



Santa Cruz County Regional Transportation Commission's
Elderly & Disabled Transportation Advisory Committee
(Also serves as the Social Service Transportation Advisory Council)

AGENDA

1:30pm - 3:30pm

Tuesday, February 8, 2022

NOTE: TELECONFERENCE

Join the online meeting to see presentations:

<https://us02web.zoom.us/j/84789432255>

Meeting ID: 847 8943 2255

Dial by your location: +1 669 900 9128

Members of the public may not attend this meeting in person. Comments and questions may be shared with the Committee through teleconference audio in real time, or by prior written submission to amarino@sccrtc.org.

Due to precautions associated with COVID-19, and following current state law (AB 361) regarding the Brown Act, all RTC and committee meetings until further notice will be held by teleconference only. Members of the public can listen and participate in meetings over the phone and through the internet.

1. **1:30pm — Call to Order**
2. **1:30pm — Introductions**
3. **1:35pm — Oral communications**
4. **1:40pm — Additions or deletions to the consent or regular agenda**

1:42pm- CONSENT AGENDA

All items appearing on the consent agenda are considered to be minor or non-controversial and will be acted upon in one motion if no member of the E&D TAC or public wishes an item be removed and discussed on the regular agenda. Members of the E&D TAC may raise questions, seek clarification or add directions to Consent Agenda items without removing

the item from the Consent Agenda as long as no other E&D TAC member objects to the change.

- 5. Approve Minutes from January 11, 2022— pg. 5**
- 6. Receive RTC Meeting Highlights— pg. 9**
- 7. Receive Transportation Development Act Revenues Report— pg. 12**
- 8. Receive FY 22-23 TDA Claims Calendar— pg. 13**
- 9. Receive Notice of Chair and Vice Chair Election— pg. 14**
- 10. Receive Information Items— pg. 15**
 - a. Regional E-Bike Rebate Summary Flyer

REGULAR AGENDA

- 11. 1:55 pm — Receive Program Updates— pg.16**
 - a. Volunteer Center
 - b. Community Bridges - TDA 1st Quarter Summary Report FY 2021-2022
 - c. Santa Cruz Metro
 - d. SCCRTC
 - e. Pedestrian Ad-hoc Subcommittee
 - i. Pedestrian Hazard Report
- 12. Committee Appointments— pg. 19**
- 13. Preliminary Draft 2022 Unmet Needs List— pg. 45**
- 14. City of Santa Cruz Regional Bike Share Program— pg. 57**
- 15. City of Santa Cruz TDA Funded Rectangular Rapid Flashing Beacons— pg. 59**
- 16. City of Santa Cruz's Chestnut Storm Drain and Pavement Rehabilitation Project— pg. 64**
- 17. Caltrans D5 Hybrid Beacons on Mission St.— pg. 75**
- 18. Capitola's Draft Design for Clares & 41st Ave Projects— pg. 81**

19. Draft Santa Cruz County Active Transportation Plan— pg. 88

20. Draft 2022 RTC Legislative Program— pg. 90

21. 3:30 pm — Adjourn

Next meeting: Special Meeting 1:30 pm, March 15, 2022 via teleconference.

HOW TO REACH US

*Santa Cruz County Regional Transportation
Commission
1101 Pacific Avenue. Suite 250,
Santa Cruz, CA 95060
Phone: (831) 460-3200 / fax (831) 460-3215
Email: info@sccrtc.org / website: www.sccrtc.org*

ACCOMMODATIONS FOR PEOPLE WITH DISABILITIES

The Santa Cruz County Regional Transportation Commission does not discriminate on the basis of disability and no person shall, by reason of a disability, be denied the benefits of its services, programs, or activities. This meeting location is an accessible facility. If you wish to attend this meeting and require special assistance in order to participate, please contact RTC staff at 460-3200 (CRS 800/735-2929) at least three working days in advance of this meeting to make arrangements. People with disabilities may request a copy of the agenda in an alternative format. As a courtesy to those person affected, please attend the meeting smoke and scent-free.

SERVICIOS DE TRADUCCIÓN/TRANSLATION SERVICES

Si gusta estar presente o participar en esta junta de la Comisión Regional de Transporte del condado de Santa Cruz y necesita información o servicios de traducción al español por favor llame por lo menos con tres días laborables de anticipo al (831) 460-3200 para hacer los arreglos necesarios. (Spanish language translation is available on an as needed basis. Please make advance arrangements (at least three days in advance by calling (831) 460-3200.

TITLE VI NOTICE

The RTC operates its programs and services without regard to race, color and national origin in accordance with Title VI of the Civil Rights Act. Any

person believing to have been aggrieved by the RTC under Title VI may file a complaint with RTC by contacting the RTC at (831) 460-3212 or 1523 Pacific Avenue, Santa Cruz, CA, 95060 or online at www.sccrtc.org. A complaint may also be filed directly with the Federal Transit Administration to the Office of Civil Rights, Attention: Title VI Program Coordinator, East Building, 5th Floor-TCR, 1200 New Jersey Ave., SE, Washington, DC 20590.

I:\E&DTAC\2022\02-08\01_Agenda-Feb.docx



Santa Cruz County Regional Transportation Commission's
Elderly & Disabled Transportation Advisory Committee

(Also serves as the Social Service Transportation Advisory Council)

Draft MINUTES

1:30pm - 3:30pm

Special Meeting

Tuesday, January 11, 2022

NOTE: Meeting was held as teleconference

1. Roll call

The meeting was called to order at 1:34 p.m.

Members present:

Patty Talbot, Social Services Provider — Seniors
Tara Ireland, Social Service Provider-Persons of Limited Means
Lisa Berkowitz, CTSA (Community Bridges)
Jesus Bojorquez, CTSA (Lift Line)
Eileen Wagley, SCMTD (Metro)
Michael Pisano, Potential transit User (60+)
Caroline Lamb, Potential Transit User (Disabled)
Janet Edwards, 1st District
Veronica Elsea, 3rd District

Unexcused absences:

Alex Weske, Social Service Provider – Disabled
Paul Elerick, 2nd District
Martha Rubbo, 4th District

RTC staff present:

Amanda Marino, Transportation Planner
Amy Naranjo, Transportation Planner

Others present:

Becky Steinbruner, Cabrillo Host Lions Club

2. Introductions

3. Oral communications

Committee member Janet Edwards provided information on redistricting in Santa Cruz County and reminded the committee of opportunities to provide input on the Soquel Drive Buffered Bike Lane Congestion Mitigation Project. More information can be found by visiting: <https://www.soqueldrivebufferedproject.com/>

Committee Member Michael Pisano expressed interest in creating a more bikeable and walkable community in Santa Cruz County.

Becky Steinbruner, member of the public, representing the Cabrillo Host Lions Club communicated the Cabrillo Host Lion's Club's interest in future collaboration with the E&D TAC supporting projects that address pedestrian hazards for individuals who are visually impaired. Ms. Steinbruner identified three areas in Santa Cruz County that are hazardous to visually impaired pedestrians.

4. Additions or deletions to consent and regular agendas - none

CONSENT AGENDA

5. Approved minutes from November 16, 2021
6. Received RTC Meeting Highlights
7. Received 2022 E&D TAC Meeting Schedule
8. Received Information Items
 - a. FTA Section 5310 Program Fact Sheet
 - b. Caltrans Complete Streets Director's Policy

A motion (Edwards/Berkowitz) was made to approve the consent agenda. The motion passed with members Patty Talbot, Tara Ireland, Lisa Berkowitz, Jesus Bojorquez, Michael Pisano, Caroline Lamb, Janet Edwards, and Veronica Elsea voting in favor with abstentions from Tara Ireland and Eileen Wagley.

REGULAR AGENDA

9. Received Program Updates
 - a. Volunteer Center

Tara Ireland informed the committee of the increase of seniors that continue to participate in the grocery shopper program.

b. Community Bridges

Jesus Bojorquez updated the committee on the continuation of the program to provide transportation for seniors to grocery stores, restaurants, and Covid-19 booster shots.

c. Santa Cruz Metro

Eileen Wagley announced that Metro is experiencing personnel and administrative changes due to the departure of Alex Clifford announced at Metro's last regular Board of Director's meeting that he will retire from his position as CEO/GM of the Santa Cruz Metropolitan Transit District in January 2022. Metro will have in place an interim CEO/GM until a permanent CEO/GM is appointed by the Board. Metro also has many job openings, more information can be found: <http://www.scmttd.com/en/agency-info/metro-employment>

d. SCCRTC

RTC Transportation Planner Amanda Marino provided an update on the Regional Transportation Improvement Program (RTIP) discretionary funds that includes using the remaining balance available for school safety and access projects - the Highway 9/SLV schools and Ecology Action Safe Routes to Schools projects.

Ms. Marino informed the committee that when conditions allow the RTC will conduct hybrid meetings and will provide Covid-19 findings at the RTC meetings to abide by AB 361 where the governor's executive orders suspend several provisions of the Brown Act to ensure that the work of government agencies could continue with virtual meetings to reduce the risk of spreading Covid-19.

e. Pedestrian Ad-Hoc Subcommittee

i. Pedestrian Hazard Report

ii. Letter to the City of Santa Cruz Public Works Department

Chair Veronica Elsea informed the committee of the continuation of reviewing the pedestrian hazard report process. Ms. Elsea also reminded the committee of the E&D TAC's request for the City of Santa Cruz to attend the February meeting to provide an update on the new regional bike share program and the need to upgrade the Rapid Flashing Beacons to be accessible.

10. Release of Draft 2045 Regional Transportation Plan

RTC Transportation Planner Amy Naranjo presented to the committee and solicited input on the Draft 2045 Regional Transportation Plan (RTP) including the policy, financial, and action elements. The three main components include draft goals, policies, targets, complete list of projects, and the fiscally constrained project list. The E&D TAC provided comments that consist of; including the term “functional” to pedestrian facilities, additional information on the Pedestrian Safety Workgroup and the Pedestrian Ad-hoc Subcommittee, reference the loss of accessible taxi service. Additional RTP comments can be sent to 2045rtp@sccrtc.org and EIR comments to hadamson@ambag.org by January 31st, 2022.

No action taken.

Meeting adjourned at approximately 2:45 pm.

The next E&D TAC meeting is scheduled for Tuesday, February 8th, 2022 at 1:30 p.m. NOTE: Teleconference may be necessary due to COVID-19.

Respectfully submitted, Amanda Marino, Staff



*Santa Cruz County Regional Transportation Commission
1101 Pacific Avenue, Santa Cruz, CA 95060
phone: (831) 460-3200
email: info@sccrtc.org; website: www.sccrtc.org*

*CONTACTS: Shannon Munz, Communications Specialist (smunz@sccrtc.org)
Guy Preston, Executive Director
Luis Pavel Mendez, Deputy Director*

Santa Cruz County Regional Transportation Commission (RTC) January 13, 2022 Meeting Highlights

Pilot Project of Vegetation Control Utilizing Goats Along the Santa Cruz Branch Rail Line

The Commission adopted a resolution to enter into an agreement for a two-month pilot program utilizing goats to control vegetation overgrowth along the Santa Cruz Branch Rail Line (SCBRL) corridor. Utilizing herds of goats to clear unwanted vegetation has potential as a viable option for the SCBRL. Goats are quieter, more energy efficient, require less person-power, and could reduce the need for and/or enhance the effectiveness of herbicides and are expected to reduce the overall maintenance cost of the corridor. This pilot project seeks to determine if utilizing goats to control vegetation could be a regular component of the overall maintenance of the SCBRL property.

Public Hearing: Draft 2045 Santa Cruz County Regional Transportation Plan

The Commission held a public hearing and received public input on the [Draft 2045 Regional Transportation Plan](#) (RTP). The RTP identifies transportation needs and priorities in Santa Cruz County over the next 25 years. It sets policy and a vision for the transportation system and estimates the amount of funding that will be available for planned transportation projects. The plan is an essential first step in securing funding from federal, state, and local sources. The RTP includes discussion of highways, local streets and roads, bicycle and pedestrian facilities, transit services, specialized transportation services for seniors and people with disabilities, airports, and goods movement. The Draft 2045 RTP is available for review at www.sccrtc.org/2045rtp and comments can be submitted through January 31, 2022 by email to 2045rtp@sccrtc.org.

San Lorenzo Valley Schools Complex Circulation Project

The Commission authorized the RTC to enter into a Memorandum of Understanding with the local and state agencies responsible for coordinating delivery of the San Lorenzo Valley (SLV) Schools Complex Circulation and Access Project. The [Highway 9/San Lorenzo Valley Complete Streets Corridor Plan](#) identified a need to improve safety, pedestrian and bicycle access, transit access, and traffic operations along Highway 9 in Felton near the SLV Schools Complex, as well as within the Schools Complex itself. To address traffic flow improvements, the RTC, Caltrans, SLVUSD, METRO, and Santa Cruz County Public Works plan to collaborate to develop the SLV Schools Complex Circulation and Access Study as a first step in implementing the project. This study will gather data, develop preliminary engineering schematics, conduct a traffic analysis, and conduct a feasibility and needs assessment for improving multimodal system performance along SR 9 in Felton and within the SLV Schools Complex.

Construction Contract Award – Phase I Coastal Erosion Repair at Manresa Along the Santa Cruz Branch Rail Line

The Commission adopted a resolution accepting a bid for construction of the Phase 1 coastal erosion repair near Manresa State Beach along the Santa Cruz Branch Rail Line. The project's scope of work includes the construction of a new 70-foot-long soldier pile retaining wall west of the existing track. Work also includes regrading of the ditch along the east side of the tracks, locating and unclogging of an existing 12-inch corrugated metal pipe at the private grade crossing, and installation of new chain link fence. Construction is anticipated to begin in February of 2022 and is scheduled to be completed by June, weather permitting.

Upcoming RTC and Committee Meetings

Due to precautions associated with COVID-19 (coronavirus), **all RTC and committee meetings have been held by video conference.** On September 16, 2021, Governor Newsom signed AB 361 which continues the Brown Act waivers and allows the RTC to continue Commission and committee meetings in a virtual format. The RTC will continue to hold its commission meetings in a virtual format until further notice. Please check the RTC website [<https://sccrtc.org/meetings/calendar/>] or call 460-3200 to confirm meeting and video conference information for future meetings. Agendas are posted to the website at least 3 days before the meeting and will also include attendance information. Meetings may be canceled if there are no action items to be considered by the committee.

The RTC is committed to its compliance with the Americans with Disabilities Act (ADA) during this time of national emergency. Please contact the RTC at least 3 days in advance of a meeting if special accommodations are needed. If any document, webpage, meeting, or recording is inaccessible to you, kindly notify us at info@sccrtc.org or by calling 831-460-3200.

Regional Transportation Commission Meeting

Thursday, February 3, 2022, 9:00 a.m.

