services, technical assistance, and program implementation for initiatives that assist individuals, business and government to maximize environmental quality and community well being.

Director: Neighborhood Childcare Center Board of Directors. As a board member, responsibilities were to oversee and govern the actions of the organization. NCC is a preschool for children of working parents committed to economic and social justice (sliding pay scale), with a focus on conflict resolution, cultural diversity, and Spanish language enrichment.

#### **List of Current Clients:**

City of Watsonville  
Ecology Action of Santa Cruz  
County of Santa Cruz  
City of Scotts Valley  
California Green Business Network  
California State University Monterey Bay

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