Interagency Technical Advisory Committee

Thursday, January 20, 2022, 1:30 p.m.

Bicycle Advisory Committee

Monday, February 7, 2022, 6:00 p.m.

Elderly & Disabled Transportation Advisory Committee

Tuesday, February 8, 2022, 1:30 p.m.

Transportation Policy Workshop

Thursday, February 17, 2022, 9:00 a.m.

Public input on transportation issues is welcomed and encouraged. For more information, visit the SCCRTC website at www.sccrtc.org or call 460-3200. Some Regional Transportation Commission meetings are televised countywide

by Community TV of Santa Cruz. Consult www.communitytv.org or call 831-425-8848 for schedule and station information.

**SANTA CRUZ COUNTY REGIONAL TRANSPORTATION COMMISSION
TDA REVENUE REPORT
FY 2021/2022**

MONTH	FY20 - 21 ACTUAL REVENUE	FY21 - 22 ESTIMATE REVENUE	FY21 - 22 ACTUAL REVENUE	DIFFERENCE	DIFFERENCE AS % OF PROJECTION	CUMULATIVE % OF ACTUAL TO PROJECTION
JULY	1,016,646	954,838	1,180,089	225,250	23.59%	123.59%
AUGUST	748,355	702,858	947,834	244,976	34.85%	128.37%
SEPTEMBER	932,896	876,180	1,008,451	132,271	15.10%	123.78%
OCTOBER	1,164,146	1,093,371	1,242,981	149,609	13.68%	120.73%
NOVEMBER	670,297	629,546	835,454	205,908	32.71%	122.51%
DECEMBER	866,405	813,731	1,015,997	202,266	24.86%	122.88%
JANUARY	898,170	843,565	990,616	147,051	17.43%	122.11%
FEBRUARY	1,074,000	1,008,706				
MARCH	741,758	696,663				
APRIL	891,479	837,281				
MAY	821,015	771,101				
JUNE	908,248	853,031				
TOTAL	10,733,415	10,080,871	7,221,422	1,307,332	12.97%	72%

Note:

I:\FISCAL\TDA\MonthlyReceipts\FY2022\[FY2022 TDA Receipts.xlsx]FY2022

FY 2022-23 Funding Cycle Calendar

Article 8 Transportation Development Act

March

- 3 ***Commission's Budget & Administration/Personnel Committee*** considers preliminary FY 2022-23 budget including TDA estimates
- 31 ***CTSA, Volunteer Center, and METRO*** submit draft claims (including the claim form and back up materials) to the Regional Transportation Commission Staff (RTC Staff) to be included in the April E&D TAC packet.

April

- 7 ***RTC*** considers revised budget including TDA allocations
- 12 The ***E&D TAC*** reviews the *Volunteer Center, CTSA, and METRO* draft claims for expending the TDA funds along with back up materials
- 12 ***RTC staff*** notifies recipients if revisions to the FY 22-23 TDA claims are necessary
- 19 ***CTSA, Volunteer Center and METRO*** send the final claim materials to the RTC staff. ***RTC staff*** prepares the SCCRTC resolution and staff report for the RTC meeting. Recommendations of approval by RTC staff for CTSA and Volunteer claim recommendations will be contingent on approval from the City of Santa Cruz to act as the claimant.

May

- 5 ***RTC*** acts on the claims and the resolution prepared by SCCRTC staff approving the Article 8c TDA claims. The resolution will outline the amount of the claim, the payment amounts and schedule and the reporting requirements.
- 9 The ***CTSA*** and ***Volunteer Center*** submit to the City of Santa Cruz TDA materials and a request for the City of Santa Cruz to act as the claimant
- 19 The ***City of Santa Cruz*** acts to approve by resolution an authorization to direct the City Manager to execute an agreement with the Volunteer Center and CTSA allowing the City to act as a claimant for TDA funds obtained from the Commission.

June

- 6-17 ***RTC fiscal officer*** readies paperwork to begin payment of claims at the beginning of the fiscal year.

[Bold italics indicate the responsible party.]

TO: Elderly and Disabled Transportation Advisory Committee
FROM: Amanda Marino, Transportation Planner
RE: Committee Chair and Vice Chair Election at the April Meeting

RECOMMENDATIONS

This is for your information only.

BACKGROUND

Below is an excerpt from the RTC/Committee's rules and regulations:

A Chairperson and Vice Chairperson for each Committee shall be elected to serve for a term of one year. The Committee shall elect its officers at the first meeting following the March SCCRTC meeting of every year. The Chairperson shall preside at all meetings of the Committee. The Chairperson shall maintain order and decorum at the meetings, decide all questions of order, and announce the Committee's decisions. The Vice Chairperson shall perform the duties of the Chairperson in his or her absence. In the event both officers are absent from the Committee, the majority of quorum may appoint a presiding officer for that meeting. All officers shall continue in their respective offices until their successors have been elected and have assumed office.

DISCUSSION

At your April 12, 2022 Elderly and Disabled Transportation Advisory Committee (E&D TAC) meeting, elections will take place for the E&D TAC chair and vice chair. As noted in the bylaws, the term is one year and elections take place annually on the first meeting following the March RTC meeting which is April 12, 2022.

Members are encouraged to forward nominations, including self nomination, to the RTC staff prior to the April meeting. Chairing the E&D TAC is an excellent opportunity and staff recommends that all members strongly consider deepening their committee experience by taking a turn as chair. The April staff report will include all nominations received. An election will be held, and the chair will begin presiding immediately.

Save on buying an e-bike with new rebates!

Since September 2021, three new e-bike rebate programs have launched in Santa Cruz County, and you could save thousands of dollars on an e-bike!



Program: [Go Santa Cruz](#)

Location: Downtown Santa Cruz

Rebate Amounts: \$200-\$400

Notes: Point of purchase voucher. View 45-minute bike webinar. Santa Cruz County participating bike shops only. Class 1, 2 & 3 e-bikes accepted.



Program: [Central Coast Community Energy \(CCCE\)](#)

Location: CCCE residential customers in Santa Cruz, Monterey, San Benito, San Luis Obispo, and Santa Barbara counties

Rebate Amounts: \$500-\$1,100

Notes: Redeem rebate after purchase. Online and physical bike shops OK. Class 1, 2 & 3 e-bikes accepted.



Program: [Monterey Bay Air Resource District](#)

Location: Santa Cruz, Monterey, and San Benito County residents

Rebate Amounts: \$1,000

Notes: **Low income residents only.** Redeem rebate after purchase. Must be purchased from a bike shop (no box-stores) within Monterey, San Benito, and Santa Cruz Counties. Class 1, 2 & 3 e-bikes accepted.

Income-based rebates

Tiered rebates available based on income level, check each program criteria

Bonuses for buying local

Receive more money, support local business, enjoy expert shop support

Up to \$2,500 in savings possible

Multiple rebates can be stacked for more money toward buying an e-bike

Via Email

January 14, 2022

Ms. Kim Krause
Finance Director
City of Santa Cruz
333 Front Street, Suite 200
Santa Cruz, CA 95060

Re: **Transportation Development Act – 1st Quarter Summary Report FY 2021-2022**
(Contract between “City of Santa Cruz and Community Bridges/Consolidated Transportation Services Agency”)

Dear Ms. Krause:

Enclosed, please find the Transportation Development Act Quarterly Report for the period beginning July 1, 2021 and ending September 30th 2021.

If you would like additional information or have any questions, please feel free to contact me.

Thank you for your attention to this matter.

Sincerely,



Douglas Underhill
Chief Financial Officer
831-688-8840 ext. 276
douglasu@cbridges.org

Encl.

cc: G. Blakeslee, Senior Transportation Planner, SCCRTC
T. New, Director of Finance and Budget, SCCRTC
N. Duarte, Accounting Technician, City of Santa Cruz
S. Buie, Admin Assistant III, City of Santa Cruz
R. Iskandarova, Grants Analyst, Community Bridges
R. Cancino, CEO, Community Bridges
S. McGibben, CAO, Community Bridges
J. Bojorquez, Interim Program Director, CTSA: List Line

Quarterly TDA Report :		FY 21/22 QUARTER 1																									
Time Period:		JULY - AUG - SEPT 2021																									
		CC 20,23,26,31,32,38,39					CC 21					CC 29					CC 24,30										
#	Performance Measures to be Included in Quarterly Reports	Medical					YTD % of Goals	Meals on Wheels					YTD % of Goals	Taxi Scrip					YTD % of Goals	Elderday					YTD % of Goals	Qtr Total	YTD Total
		July	Aug	Sept	Qtr	YTD		July	Aug	Sept	Qtr	YTD		July	Aug	Sept	Qtr	YTD		July	Aug	Sept	Qtr	YTD			
1	Unduplicated Passengers per Month	205	180	193	322	322		81	99	90	123	123		16	16	16	27	27		48	46	55	61	61		533	533
2	Total Passenger Trips (Units of Service) per Month	846	831	987	2,664	2,664	44%	782	900	930	2,612	2,612	42%	81	66	75	222	222	15%	973	1,042	1,058	3,073	3,073	19%	8,571	8,571
3	Number of Incidents per Month	0	0	0	0	0		0	0	0	-	0		0	0	0	0	0		0	0	0	0	0		0	0
4	Number of Accidents per Month	1	0	0	1	1		0	0	0	-	0		0	0	0	0	0		0	0	0	0	0		1	1
5	Number of Mechanical Failures (including lift failure) per Month	0	0	0	0	0		0	0	0	-	0		n/a	n/a	n/a	n/a	n/a		0	0	0	0	0		0	0
6	Number of No-Shows per Month	21	25	36	82	82		2	3	0	5	5		n/a	n/a	n/a	n/a	n/a		43	32	28	103	103		0	0
7	Number of Turndowns or Referrals per Month	0	0	0	0	0		0	0	0	-	0		0	0	0	0	0		0	0	0	0	0		0	0
8	Number of Cancels per Month	1275	1054	1340	3,669	3,669		51	55	69	175	175		0	0	0	0	0		2281	1341	690	4,312	4,312		8,156	8,156
9	Total Donations per Month	680	20	70	770	\$770					-	0					\$0	\$0					0	0		770	770
10	Number of Complaints per 1,000 Passenger Trips	0	1	0	1	1		0	0	0	-	0		n/a	n/a	n/a	0	0		0	0	0	0	0		1	1
10	Complaints	0	1	0	1	1		0	0	0	-	0					0	0		0	0	0	0	0		1	533
11	Number of Safety Incidents per 100,000 Vehicle Miles	0				0		0				0		n/a	n/a	n/a	0	0		0.0				0		0	533
12	Operating Cost per Passenger Trip				\$68.23						\$15.13						\$39.08						\$26.74				
13	Operating Cost per Vehicle Service Hour				\$92.88						\$81.30												\$65.73				
14	Passengers per Vehicle Service Hour				1.43						5.37												4.93				
15	Passengers per Vehicle Service Mile				0.13						0.32												0.27				
16	Van Mileage per Program				18,885						8,083												11,117				

E & D TAC Pedestrian Hazard Reports

1/24

Date	First Name	Last Name	Location	Cross Street	City	Category	Additional Comments	Forwarded to	Forwarded Date	Response
12/27/21	Jules	Rodriguez	Soquel Drive	South Main Street	Santa Cruz	Ped: Rough pavement or potholes, Pavement cracks, Other	Other: Lifted manhole cover. I am in recovery for rotator cuff surgery. I fell on that shoulder and broke my right wrist and my ninth rib.	DPW	12/28/21	1/24/22 Travis Rieber: PG&E will be repairing the sidewalk adjacent to their manhole. My understanding is that the concrete repair will be happening in the next couple weeks.
12/06/21	Deborah	Benham	224 Kings Village Rd	N/A	Scotts Valley	Ped: Lighting problem	Flashing lights on pedestrian crosswalk signal not working on the Goodwill Store side of the street.	Trish McGrath, Kimarie Jones, Chris Lamm, Tyler Thomas	12/07/21	12/28/21 Tyler Thomas: We had the solar panel adjusted and battery fixed. The flashing lights now work on both sides.

TO: Elderly and Disabled Transportation Advisory Committee

FROM: Amanda Marino, Transportation Planner

RE: Elderly and Disabled Transportation Advisory Committee (E&D TAC) New Member Appointments

RECOMMENDATION

RTC staff recommends that the E&D TAC recommend that the RTC appointment new members and member alternate positions to fill vacancies on the E&D TAC.

BACKGROUND

Seats on the Elderly & Disabled Transportation Advisory Committee (E&D TAC) correspond to City and Supervisorial District seats on the Regional Transportation Commission (RTC). Commissioners may nominate individuals for Committee and RTC consideration.

DISCUSSION

Based on the public outreach, interest was expressed by seven individuals and resulting in seven applications received for the Elderly & Disabled Transportation Advisory Committee. In an effort to accommodate all interested applicants, staff recommends new positions noted as *pending* in the attached roster (Attachment 1). The applicants include **Clay Kempf, Patty Talbot, Lisa Berkowitz, Jesus Bojorquez, Daniel Zargoza, Veronica Elsea, and Alicia Morales** with their full applications in the same order in Attachment 2.

RTC staff recommends that the E&D TAC recommend that the RTC appoint new members to fill vacancies on the E&DTAC as shown in Attachment 1.

SUMMARY

The Elderly & Disabled Transportation Advisory Committee (E&D TAC) functions best when all committee membership and alternate positions are filled. Seven individuals have expressed interest in joining the E&D TAC. Staff recommends that new member and alternate positions be filled as shown (see Attachment 1 for current roster).

Attachment 1: February 2022 E&D TAC Roster

Attachment 2: Member Applications Form



Santa Cruz County Regional Transportation Commission
ELDERLY & DISABLED TRANSPORTATION ADVISORY COMMITTEE (E&D TAC)
SOCIAL SERVICES TRANSPORTATION ADVISORY COUNCIL (SSTAC)

Membership Roster
February 2022
 (Membership Expiration Date)

Members	Representing	Alternate
Clay Kempf (Pending)	Social Services Provider - Seniors	Patty Talbot (Pending)
Alicia Morales (Pending)	Social Services Provider - Seniors (County)	vacant
Alex Weske (2023)	Social Service Provider - Disabled	vacant
vacant	Social Service Provider - Disabled (County)	vacant
Tara Ireland (2024)	Social Service Provider - Persons of Limited Means	vacant
Lisa Berkowitz (Pending)	CTSA (Community Bridges)	vacant
Jesus Bojorquez (2022)	CTSA (Lift Line)	Nadia Noriega (Pending)
Eileen Wagley (2024)	SCMTD (Metro)	Daniel Zaragoza (Pending)
Michael Pisano (2023)	Potential Transit User (60+)	Patricia McVeigh (2023)
Caroline Lamb (2023)	Potential Transit User (Disabled)	Vacant

Supervisory District Representatives		
Members	Representing	Alternate
Janet Edwards, Vice Chair (2023)	1st District (Manu)	Phil Kipnis (2023)
Paul Elerick (2023)	2nd District (Friend)	vacant
Veronica Elsea, Chair (Pending)	3rd District (Coonerty)	vacant
Martha Rubbo (2023)	4th District (Caput)	Patricia Fohrman (2023)
Ed Hutton (2023)	5th District (McPherson)	Vacant

Staff: Amanda Marino, Regional Transportation Commission

COMMITTEE APPOINTMENT APPLICATION

Santa Cruz County Regional Transportation Commission (SCCRTC) Elderly & Disabled Transportation Advisory Committee (E&D TAC)

Meetings are scheduled for the second Tuesday of every other month at 1:30 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1101 Pacific Avenue, Suite 250 in downtown Santa Cruz. At least one meeting each year is scheduled for an alternate location. Please refer to the Committee description, bylaws and recruitment process for more information.

If you are interested in serving on this committee, please complete this application, and return it to the Regional Transportation Commission office.

PLEASE TYPE OR PRINT CLEARLY

Name: Clay Kempf

Home address: [REDACTED]

Mailing address (if different):

Phone: (home) [REDACTED] (business/message) [REDACTED]

E-mail: [REDACTED]

Length of residence in Santa Cruz County: 45 years

Position(s) I am applying for: ☐ Any appropriate position

☒ Social Service Provider for the Elderly ☐

Previous experience on a government commission or committee (please specify)

First appointed to this committee in 1989; member ever since. Also a member of the Social Services Transportation Advisory Committee in San Benito County and the Aging & Long Term Care Commission there. Former member of the Santa Cruz County Long Term Care Commission until it was dissolved.

Relevant Work or Volunteer Experience

Organization	Town or Address	Position	Dates
Seniors Council/Area Agency on Aging	Same as current	Executive Director	2000 - present
Food & Nutrition Services/Lift Line. (now Community Bridges)	519 Main State, Watsonville, CA	Director of Transportation	1990-98

Statement of Qualifications: Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

Without access to work, services, and socialization, seniors and people with disabilities quickly become isolated and at-risk of numerous threats to their independence. Specialized transportation is an essential element of keeping people connected with their community, and living a healthy and independent life. The Area Agency on Aging supports the delivery of specialized transportation services, including contributing funding for the transportation of older adults to various

congregate meal sites in the County. The E&DTAC continues to be the most effective local forum to discuss these issues.

As the Director of the Area Agency on Aging and the Executive Director of the Seniors Council, serving on the E&D TAC provides the opportunity to continue to provide input into our local transportation delivery models. Besides discussing regularly scheduled items and plans, I remain available to assist in activities and special projects as staff and the Committee deems appropriate.

Certification: I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.

[Redacted Signature]

Signature

1/18/22

Date

How did you learn about this opportunity?

☐ newspaper

☐ radio

☐ internet

☐ flyer

☐ friend/family member

☒ other

Return Application to:

SCCRTC

Elderly & Disabled Transportation Advisory Committee

1101 Pacific Avenue, Suite 250

Santa Cruz, CA 95060

fax: 460-6178 email: amarino@sccrtc.org

Questions or Comments: (831) 460-3200

I:\E&DTAC\MEMBERS\Application\COMMITTEE APPOINTMENT APPLICATION.doc

COMMITTEE APPOINTMENT APPLICATION

Santa Cruz County Regional Transportation Commission (SCCRTC) Elderly & Disabled Transportation Advisory Committee (E&D TAC)

Meetings are scheduled for the second Tuesday of every other month at 1:30 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1101 Pacific Avenue, Suite 250 in downtown Santa Cruz. At least one meeting each year is scheduled for an alternate location. Please refer to the Committee description, bylaws and recruitment process for more information.

If you are interested in serving on this committee, please complete this application, and return it to the Regional Transportation Commission office.

PLEASE TYPE OR PRINT CLEARLY

Name: Patty Talbott

Home address: [REDACTED]

Mailing address (if different): _____

Phone: [REDACTED] (business/message) [REDACTED]

E-mail: [REDACTED]

Length of residence in Santa Cruz County: 39 years

Position(s) I am applying for: ☐ Any appropriate position

☐ Alternate for Clay Kempf, Social Services Provider, Seniors

☐ _____

Previous experience on a government commission or committee (please specify)

Alternate for E&D TAC Social Services Provider for 15 years. Staff to Area Agency on Aging on Aging Advisory Council for 20 years

Relevant Work or Volunteer Experience

Organization	Town or Address	Position	Dates
Seniors Council/Area Agency on Aging	Aptos	Planner/AAA Administrator	2005- present
Lift Line		Program Analyst	1995- 1997

Statement of Qualifications: Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

Certification: I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.



Signature

2/2/22

Date

How did you learn about this opportunity?

☐ newspaper

☐ radio

☐ internet

☐ flyer

☐ friend/family member

☐ other

Return Application to:

SCCRTC

Elderly & Disabled Transportation Advisory Committee

1101 Pacific Avenue, Suite 250

Santa Cruz, CA 95060

fax: 460-6178 email: amarino@sccrtc.org

Statement of Qualifications:

I have over 30 years experience in the senior services sector as a Planner and Contracts Manager for the Area Agency on Aging. I also worked in the Lift Line program for two years. As a planner myself, I appreciate the opportunity to review transportation plans and provide feedback. As the senior population continues to grow, maintaining and expanding transportation options for seniors and the disabled is a critical issue.

COMMITTEE APPOINTMENT APPLICATION

Santa Cruz County Regional Transportation Commission (SCCRTC) Elderly & Disabled Transportation Advisory Committee (E/D TAC)

Meetings are scheduled for the second Tuesday of every other month at 1:30 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1101 Pacific Avenue, Suite 250 in downtown Santa Cruz. At least one meeting each year is scheduled for an alternate location. Please refer to the Committee description, bylaws and recruitment process for more information.

If you are interested in serving on this committee, please complete this application, and return it to the Regional Transportation Commission office.

PLEASE TYPE OR PRINT CLEARLY

Name: Lisa Berkowitz

Home address: [REDACTED] [REDACTED] [REDACTED]

Mailing address (if different): _____

Phone: (home) [REDACTED] (business/message) [REDACTED]

E-mail: [REDACTED]

Length of residence in Santa Cruz County: 45 years

Position(s) I am applying for: • Any appropriate position

X CTSA Community Bridges • _____

Previous experience on a government commission or committee (please specify)

SCCRTC Elderly and Disabled Transportation Advisory Committee

Area on Aging Advisory Council

Relevant Work or Volunteer Experience

Organization	Town or Address	Position	Dates
Community Bridges	519 Main St. Watsonville, CA 95076	Meals on Wheels Program Director	7/79- Present

Statement of Qualifications: Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

Since 1976 I have worked in the field of aging. I have worked for organizations whose mission it has been is to address the challenges faced by adults as we age. Maintaining independence is a common component of many of those challenges. Transportation services are a key concern and frequently play a pivotal role in determining the success of many other life choices and decisions. I hope to stay involved in the committee's efforts to ensure that transportation services for seniors and disabled adults remain an important issue in our community.

Certification: I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.



Signature

1-27-2022
Date

Return Application to: SCCRTC
Elderly & Disabled Transportation Advisory Committee
1101 Pacific Avenue, Suite 250,
Santa Cruz, CA 95060
fax: 460-3215 email: amarino@sccrtc.org

Questions or Comments: (831) 460-3200

COMMITTEE APPOINTMENT APPLICATION

Santa Cruz County Regional Transportation Commission (SCCRTC) Elderly & Disabled Transportation Advisory Committee (E&D TAC)

Meetings are scheduled for the second Tuesday of every other month at 1:30 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1101 Pacific Avenue, Suite 250 in downtown Santa Cruz. At least one meeting each year is scheduled for an alternate location. Please refer to the Committee description, bylaws and recruitment process for more information.

If you are interested in serving on this committee, please complete this application, and return it to the Regional Transportation Commission office.

PLEASE TYPE OR PRINT CLEARLY

Name: Jesus Bojorquez

Home address: [REDACTED]

Mailing address (if different): _____

Phone: (home) [REDACTED] (business/message) [REDACTED]

E-mail: [REDACTED]

Length of residence in Santa Cruz County: 7 Years

Position(s) I am applying for: ☐ Any appropriate position

☐ E&D TAC Membership Renewal ☐ _____

Previous experience on a government commission or committee (please specify)

I have been a member of the E&D TAC since 2018 serving as an alternative for the former Community Bridges Lift Line director Kirk Ance; as the new Lift Line Director, I will like to continue to serve Santa Cruz county residents needs by being a member of this committee

Relevant Work or Volunteer Experience

Organization	Town or Address	Position	Dates
Community Bridges Lift Line	545 Ohlone Parkway Watsonville CA 95076	Lift Line Program Director	12/2020 current
Community Bridges Lift Line	545 Ohlone Parkway Watsonville CA 95076	Lift Line Operation Manager	07/2017 to 12/2020
Community Bridges Lift Line	545 Ohlone Parkway Watsonville CA 95076	Lift Line Data Analyst	02/2015 to 07/2017

Statement of Qualifications: Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

Certification: I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.



Signature

1/27/2022
Date

How did you learn about this opportunity?☐ newspaper☐ radio☐ internet☐ flyer☐ friend/family member☐ other**Return Application to:** SCCRTC

Elderly & Disabled Transportation Advisory Committee
1101 Pacific Avenue, Suite 250
Santa Cruz, CA 95060
fax: 460-6178 email: amarino@scrtc.org

Questions or Comments: (831) 460-3200

F:\E&DTAC\MEMBERS\Application\COMMITTEE APPOINTMENT APPLICATION.doc



Dear Member of the committee

My name is Jesus Bojorquez. In recent years, my performance on the committee has been limited to being the alternate for the former Lift Line program director Kirk Ance. Now, as the new Director of the Lift Line program, I would like to continue on the committee and be more active in decisions that affect the elderly and people with disabilities. This group has done wonderful things for the citizens of the county of Santa Cruz. My mission is to contribute my knowledge and experience to continue giving this group of people the freedom and independence they deserve and make our county a place where they can carry out their daily activities like any other citizen without dealing with obstacles that hinder their lifestyle.

Thank you

COMMITTEE APPOINTMENT APPLICATION

Santa Cruz County Regional Transportation Commission (SCCRTC) Elderly & Disabled Transportation Advisory Committee (E&D TAC)

Meetings are scheduled for the second Tuesday of every other month at 1:30 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1101 Pacific Avenue, Suite 250 in downtown Santa Cruz. At least one meeting each year is scheduled for an alternate location. Please refer to the Committee description, bylaws and recruitment process for more information.

If you are interested in serving on this committee, please complete this application, and return it to the Regional Transportation Commission office.

PLEASE TYPE OR PRINT CLEARLY

Name: Daniel L. Zaragoza

Home address: [REDACTED]

Mailing address (if different): _____

Phone: (home) [REDACTED]

(business/message) [REDACTED]

E-mail: [REDACTED]

Length of residence in Santa Cruz County: 48 Years

Position(s) I am applying for:

☐ Any appropriate position

X SC Metro Alternate for Eileen Wagley ☐ _____

Previous experience on a government commission or committee (please specify)

I have served on this committee as an alternate since 2018, and I wish to continue to serve the community, I am also a member of the APTA Access Committee.

Relevant Work or Volunteer Experience

Organization	Town or Address	Position	Dates
Santa Cruz METRO	110 Vernon St. Santa Cruz	Paratransit and Bus operator	2004- 2012
Santa Cruz METRO	110 Vernon St. Santa Cruz	Assistant Paratransit Superintendent	2013- 2018
Santa Cruz METRO	110 Vernon St. Santa Cruz	Operations Manager: Paratransit Division	2018- Present.

Statement of Qualifications: Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

Certification: I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.


Signature1/31/22
Date**How did you learn about this opportunity?**☐ newspaper☐ radio☐ internet☐ flyer☐ friend/family member☒ other**Return Application to:** SCCRTC

Elderly & Disabled Transportation Advisory Committee
1101 Pacific Avenue, Suite 250
Santa Cruz, CA 95060
fax: 460-6178 email: amarino@sccrtc.org

Questions or Comments: (831) 460-3200

I:\E&DTAC\MEMBERS\Application\COMMITTEE APPOINTMENT APPLICATION.doc

*Santa Cruz Metropolitan
Transit District*



January 27, 2022

To: Committee Members.

I would like to continue be a member to the E&D TAC, this committee performs an important role for the elderly and disable people of Santa Cruz County, and I would like to continue to serve. I believe that my knowledge and experience in providing transportation to the Disabled and elderly community, as well as having an understanding of the challenges that they face, can be useful for the committee. We must continue to improve our transportation options and services to give people the independence to access schools, employment, shopping, entertainment, church and their medical providers, etc.

Thank you,

Daniel L. Zaragoza
Operations Manager: Paratransit Division
Santa Cruz Metropolitan Transit District

COMMITTEE APPOINTMENT APPLICATION

Santa Cruz County Regional Transportation Commission (SCCRTC) Elderly & Disabled Transportation Advisory Committee (E&D TAC)

Meetings are scheduled for the second Tuesday of every other month at 1:30 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1101 Pacific Avenue, Suite 250 in downtown Santa Cruz. At least one meeting each year is scheduled for an alternate location. Please refer to the Committee description, bylaws and recruitment process for more information.

If you are interested in serving on this committee, please complete this application, and return it to the Regional Transportation Commission office.

PLEASE TYPE OR PRINT CLEARLY

Name: Veronica Elsea

Home address: [REDACTED]

Mailing address (if different): _____

Phone: (home) [REDACTED] (business/message) business located at home

E-mail: [REDACTED]

Length of residence in Santa Cruz County: 41 years

Position(s) I am applying for: 3rd District Representative

Previous experience on a government commission or committee (please specify)

Served for the past 14 years on E&DTAC, current chair; chaired Pedestrian Safety Work group subcommittee; now chair pedestrian ad-hoc projects subcommittee; Served as charter member of the CalTrans Accessibility Advisory Committee until disbanded by CalTrans; member of the Class IV Bikeway design subcommittee; member of Santa Cruz County voting Accessibility advisory committee; currently serving on Metro Advisory committee, served as chair of and on according to bylaws since 2015; served on Mission Street Widening Task Force 1998-its end in 2002.

Relevant Work or Volunteer Experience (see separate page as I cannot write in the table.)

Organization	Town or Address	Position	Dates
E&DTAC and Pedestrian Safety Work Group (now Pedestrian ad-hoc projects Subcommittee		Chair	2008-2022
CalTrans Accessibility Advisory Committee		Appointed as a result of my work with E&DTAC and PSWG.	2013 to disbandment
Metro Advisory Committee, served many years as chair.		Served many years as chair. Allows me to share information among groups and be a more informed member of both committees.	2013 to present

Statement of Qualifications: Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.
See separate page.

Certification: I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.



Signature

January 27, 2022

Date

How did you learn about this opportunity?

☐ newspaper

☐ radio

☒ internet, already serving on
committee

☐ flyer

☐ friend/family member

☐ other

Return Application to:

SCCRTC

Elderly & Disabled Transportation Advisory Committee

1101 Pacific Avenue, Suite 250

Santa Cruz, CA 95060

fax: 460-6178 email: amarino@sccrtc.org

Questions or Comments: (831) 460-3200

I:\E&DTAC\MEMBERS\Application\COMMITTEE APPOINTMENT APPLICATION.doc

Additional Volunteer Experience:

Voting Accessibility Advisory Committee: helped to include transportation needs of residents as part of the voting process. Helped to train poll workers in the needs of, and best ways of interacting with seniors and those with disabilities.

Soroptimist International of Capitola-by-the-Sea: held many different offices, guided many fund-raising and service projects to successful completion.

American Federation of Musicians: Held several offices, headed contract negotiation team.

Guide Dog Users, Inc: Served as chair of many different committees; organized many presentations, fund raisers and performed administrative and contract tasks.

National Federation of the Blind Accessible medical device task force (now accessible insulin pump task force): currently serve as chair; requires finding and working with several device manufacturers, helping them to understand the needs of consumers who are blind and disabled, more experience working with committees, keeping projects on track, organizing writing and seeing work to its conclusion.

Statement of Qualifications:

During my first term as a member of E&DTAC, I was able to act on one of my main concerns by helping to reinvigorate a pedestrian subcommittee, The Pedestrian Safety Work Group. During my tenure, the PSWG has been very active in securing grants and producing reports and projects related to sidewalk safety and accessibility in Santa Cruz County. We have created a brochure designed to foster better relationships and increased safety among motorists and pedestrians with additional emphasis on the needs and behaviors of those with disabilities. We have also completed a similar brochure for pedestrians and bicyclists. I have chaired this group and given many of its public presentations, as well as participated in workshops as a representative of the PSWG.

As the pedestrian projects ad-hoc subcommittee, I am helping to work with jurisdictions on intersection design, following many different projects and plans in order to help jurisdictions make any changes which benefit pedestrians with disabilities early in the design process, thus saving money and complaints from citizens. I am currently serving as chair of the E&DTAC. I am committed to fostering a welcoming and productive atmosphere during meetings and making a strong effort to balance respect for everyone's time along with thoroughly covering all important items on an often packed agenda.

I strive to work in a respectful manner with RTC staff and to be thorough, professional and true to the committee's desires when writing any correspondence on the committee's behalf. Examples exist in the committee's minutes.

My attendance record has been very good and I believe that my level of participation, in the pedestrian subcommittee, at meetings, and in the public demonstrates my commitment to the issues facing this committee, my ability to do what I say I'll do, and the contributions I've been able to bring to this committee.

I'm very proud of what I've been able to accomplish as a member of the E&DTAC and firmly believe that I could not have made this much of a difference as an individual citizen, without the wisdom, resources and staff working together. I very much look forward to continuing my efforts on behalf of the E&DTAC to improve access throughout Santa Cruz County for all residents, especially for seniors and persons with disabilities.

Thanks for considering my application renewal.

COMMITTEE APPOINTMENT APPLICATION

Santa Cruz County Regional Transportation Commission (SCCRTC) Elderly & Disabled Transportation Advisory Committee (E&D TAC)

Meetings are scheduled for the second Tuesday of every other month at 1:30 p.m. in the Santa Cruz County Regional Transportation Commission conference room, located at 1101 Pacific Avenue, Suite 250 in downtown Santa Cruz. At least one meeting each year is scheduled for an alternate location. Please refer to the Committee description, bylaws and recruitment process for more information.

If you are interested in serving on this committee, please complete this application, and return it to the Regional Transportation Commission office.

PLEASE TYPE OR PRINT CLEARLY

Name: AULIA MORALES

Home address: [REDACTED]

Mailing address (if different): [REDACTED]

[REDACTED]

Phone: (home) [REDACTED] (business/message) [REDACTED]

E-mail: [REDACTED]

Length of residence in Santa Cruz County: N/A

Position(s) I am applying for: ☐ Any appropriate position

☒ COMMITTEE MEMBER E&D TAC


Previous experience on a government commission or committee (please specify)

Relevant Work or Volunteer Experience

Organization	Town or Address	Position	Dates
SANTA CRUZ COUNTY HUMAN SERVICES DEPT	1400 ENCLAVE SANTA CRUZ	DIRECTOR, DIVISION OF ADULT + LONG TERM CARE	11/16/00 - PRESENT
ARMENIA COUNTY SOCIAL SERVICES AGENCY	OAKLAND, CA	DIRECTOR, DIVISION OF ADULT + ADULT PROTECTION	3/1/2002 - 11/15/00

Statement of Qualifications: Please attach a brief statement indicating why you are interested in serving on this committee and why you are qualified for the appointment. If you have served on this committee in the past, please summarize your accomplishments on the committee and indicate which of the committee's potential future endeavors most interest you.

Certification: I certify that the above information is true and correct and I authorize the verification of the information in the application in the event I am a finalist for the appointment.


Signature

2/1/2022
Date

How did you learn about this opportunity?

☐ newspaper
☐ radio
☐ internet

☐ flyer
☐ friend/family member
☒ other

Return Application to:

SCCRTC
Elderly & Disabled Transportation Advisory Committee
1101 Pacific Avenue, Suite 250
Santa Cruz, CA 95060
fax: 460-6178 email: amarino@sccrtc.org

Questions or Comments: (831) 460-3200

I:\AE\DTAC\MEMBERS\Application\COMMITTEE APPOINTMENT APPLICATION.doc

Statement of Qualifications

I am interested in serving on this committee because I have a deep commitment to improving the lives of vulnerable residents in our community. I have spent more than 23 years of my professional career working with older adults and person with disabilities in three different counties: LA County, Alameda County and Santa Cruz County. In my current role in Santa Cruz County I am the Director of Adult and Long Term Care for the Human Services Department. In that role, I oversee Adult Protective Services, In Home Support Services, The Public Authority and the County Veterans Services Office. I am also the Public Guardian for Santa Cruz County. I see through the course of my work how accessible transportation can have a tremendous impact on the community and those with access and functional needs. Accessible transportation is critical in maintaining independence and quality of life for older adults and persons with disabilities. It can help minimize isolation and elder and dependent adult abuse.

I am also working with the County, the cities and local nonprofit leadership to develop a strategic plan for the implementation of the Master Plan on Aging throughout the county. Transportation is a pillar of that plan and the work of this committee is essential to help further those efforts.

Thank you for your consideration.

Best,

Alicia Morales, MA

Director, Adult and Long Term Care Division

1400 Emeline Avenue,

Santa Cruz, CA 95060

Alicia.morales@santacruzcounty.us

Pronouns: she, her, hers

AGENDA: February 8, 2022

TO: Elderly & Disabled Transportation Advisory Committee
FROM: Amanda Marino, Transportation Planner
RE: Preliminary Draft Unmet Paratransit and Transit Needs List

RECOMMENDATION

Staff recommends that the Elderly & Disabled Transportation Advisory Committee provide input into the preliminary draft unmet paratransit and transit needs list.

BACKGROUND

Local sales taxes in the amount of ¼ cent per dollar are collected by the State and allocated to the region according to the Transportation Development Act (TDA). The Regional Transportation Commission (RTC) allocates these funds according to formula as adopted in its Rules and Regulations. The majority of TDA funding is apportioned to the Santa Cruz Metropolitan Transit District. Other amounts are apportioned to the RTC for administration and planning, Community Bridges for specialized transportation, the Volunteer Center for their transportation program and to local jurisdictions for bicycle and pedestrian projects.

TDA statutes require transportation planning agencies allocating TDA funds to local streets and roads, to implement a public process, including a public hearing, to identify unmet transit needs of transit dependent or disadvantaged persons, and determine if unmet transit needs can be reasonably met. TDA statutes also require transportation planning agencies to consult with their designated social services transportation advisory councils to annually identify transit needs. Although the RTC does not allocate TDA funds to local streets and roads, and therefore is not required to perform this analysis, the RTC endeavors to solicit regular input on unmet transit and paratransit needs to provide a useful tool to assess and prioritize needs in the region. The *Unmet Needs List* related to paratransit and transit needs is used to prioritize projects as funds become available.

DISCUSSION

Serving as the social services transportation advisory council, the E&D TAC regularly hears and considers unmet paratransit and transit needs in Santa Cruz County. Unmet paratransit and transit needs are those transportation requests which are not being met by the current public transit system as identified at a public hearing or E&D TAC meeting, a transportation request that has community support, and transportation request that does not duplicate transit services provided publically or privately.

RTC staff recommends that the E&D TAC provide input on the *Preliminary Draft Unmet Paratransit and Transit Needs List* for Santa Cruz County and identify strategies for addressing each of the unmet paratransit and transit needs. RTC

staff also recommends that the E&D TAC identify one or two areas to focus on advancing in 2022.

Schedule for development of *Unmet Paratransit and Transit Needs List*:

- Feb 8 - *Preliminary Draft Unmet Paratransit and Transit Needs List* reviewed by the E&D TAC
- Feb 16 – RTC staff circulates the list to the Santa Cruz Metropolitan Transit District staff, partner agencies and RTC Advisory Committees
- April 12 – *2022 Draft Unmet Paratransit and Transit Needs List* reviewed by the E&D TAC
- April 13- May 6: Outreach for *2022 Draft Unmet Paratransit and Transit Needs List* including public notice of availability and public hearing on RTC website, in local newspapers, to RTC elderly and disabled stakeholders and transportation providers
- May 6 – Public hearing and RTC considers adoption of the *2022 Final Unmet Paratransit and Transit Needs List*

SUMMARY

TDA statutes require transportation planning agencies to consult with their designated social services transportation advisory councils to annually identify transit needs. Although the RTC does not allocate TDA funds to local streets and roads, and therefore is not required to perform an analysis of unmet transit needs, the RTC endeavors to solicit regular input on unmet paratransit and transit needs to provide a useful tool to prioritize needs in the region. RTC staff recommends that the E&D TAC provide input on the *2022 Preliminary Draft Unmet Paratransit and Transit Needs List* for Santa Cruz County and identify strategies for addressing each of the unmet needs.

Attachment 1: 2022 Preliminary Draft Unmet Transit Needs List

I:\E&DTAC\2021\02-2021\Word docs\SR-PrelimDraftUnmetNeeds.docx

2022 Preliminary Draft Unmet Paratransit and Transit Needs List

Prioritization of Need:

H - High priority items are those items that fill a gap or absence of ongoing of service.

M - Medium priority items that supplement existing service.

L - Low priority items should become more specific and then be planned for, as funds are available.

1-3 Graduated scale indicates to what extent the need, if addressed, would: increase the number of individuals who are within a 30-minute transit trip to key destinations; improve safety; support economic vitality by way of decreasing transportation costs; or, improve cost-effectiveness of transportation services.

Strategies:

- Proposals and suggestions to address need, including programs and projects.

General

1. **H1** - Safe travel paths between senior and/or disabled living areas, medical facilities, educational facilities, employment locations, retail centers, entertainment venues, bus stops, and/or railroad crossings.
 - Improve accessibility at and to bus stops - such as, but not limited to, sidewalk and crosswalk improvements connecting destinations frequented by senior and disabled individuals and transit stops such as, but not limited to, those identified in the RTC Safe Paths of Travel Final Report.
 - Secure funding assistance to make Safe Paths of Travel improvements.
 - Expand publicity regarding sidewalk maintenance.
2. **H1** - Transportation services to areas with high concentrations of seniors, disabled and low income individuals.
 - Support alternative transportation programs, such as vanpool programs, serving low income and senior housing areas outside of the transit service area in south county.
 - Explore pilot projects, such as regularly scheduled paratransit trips two-three times per week, to serve residents.
 - Secure funding for taxi voucher programs for senior and low income individuals.
 - Provide affordable and desirable housing for seniors and low income individuals within the existing transit service area.

- Provide incentives for senior and social services and medical and social services providers to be located in existing transit service areas.
 - Support programs that encourage ridesharing to destinations popular with seniors or high concentrations of seniors.
 - Seek volunteer drivers to provide transportation services.
 - Evaluate on-demand transit services.
3. **H3** - Transportation services for low-income families with children, including transportation for people transitioning from welfare to work.
- Support welfare to work programs and training programs.
 - Support transportation programs dedicated to serving low-income families with children.
 - Seek volunteer drivers for transportation family members to visits at detention facilities.
 - Provide taxi vouchers to low income families.
 - Reinstate ride to work programs.
 - Provide youth bus passes to low income households
4. **H1** - Transportation services for caregivers of senior and disabled clients.
- Support programs providing transportation for caregivers to clients.
 - Provide taxi voucher to caregivers.
 - Reinstate ride to work programs.

Paratransit/Specialized Transportation Services

5. **H1** - Coordinated and seamless-to-the-public system of specialized transportation with a Mobility Management Center (central information point, one stop shop).
- Assess feasibility and seek funds for development/start-up of the center, and assess entities already providing information and referral services).
 - Utilize information technology solutions to provide transit information that is accessible to all users.
6. **H1** - Wheel chair accessible vehicles for taxis and transportation network company services
- Monitor the Transportation Network Company (TNC) Access for All program.
 - Evaluate other comparable options to provide services (i.e new companies, subcontract with services equipped with wheelchair vehicles).
 - Provide on demand paratransit service.
7. **M1** - Paratransit service for the people who lost paratransit service due to changes in Santa Cruz Metro ParaCruz program in 2015.

- Support policies that expand ADA mandated paratransit service area.
 - Support programs providing specialized transportation to areas outside the ADA-mandated paratransit service area for a fee or at no cost.
 - Expand taxi voucher program.
8. **H2** – Access to paratransit services on all holidays.
- Extend existing paratransit services to holidays.
 - Support taxi voucher programs.
9. **H2** - Specialized transportation for areas outside the ADA-mandated paratransit service area for medical, non-medical trips.
- Secure funding for taxi voucher programs.
 - Provide affordable and desirable housing for seniors and disabled individuals within ADA paratransit service area.
 - Provide incentives for senior and social services to be located in transit service areas.
 - Support programs providing specialized transportation to areas outside the ADA-mandated paratransit service area for a fee or at no cost.
 - Support continuous funding for transportation to medical services.
 - Seek volunteer drivers to provide transportation services from areas not served by transit or ADA paratransit service.
 - Identify priority origins and destinations outside the ADA service area.
10. **H2**- Free or low-cost paratransit options.
- Provide funding for programs that provide discounted and free paratransit rides.
 - Support programs that provide on-demand ADA accessible rides.
 - Support increased specialized transportation services to low-income and disabled individuals for educational and work opportunities at higher education institutions (UCSC and Cabrillo).
11. **H2** - Direct paratransit and accessible transit connections with neighboring counties- including Monterey (Pajaro), San Benito, Santa Clara and other points north.
- Establish direct inter-regional fixed route accessible transit service.
 - Develop plan to coordinate between agencies providing specialized transportation services in neighboring counties.
 - Support programs providing inter-regional specialized transportation for a fee or at no cost.
 - Establish feeder services to inter-regional accessible transit services.

12. **M1** - Affordable transportation for dialysis and other medical appointments, including 'same day' specialized transportation services for medical trips, on a continuous basis.
 - Support continuous funding for 'same day' transportation to medical services.
 - Support continuous funding for no or low-cost specialized transportation to medical appointments.
 - Increase capacity of existing programs providing transportation to dialysis and other medical appointments.
 - Secure funding for taxi voucher programs.
13. **M2** - Transportation for programs that promote senior and disabled individuals health, safety and independence including, but not limited to, all senior meal sites in the county, the stroke centers, medical facilities, and senior activity centers.
 - Support continuous funding for transportation services to meal sites.
 - Provide transit and paratransit services to medical service centers.
 - Support volunteer drivers to provide transportation services.
14. **M2** – Conduct targeted outreach to seniors, people living with disabilities, and transportation service providers to provide information about transportation options and safety.
 - Provide safe driving and transit information at locations with concentrations of seniors.
 - Support field trips to events by bus ("Mobility Trainer" and "Bus by Choice" models)
 - Annual updates to transportation service providers including, but not limited to, providers included in the Guide for Specialized Transportation about paratransit service options.
15. **L2** - Publicity about existing specialized transportation services including ADA paratransit, non-ADA paratransit, taxi services, Medi-Cal rides and mobility training for people to use regular fixed route buses.
 - Streamline communication activities by establishing a central point of contact within health providers to disseminate information about specialized transportation services.
 - Support continuous funding for communication and outreach activities.
16. **H2** - Volunteer drivers in Santa Cruz County particularly in south-county and San Lorenzo Valley.
 - Expand outreach efforts to recruit drivers and promote services.
 - Support for the Volunteer Center Transportation Program.

17. **M2** - Affordable special care trips and gurney vehicle for medically fragile individuals and those needing "bed to bed" transportation.
 - Provide vouchers for specialized care trips.
 - Identify a service provider for gurney trips and assist in procurement of a vehicle for services.
 - Partner with assisted living and hospice care to provide services.
 - Publicize availability of services, if available.
18. **M3** - Ongoing provision of ADA Paratransit certification, provided by Santa Cruz Metro, at group facilities.
 - Provide on-site services to reach a greater number of individuals.
19. **M3** - Specialized transportation services for people living with a cognitive impairments, dementia or mental health diagnosis.
 - Provide on demand transportation services for people living with a mental health diagnosis.
 - Provide services designated to assisting people with mental illness navigate transit and paratransit eligibility requirements.
20. **L1** - Specialized transportation for 'same day' low cost non-medical trips.
 - Expand taxi voucher program.
 - Support "on-call" volunteer drive programs.
21. **L3** - Anticipate growing demand for services by projecting funding needs for specialized transportation (including fixed route, ADA and non-ADA Paratransit) to provide transportation services to the senior population expected to increase over the next 15 to 30 years.
 - Identify funding needs for paratransit over a 15-30 year horizon.
 - Designated funding source for paratransit service.
22. **M1** - Provide increased UCSC on-campus paratransit service between campus to campus destinations.
 - Increase existing UCSC specialized transportation services to supplement increased demand.

Paratransit/Specialized Transportation Capital

23. **H1** - ParaCruz operating facilities.
 - Acquire and develop permanent operations and maintenance facility for ParaCruz to reduce operating cost.
 - Increase funding opportunities for paratransit capital projects.

24. **M2** - Consolidated Transportation Services Agency operating facilities.
- Increase funding opportunities for paratransit capital projects.
25. **H2** - Paratransit vehicle replacements.
- Increase funding opportunities for paratransit capital projects including funding for electric vehicles and/or zero emission vehicles.
 - Take measures to include electric vehicles as option for purchase in the Section 5310 grant program.
26. **H2** – Electric Vehicle Charging Stations
- Support funding for electric vehicle charging infrastructure.

Provide an electric vehicle emergency preparedness plan that includes battery storage, vehicles, and facilities.

Transit Services

27. **H1** – Greater frequency and span of transit service in densely populated areas with a mix of land uses.
- Increase service level between downtown Santa Cruz and Capitola Mall Transit Center through the Live Oak corridor.
 - Enhance service on Mission Street.
 - Extend transit service hours later in the evening and early in the morning serving Cabrillo College and commercial centers of Santa Cruz/Live Oak/ Watsonville.
 - Enhance service to employment entities.
 - Enhance service on Scotts Valley Drive.
28. **H1** – Greater evening frequency and span of transit service in coverage-oriented areas, in keeping with METRO service standards.
- San Lorenzo Valley Route 35 variants (Mt. Store and Country Club)
 - Local Watsonville services
 - La Selva Beach
29. **M1** – More transit service to UCSC.
- Increase weekend and weekday UCSC service.
 - Increase service to UCSC campus.
 - Increase service to the University of Santa Cruz employment center in Scotts Valley.
30. **H1** - More interregional and cross county transit services.
- Increase Hwy 17 weekend service frequency.
 - Provide transit service from Santa Cruz County to Los Gatos.
 - Provide direct transit service to San Jose Airport.
 - Enhance Monterey County to Santa Cruz County service.

- Support for an integrated transit network, which includes transit services on a dedicated transit facility on the rail right-of-way consistent with the Unified Corridor Investment Study and the Transit Corridor Alternatives Analysis.
 - Provide transit service from San Lorenzo Valley to Los Gatos.
 - Provide direct transit connection between Live Oak and San Jose Diridon Station.
31. **M1** – Free and low-cost transportation options, including fixed-route transit services.
- Support programs that provide transportation services, including, but not limited to bus services, for a reduced or no fee.
 - Seek volunteer drivers to provide transportation services.
 - Support programs that allow seniors and disabled individuals to ride free during designated time periods.
32. **M2** – More transit service between primary destinations in Santa Cruz County.
- Provide service between Capitola Mall and Cabrillo.
 - Expand transit service to new residential and commercial areas in Watsonville.
 - Improve north - south transit connections (ex. Soquel Ave/Drive \ to coastal communities).
 - Support for an integrated transit network, which includes transit services on a dedicated transit facility on the rail right-of-way consistent with Unified Corridor Investment Study and the Transit Corridor Alternatives Analysis.
 - Provide single trip service.
33. **M2** - More transit service to facilities providing medical, health and other social services.
- Provide transit service to medical facilities.
 - Provide medical, health, and social services within the existing transit service area.
34. **M2** - Access to transportation services on all holidays.
- Provide regular Santa Cruz Metro service on holidays.
 - Support taxi voucher programs.
 - Support volunteer transportation services.
35. **M2** - Easier and faster transit trips system wide.
- Enhance connections through increasing the span and frequency of service.

36. **M2** - Faster run times on transit routes.
- Investigate opportunities for transit priority lanes and signal priority.
 - Pursue right turn pockets for bypass lanes for buses service and transit priority on Soquel Ave/Drive and Freedom consistent with the Unified Corridor Investment Study.
 - Consider direct services between more locations, reducing need for transfers.
37. **M2** - Intra-community service in Santa Cruz County communities.
- Develop San Lorenzo Valley circulator.
 - Develop Scotts Valley circulator.
 - Investigate need for intra-community and neighborhood transit services
 - Consider partnerships with ride-hail and/or taxi services for first/last mile connections.
38. **L2** - Transit service to major tourist destinations.
- Provide transit service to Waddell Creek and North Coast and Highway 17 direct service to Boardwalk on weekends.
39. **L2** - Commuter transit service.
- Extend Highway 17 service to Watsonville, or improve connections between Watsonville-Santa Cruz service and Highway 17 service.
 - Provide faster commute option for transit riders between SLV and Santa Cruz.
40. **L3** - Special event services.
- Establish program to coordinate with Santa Cruz Visitor Center and partner agencies to provide special event services.

Transit Capital

41. **M1** – Bus stops.
- Provide ADA compliant bus stops.
 - Prioritize bus stop improvements and shelter replacement based on high usage by seniors and people with disabilities.
 - Install braille and raised numbers on bus signage at bus stops indicating which bus routes are being offered at each stop (or a technology-based way finding alternative).
 - Provide the ability to lower the height of braille for wheelchair access.
 - Provide benches and increased lighting at bus stops and connecting crosswalks.

42. **M1** – Maintenance of existing transit facilities.
- Support funding for maintenance of bus stops, parking lots, transit centers, buildings.
43. **H1** – Bus replacement: Replace buses beyond useful life as needed including buses, including buses providing rural service.
- Support funding for transit capital improvements.
 - Support funding for electric vehicle bus replacements and electric vehicle charging stations.
44. **H1** - Transit station improvements.
- Investigate options for renovation or redevelopment of Santa Cruz Metro Center.
 - Coordinate improvements to Capitola Transit Center with Capitola Mall ownership.
45. **H2** - Faster transit travel times.
- Installation of transponders on all buses for signal priority on major corridors improving traffic flow, reducing travel time, and improving on-time performance.
 - Support and seek funding for bus on shoulder on Highway 1.
46. **H1** – Dedicated transit facilities.
- Right-of-way improvements and stations along Santa Cruz Branch Rail Line if a bus rapid transit (BRT) or rail service is developed consistent with the Unified Corridor Investment Study and the Transit Corridor Alternatives Analysis.
 - Multimodal transfer facilities at stations along the Santa Cruz Branch Rail Line if BRT or rail service is developed.
47. **H3** - New equipment to assist with real-time operations, security, scheduling and planning.
- Automated Vehicle Location (AVL) System to provide better monitoring of on-time performance and more accurate data reporting.
 - Automatic Passenger Counting (APC) system to make mandatory reporting more efficient and improve data for service planning.
 - Install audio and video surveillance system for all buses.
 - Install audio and video surveillance system for all buses
 - Electronic fare payment for more convenient payment options and to speed up boarding.
 - Modernize planning and scheduling software for more efficient service planning and better community outreach.

48. **M1**– More multimodal connections to transit.
- Construct park and ride lots in strategic locations along inter-city routes that lack adequate feeder service.
 - Bike lockers and/or bike share stations at key locations to facilitate first/last mile of travel.
49. **M3** - Wifi expansion on buses.
- Install wifi equipment at all facilities and on all buses.
 - Partner with private companies to provide wifi.

i:\e&dtac\unmet\2021\drafts\2021 final draft unmet needs list.docx

TO: Elderly and Disabled Transportation Advisory Committee
FROM: Amanda Marino, Transportation Planner
RE: City of Santa Cruz Regional Bike Share Program

RECOMMENDATION

Staff recommends that the Elderly and Disabled Transportation Advisory Committee (E&D TAC) receive follow up information from the City of Santa Cruz regarding the implementation of the Regional Electric Bike Share Program.

BACKGROUND

The City of Santa Cruz Public Works Department, in partnership with the cities of Capitola and Watsonville, County of Santa Cruz, UCSC and Cabrillo College, have released a Request for Proposals for a Regional Electric Bike Share Program.

DISCUSSION

Members of the Elderly and Disabled Transportation Advisory Committee (E&D TAC) have expressed concern about bikes parked on sidewalks and other locations that obstruct pedestrian access.

City Staff attended the November and December 2019 E&D TAC meetings to discuss the City of Santa Cruz Bike Share Program and heard the concerns expressed by E&D TAC members. While more detail on the discussion is included in the minutes, the following are the main points discussed by E&D TAC members:

- Individuals with disabilities have expressed safety concerns with JUMP bikes parked on the sidewalk
- Individuals with visual impairments have difficulty navigating around JUMP Bikes parked on the sidewalk
- Individuals using wheelchairs have found JUMP bikes blocking pedestrian access
- Individuals need to be able to report problem JUMP bikes and receive a timely response
- JUMP bike users should receive more robust parking instructions to reduce conflicts with individuals with disabilities
- JUMP bike users parking inappropriately should be notified if they have violated parking rules and receive penalties with multiple infractions
- More education about JUMP bike parking safety is desired and could be combined with other education programs such as the Street Smarts program

SUMMARY

The E&D TAC requested an opportunity to provide input on the City of Santa Cruz in partnership with the Cities of Capitola and Watsonville, the County of Santa Cruz, UCSC, and

Cabrillo College Electric Bike Share Program. In the past, the E&D TAC has provided input on bike parking including pedestrians and individuals with disabilities navigating the bikes. The committee is requesting to engage in a collaborative process to identify and implement actions that prevent bike-share bikes from creating obstacles and hazards for pedestrians early on in the development process of this new program.

TO: Elderly and Disabled Transportation Advisory Committee
FROM: Amanda Marino, Transportation Planner
RE: City of Santa Cruz TDA funded Rectangular Rapid Flashing Beacons

RECOMMENDATION

Staff recommends that the Elderly and Disabled Transportation Advisory Committee (E&D TAC) receive follow up information from the City of Santa Cruz regarding a letter ([Attachment 1](#)) sent concerning the TDA funded Rectangular Rapid Flashing Beacons (RRFM).

BACKGROUND

At the E&D TAC's November 2019 meeting, the committee recommended approval to the RTC of the City of Santa Cruz's Article 8 FY 19/20 Transportation Development Act allocation claims for crosswalk improvements. At the E&D TAC's February 2020 meeting the committee recommended approval to the RTC of the City's request to deduct from the City's fund balance \$49,746 to improve 22 unsignalized crosswalks on Soquel Ave., Seabright Ave., North Branciforte Ave., King St., Bay St., Alta Ave., Bethany Curve, and East Cliff Dr. by adding crosswalk markings, signs, access ramps, streetlights, sidewalk bulb-outs, and rapid-flashing beacons, and Rectangular Rapid Flashing Beacons (RRFM).

DISCUSSION

The motion to recommend approval included that the City of Santa Cruz return to the E&D TAC with information about what type of push button equipment will be installed to activate the Rectangular Rapid Flashing Beacons at crossings before construction moves forward to ensure that push buttons are accessible by visually impaired individuals.

Members of the E&D TAC have expressed concern about the accessibility of the installed Rectangular Rapid Flashing Beacons (RRFM) and sent a letter ([Attachment 1](#)) to the City of Santa Cruz regarding the safety hazards especially by those who are blind or visually impaired.

SUMMARY

The E&D TAC requested that the city of Santa Cruz provide updated information on the installation of TDA funded Rectangular Rapid Flashing Beacons (RRFM) as well as plans to make this vital safety equipment accessible to all pedestrians, including those who are blind or visually impaired.

[Attachments](#)

1. Letter to the City of Santa Cruz Article 8 Transportation Development Act Allocation Request, approved February 2020



SANTA CRUZ COUNTY REGIONAL TRANSPORTATION COMMISSION

1523 Pacific Ave., Santa Cruz, CA 95060-3911 • (831) 460-3200 FAX (831) 460-3215 EMAIL info@sccrtc.org

October 4, 2021

Re: City of Santa Cruz Article 8 Transportation Development Act Allocation Request, approved February 2020

To: City of Santa Cruz Public Works Dept.,

The Elderly & Disabled Transportation Advisory Committee (E&D TAC) advises the Santa Cruz County Regional Transportation Commission (RTC), the Santa Cruz Metropolitan Transit District (Metro), and other service providers on transportation needs for people with disabilities, seniors and persons with limited means.

At its meeting on November 5, 2019, the city requested that this committee recommend approval to the RTC of the City of Santa Cruz's Article 8 FY 19/20 Transportation Development Act allocation claims for crosswalk improvements; and Recommend to the Regional Transportation Commission approval of the City's request to deduct from the City's fund balance \$49,746.

Following the presentation from the city of its proposal to improve 22 unsignalized crosswalks on Soquel Ave., Seabright Ave., North Branciforte Ave., King St., Bay St., Alta Ave., Bethany Curve, and East Cliff Dr. by adding crosswalk markings, signs, access ramps, streetlights, sidewalk bulb-outs, and rapid-flashing beacons, and Rectangular Rapid Flashing Beacons (RRFM), the committee expressed its appreciation for Santa Cruz's strong commitment to active transportation by planning projects which are designed to increase pedestrian safety and comfort. However, during the discussion, the committee expressed serious concerns regarding the apparent inaccessibility of the flashing beacons to pedestrians who are blind or visually impaired. The committee first acknowledged the advantages offered by these beacons:

- Less expensive than installing actual signals
- Do not require power
- Make pedestrians more visible to vehicle drivers
- Encourage pedestrians to make safe crossing choices
- Accommodate pedestrians when signalized intersections are very far apart
- Allow for free flow of vehicular traffic when no pedestrians are present

The committee next acknowledged the serious safety hazards faced by those who are blind or visually impaired when inaccessible beacons are installed:

- The pushbuttons look and feel the same as those which activate signalized pedestrian crossings.

Problem: Normal pedestrian behavior is different when confronting a beacon. Blind pedestrians who cross at a signalized intersection, wait for a complete stop of cross-traffic or for parallel traffic to move. Since a flashing beacon displays a light, some

traffic may stop. If the blind pedestrian waits for normal signal behavior from vehicles, he or she may cross when the beacon light is no longer flashing, thus facing a greater safety risk.

- Vehicle drivers will learn that they only need to stop when the light is visible.
Problem: If the blind pedestrian doesn't know the beacon is present, can't find the button or thinks it's a signal, they risk crossing when vehicle drivers do not expect a pedestrian. This increases the risk of serious injury or worse to the pedestrian.
- Beacons are being installed in lieu of signalized crossings.
Problem: Pedestrians who are blind or visually impaired are placed at greater risk when crossing at intersections with heavy or fast traffic, where the free flow of vehicular traffic is deemed important. They may either be forced to walk a long distance to find a known signalized crossing or may take a chance and cross with no assistance from the beacon at all, placing them at great risk of serious injury.
- Many new types of beacons are being installed.
Problem: If there is no message explaining that a beacon has a red phase, flashes immediately, etc., not only does the blind or visually impaired pedestrian lack the knowledge that a beacon is present instead of a signal, but their behavior may be inappropriate for the type of beacon which is installed. Vehicle drivers do not generally realize that a pedestrian may not know the state of the hardware and tend to expect all pedestrians to behave according to its instructions.
safety risks for pedestrians who are blind can come not only from an encounter with a vehicle, but from others yelling instructions and startling or frightening the pedestrian.
- Pushbuttons can be challenging or impossible to locate.
Problem: If the pedestrian who is blind or visually impaired is not aware of, or can't find the button, such pedestrians may likely assume that it is not a reasonable place to cross and be forced to walk out of their way to reach a destination. It might also be assumed that the crossing is a four-way stop or should be treated as it was before beacons were installed, presenting a safety hazard because the expectation of vehicle drivers has changed. If pedestrians who are sighted do not find an obvious button in close proximity to the crossing, they may simply ignore it, depriving them of the safety these beacons are designed to provide.

At the conclusion of the discussion, the committee passed a motion, approving the TDA claim but asking that the flashing beacons be made accessible to all pedestrians and requesting an update from the city as to the type of equipment being installed. As part of the presentation of the TDA claim on the RTC's February 2020 consent agenda, the staff recommendation read as follows: "The Elderly and Disabled Transportation Advisory Committee and staff recommend that the RTC approve by resolution (Attachment 1) the City of Santa Cruz TDA claim for citywide pedestrian crossing improvements and deduction of the FY18/19 overallocation. The E&D TAC further recommends that the City of Santa Cruz return to the E&D TAC with information about what type of push button equipment will be installed to activate the Rectangular Rapid Flashing Beacons at crossings before construction moves forward to ensure that push buttons are accessible by visually impaired individuals."

At present, intersection improvements have been completed and no accessible beacons have been installed. Furthermore, no one from the city has returned to the E&D TAC "with information about what type of push button equipment will be installed to activate the Rectangular Rapid Flashing Beacons at crossings before construction moves forward to ensure that push buttons are accessible by visually impaired individuals."

The E&D TAC is requesting that the city of Santa Cruz revisit the installation of accessible flashing beacons covered in this TDA claim and return to the committee at either the November 2021 or the February 2022 meetings to provide updated information on current beacons as well as plans to make this vital safety equipment accessible to all pedestrians, including those who are blind or visually impaired. Please feel free to send any responses or questions to the committee's RTC staff, Amanda Marino at amarino@sccrtc.org.

Thank you for your prompt response as we look forward to working with you to provide increased safety for all pedestrians in Santa Cruz.

Sincerely,

A handwritten signature in dark ink, appearing to read "Veronica Elsea", is written over a light gray rectangular background.

Veronica Elsea, Chair
SCCRTC Elderly and Disabled Transportation Advisory Committee

AGENDA: February 8, 2022

TO: Elderly and Disabled Transportation Advisory Committee (E&D TAC)

FROM: Tommy Travers, Transportation Planner

The Discussion information was provided by city of Santa Cruz engineering staff

RE: City of Santa Cruz's Chestnut Storm Drain and Pavement Rehabilitation Project

RECOMMENDATION

Staff recommends that the E&D TAC receive information and provide input regarding the Chestnut Street Storm Drain Replacement and Pavement Rehabilitation Project.

BACKGROUND

The city of Santa Cruz seeks to make improvements to Chestnut Street, based on a failing storm drain which runs along the west side of the street as well as poor pavement condition. Funding for the project comes from developer fees of recent nearby development, County funds, and Regional Surface Transportation Program Exchange funds allocated for the project by the Santa Cruz County Regional Transportation Commission.

In fall 2021, Santa Cruz applied for \$1.1 million for this project from the RTC's consolidated Regional Transportation Improvement Program of various state, federal, and regional transportation funds. In December, the RTC allocated \$1 million for this project.

One of the Committee's roles is to provide input on local projects receiving RTC funding.

DISCUSSION

This project will replace approximately 1,700 feet of the failing storm drain pipe and construct new manholes and catch basins. The project location is between Laurel Street and Green Street.

The current condition of the existing asphalt and non-compliant curb ramps along this corridor are disruptive to motorists, bicyclists, and pedestrians. The impact to pedestrians is especially important because of the proximity to Santa Cruz High School. Also, it has been observed that motorists tend to encroach into the bike lanes to avoid the damaged road sections adjacent to the railroad tracks that bisects the street, creating potential conflicts with bicycles.

The project will enhance safety for all roadway users, but in particular pedestrians and bicyclists. The proposed improvements include:

- Removal of non-ADA compliant curb ramps and installation of ADA compliant curb ramps along Chestnut Street from Laurel Street to Green Street.
- Removal and replacement of asphalt pavement the width of Chestnut Street from Laurel Street to Green Street.
- Installation of new striping that will help reduce any potential conflict between motorists, bicyclists, and pedestrians. These striping improvements include, but are not limited to, shared lane markings, high visibility yellow crosswalks in school zones, and bike lane buffers where possible.

Select sheets of the preliminary design plans are included as ATTACHMENT 1.

The project construction is estimated at \$2,150,000, including construction management costs, and includes funding from Measure D, Gas Tax, and the city General Fund. Staff anticipates that the project will be constructed during summer 2022.

SUMMARY

The city of Santa Cruz is seeking the input of the Committee prior to construction of the Chestnut Street Storm Drain Replacement and Pavement Rehabilitation Project.

ATTACHMENTS

1. Select sheets of preliminary project design plans

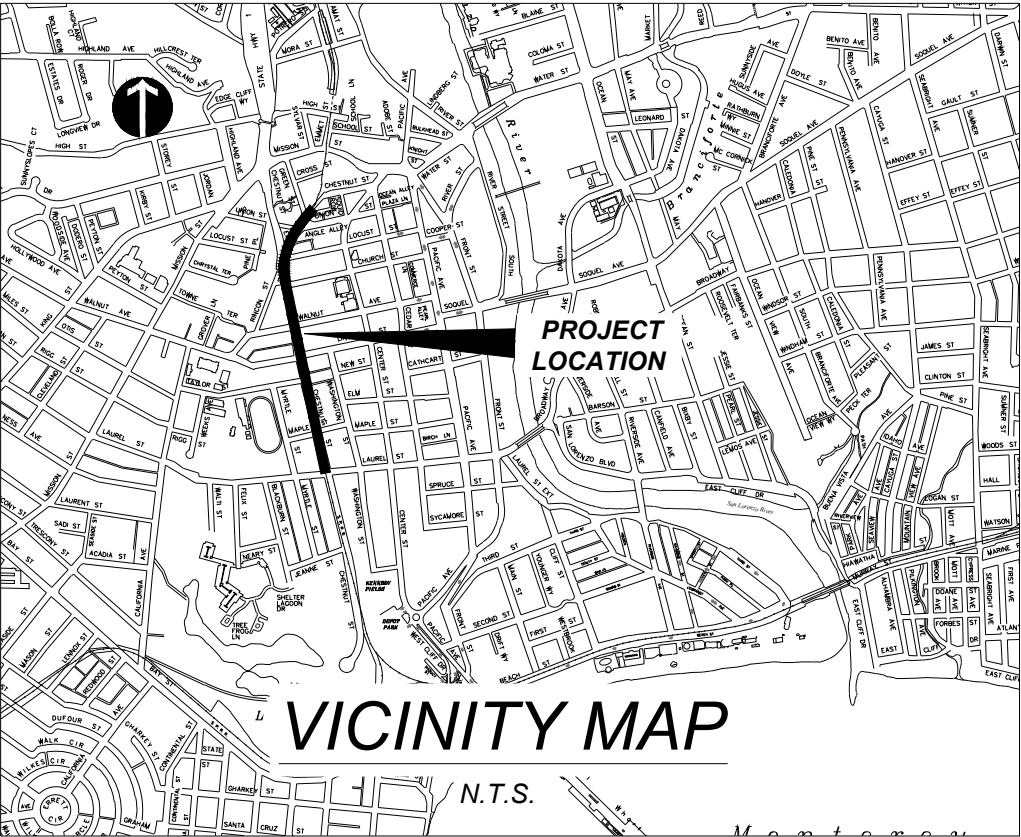
S:\Bike\Committee\Agenda Packets\BC2022\1. February\Chestnut SR.docx



PUBLIC WORKS DEPARTMENT

CHESTNUT STREET STORM DRAIN REPLACEMENT AND PAVEMENT REHABILITATION

PROJECT NO C401505



SHEET INDEX

- 1) COVER SHEET
- 2) CHESTNUT ST OVERLAY
- 3) CHESTNUT ST OVERLAY
- 4) CHESTNUT ST OVERLAY
- 5) CHESTNUT ST STRIPING PLAN
- 6) CHESTNUT ST STRIPING PLAN
- 7) CHESTNUT ST STRIPING PLAN
- 8) DETAILS CROSS SECTIONS
- 9) CALTRANS STANDARD X-WALK DETAILS
- 10) STORM PLAN AND PROFILE: SECTION 1
- 11) STORM PLAN AND PROFILE: SECTION 2
- 12) STORM PLAN AND PROFILE: SECTION 3
- 13) DETAILS

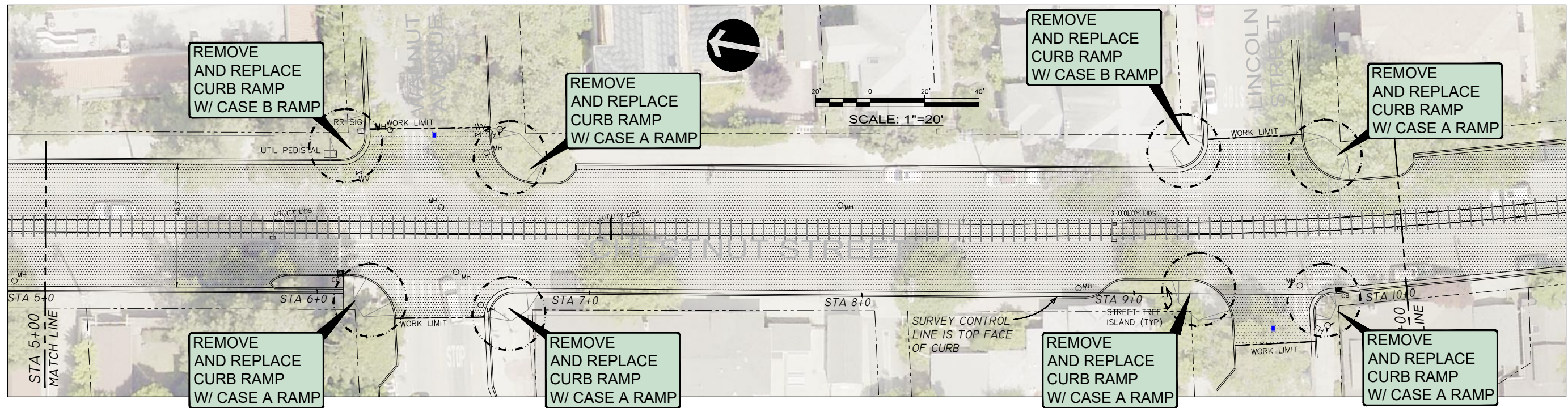
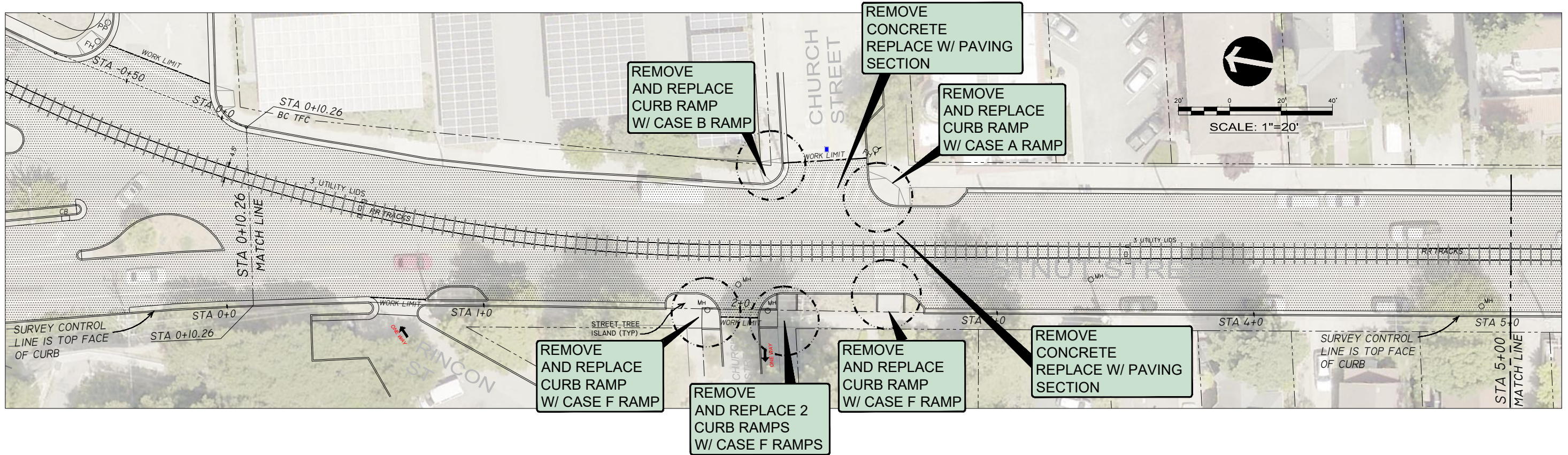
APPROVED BY:

R.C.E. NO. 79670

ASSISTANT DIRECTOR / CITY ENGINEER
CITY OF SANTA CRUZ



REFERENCES	DATE	JANUARY 2022	SCALE	AS SHOWN
FIELD BOOK:	DRAWN	S METZ	SHEET	1 OF 13
#	DESIGN	STAFF	VAULT NO.	
DRAWING #:	CHECKED	SUPV.	#	
#				



REVISIONS



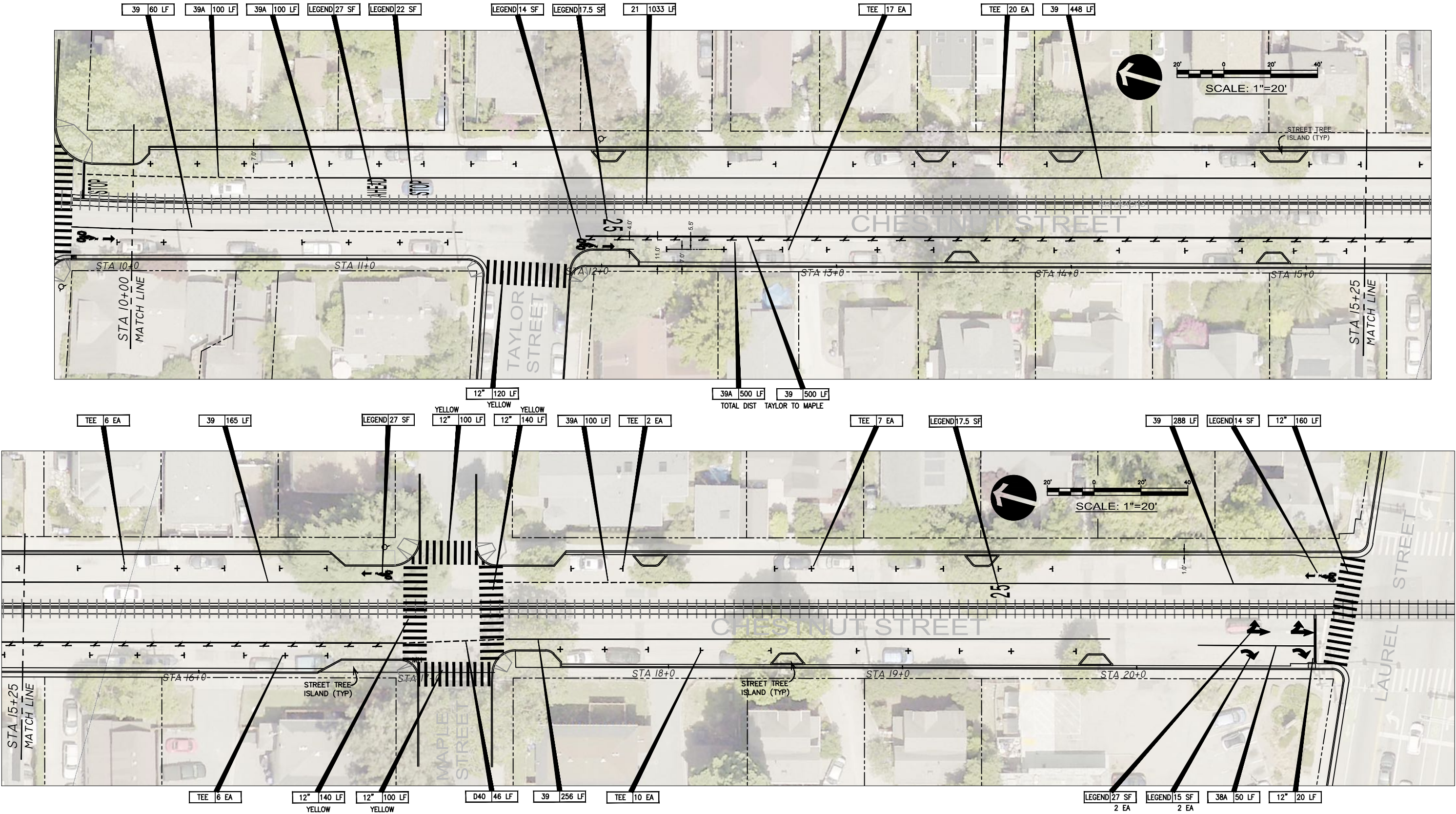
CITY OF
SANTA CRUZ
PUBLIC WORKS DEPARTMENT
809 Center Street, Room 201
Santa Cruz, CA 95060

SPRING 2022 CHESTNUT STREET PAVING PLAN

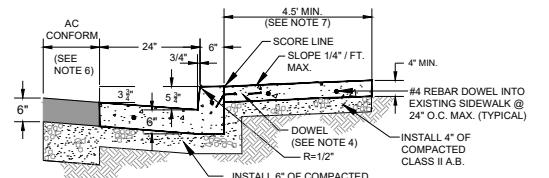
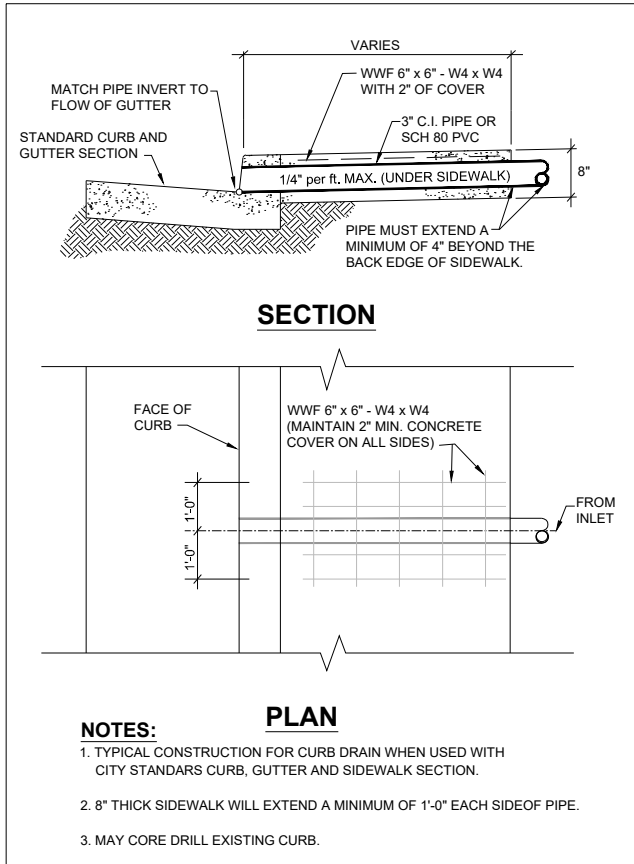
REFERENCES
FIELD BOOK:
DRAWING #:

DATE	JANUARY 2022	SCALE	1"=20'
DRAWN	S.METZ	SHEET	3 OF 13
DESIGN		Vault No.	
CHECKED			

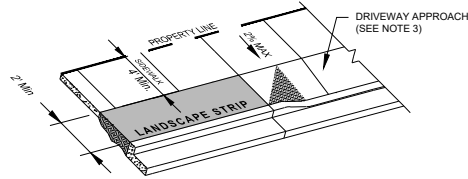
Project: Santa Cruz, California, City of Santa Cruz, Public Works Department, 809 Center Street, Room 201, Santa Cruz, CA 95060. Project No. 2021-001. Drawing No. 72. Scale: 1"=20'. Date: January 2022. Designer: S. Metz. Checker: S. Metz. Engineer: N. Nguyen. State of California, Civil Engineer, No. C 79670. Project Description: Spring 2022 Chestnut Street Striping Plan. This drawing shows the proposed striping for Chestnut Street, including lane markings, crosswalks, and street tree islands. The plan includes stationing from STA 10+00 to STA 20+00. Key features include Taylor Street, Maple Street, and Laurel Street intersections. The drawing is a plan view showing the layout of the street and the proposed striping. It includes a north arrow and a scale bar. The drawing is a plan view showing the layout of the street and the proposed striping. It includes a north arrow and a scale bar.



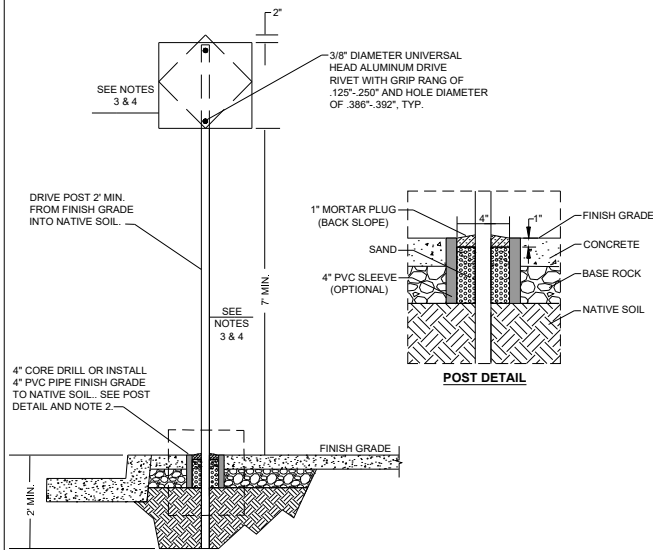
	REVISIONS		 <p>CITY OF SANTA CRUZ PUBLIC WORKS DEPARTMENT 809 Center Street, Room 201 Santa Cruz, CA 95060</p>	<p>SPRING 2022 CHESTNUT STREET STRIPING PLAN</p>		REFERENCES FIELD BOOK: DRAWING #:	DATE	JANUARY 2022	SCALE	1"=20'
	DRAWN	S. METZ					SHEET	7 OF 13		
	DESIGN						VAULT NO.			
	CHECKED									



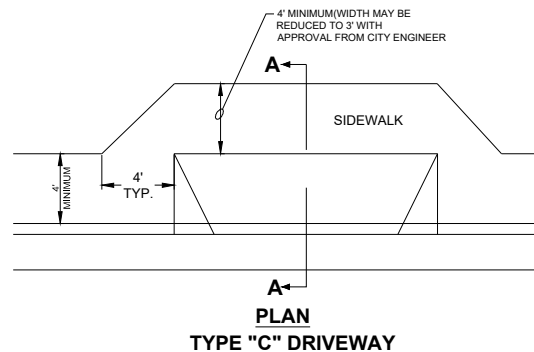
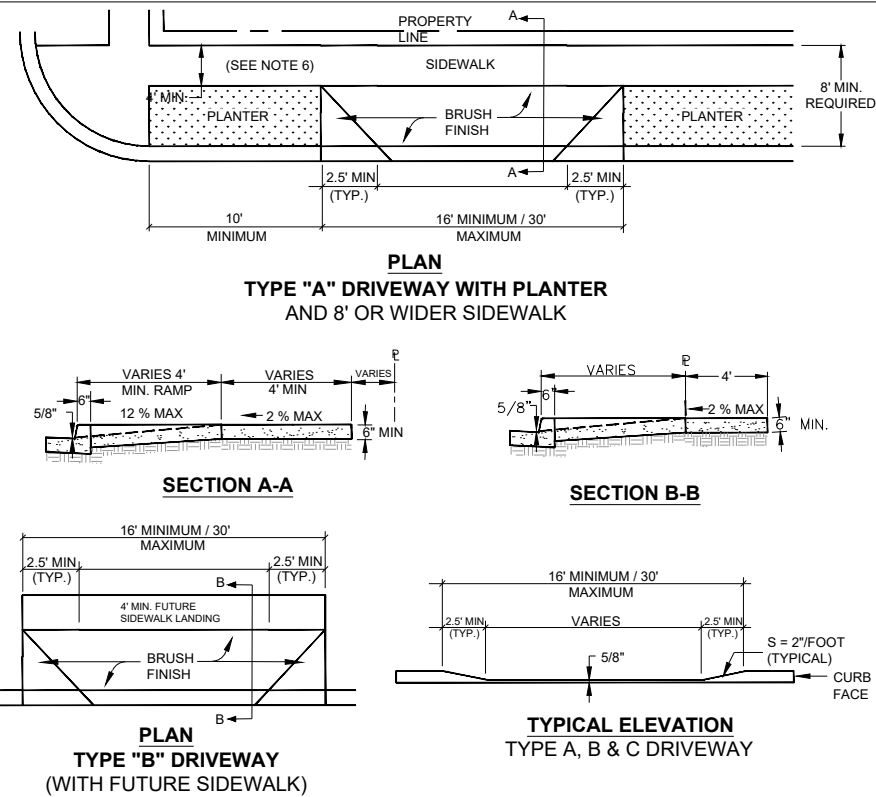
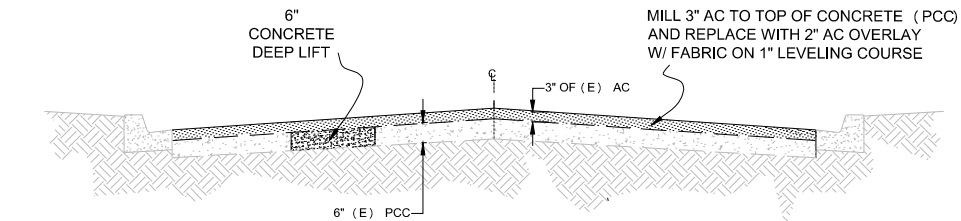
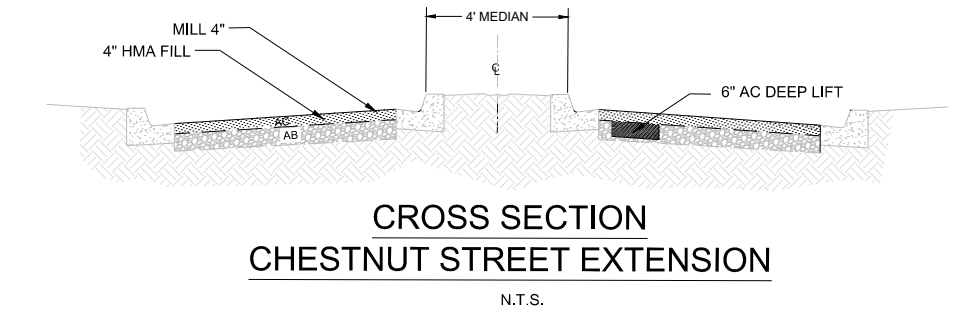
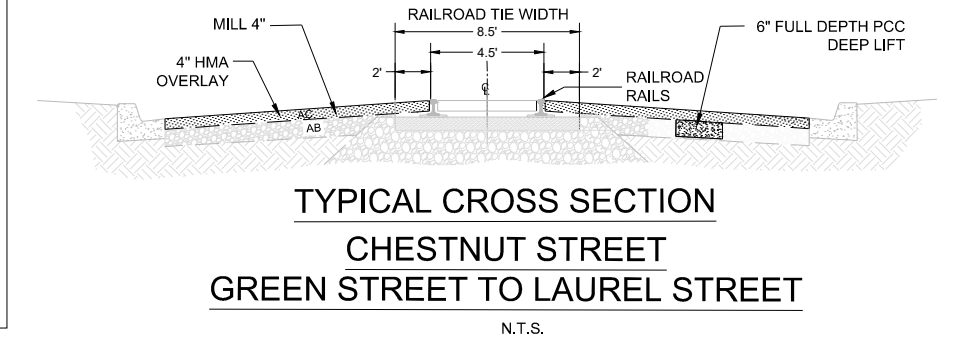
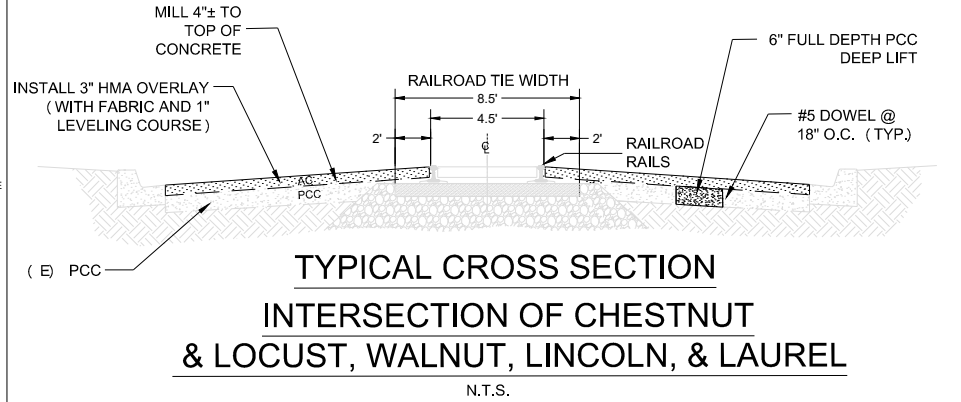
- NOTES:**
1. ALL CONCRETE TO BE SIX SACK, CLASS "A" PLANT MIXED PCC.
 2. COMPACT SUBBASE SOIL TO 95% RELATIVE COMPACTION AT OPTIMUM MOISTURE CONTENT TO A DEPTH OF 6".
 3. STANDARD CURB, GUTTER AND SIDEWALK SHALL BE A MONOLITHIC POUR UNLESS CURB AND GUTTER ARE EXISTING.
 4. IF ANY CURB, GUTTER OR SIDEWALK ARE EXISTING, DOWEL INTO EXISTING CONCRETE EVERY 24". USE 18" LONG #4 DOWELS, INSERT 4" INTO CURB. BEND DOWEL AT BACK OF CURB AS SHOWN.
 5. SCORE SIDEWALK EVERY 4 FT. PLACE EXPANSION JOINTS EVERY 60 FEET WITH DEEP JOINTS EVERY 12 FT.
 6. WIDTH OF A.C. CONFORM SHALL BE A MINIMUM OF 2 FEET. A.C. CONFORM SHALL BE A MINIMUM OF 6" THICK ON TOP OF 6" OF CLASS II A.B.
 7. CITY ENGINEER MAY REQUIRE MODIFICATIONS OF MINIMUM WIDTH TO CONFORM TO SURROUNDING NEIGHBORHOOD.



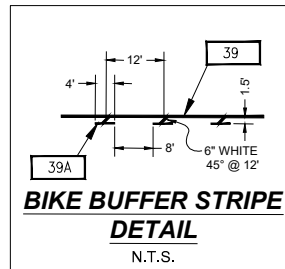
- NOTES:**
1. PROPERTIES THAT HAVE A 6 FT. OR GREATER PROPERTY LINE SETBACK FROM THE STREET WILL BE REQUIRED TO INSTALL A LANDSCAPE STRIP BETWEEN THE BACK OF CURB AND SIDEWALK.
 2. SLOPE SIDEWALK AND LANDSCAPING STRIP TOWARD CURB AT 1/4" / FT. (2%) MAX.
 3. FOR DRIVEWAY APPROACH CONSTRUCTION SEE CITY DETAIL 9 OF 23 TYPE "A" DRIVEWAY WITH PLANTER.



- NOTES:**
1. SIGN POST SHALL BE 2" X 2" UNISTRUT "TELESPAR" PRE-DRILLED SQUARE TUBING OR APPROVED EQUAL.
 2. CONTRACTOR HAS THE OPTION TO:
(A) 4" CORE DRILL TO NATIVE SOIL AFTER POURING CONCRETE OR
(B) INSTALL 4" PVC SLEEVE PRIOR TO POURING CONCRETE.
 3. 4" MINIMUM CLEARANCE BETWEEN EDGE OF POST AND BACK OF WALK. POST MAY BE INSTALLED AT BACK OF WALK IF 4" CLEARANCE CAN NOT BE MET.
 4. INSTALL POST SO THAT THE EDGE OF SIGN IS SET BACK 6" FROM FACE OF CURB. SIGN MAY BE MOUNTED OFF CENTER ON SIGN POST TO PROVIDE 4" POST CLEARANCE AND/OR 6" EDGE OF SIGN SET BACK.
 5. LOW PROFILE ALTERNATIVE IS 2" BELOW GRADE WITH 5" TO TOP OF SIGN (NOT ALLOWED IN PEDESTRIAN AREAS).



- NOTES:**
1. SIDEWALK AREA ADJACENT TO DRIVEWAY APPROACH SHALL HAVE A $\leq 2\%$ CROSS SLOPE
 2. ALL CONCRETE TO BE SIX SACK, CLASS "A" CONCRETE.
 3. ALL CONCRETE SHALL BE REINFORCED WITH #4 BARS AT 18" O.C. BOTH WAYS OR WELDED WIRE MESH 4"x4" W4XW4
 4. MAINTAIN 2" OF COVER FOR REINFORCEMENT.
 5. NATIVE SUBBASE SHALL BE COMPACTED. IF MATERIAL IS SILT OR CLAY IT SHALL BE "FIRM" ACCORDING TO ASTM. OTHERWISE EXCAVATE TO 12" BELOW FINISH GRADE AND REPLACE WITH CLASS II AGGREGATE BASE AND COMPACT.
 6. WIDTH MAY BE REDUCED TO 3' WITH APPROVAL FROM THE CITY ENGINEER.



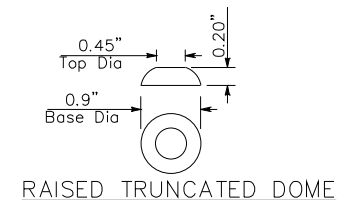
REVISIONS



CITY OF
SANTA CRUZ
PUBLIC WORKS DEPARTMENT
809 Center Street, Room 201
Santa Cruz, CA 95060

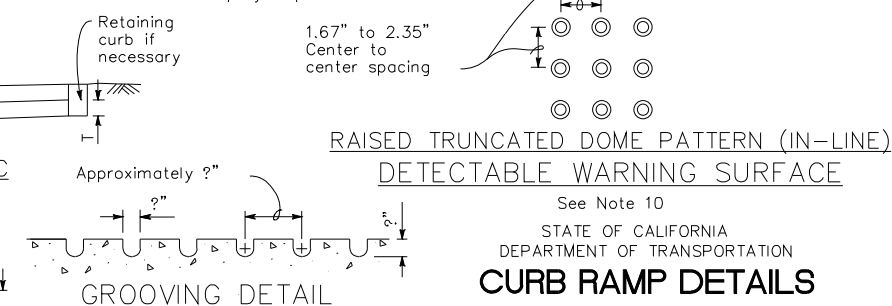
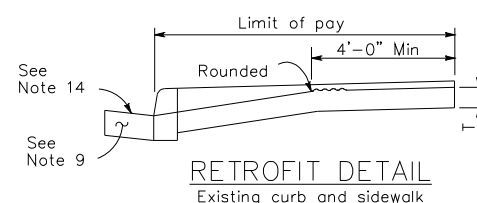
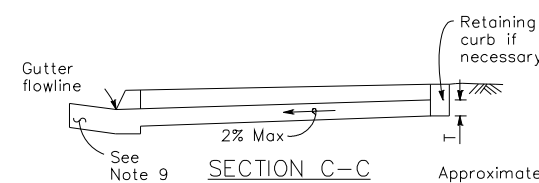
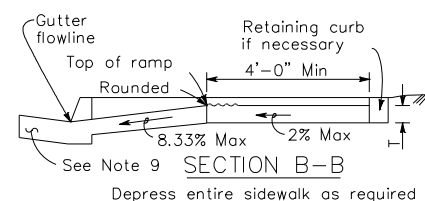
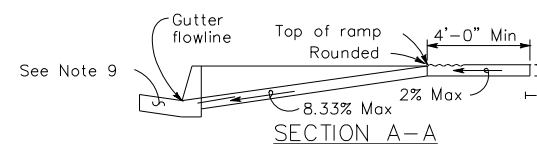
SPRING 2022 CHESTNUT STREET DETAILS AND SECTIONS

REFERENCES FIELD BOOK: DRAWING #:	DATE	JANUARY 2022	SCALE	AS SHOWN
	DRAWN	S METZ	SHEET	8 OF 13
	DESIGN		VAULT NO.	
	CHECKED			



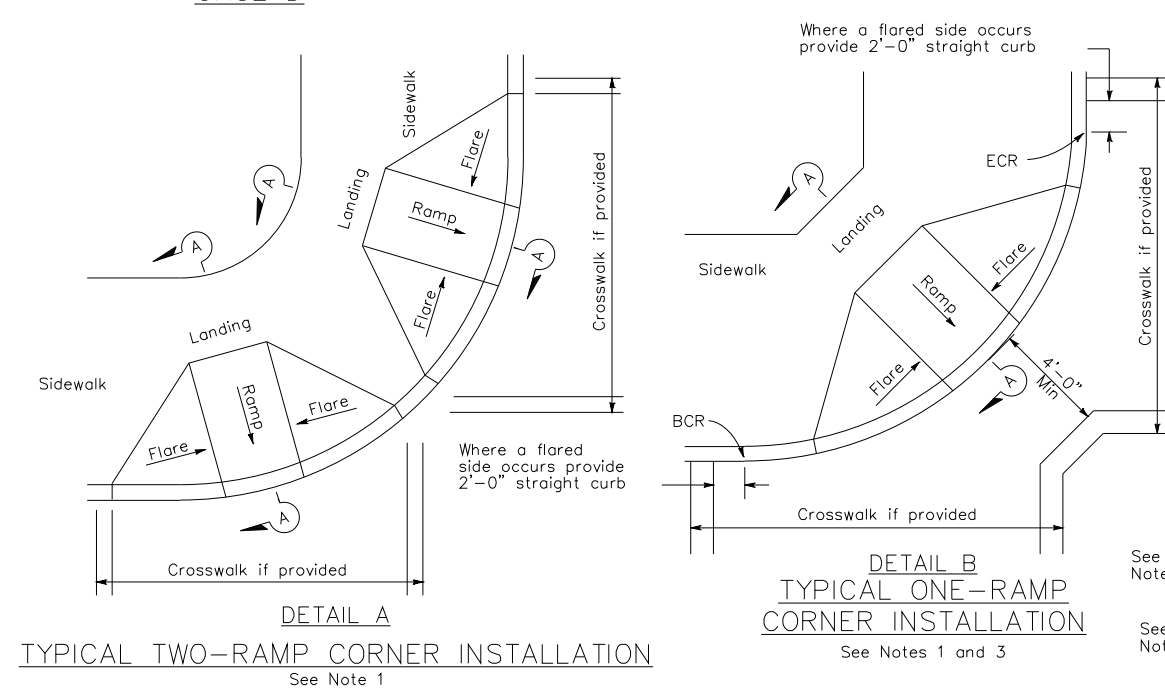
1. As site conditions dictate, Case A through Case G curb ramps may be used for corner installations similar to those shown in Detail A and Detail B. The case of curb ramps used in Detail A do not have to be the same. Case A through Case G curb ramps also may be used at mid block locations, as site conditions dictate.

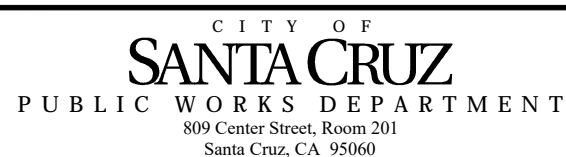
2. If distance from curb to back of sidewalk is too short to accommodate ramp and 4'-0" platform (landing) as shown in Case A, the sidewalk may be depressed longitudinally as in Case B, or C or may be widened as in Case D.
3. When ramp is located in center of curb return, crosswalk configuration must be similar to that shown for Detail B.
4. As site conditions dictate, the retaining curb side and the flared side of the Case G ramp shall be constructed in reversed position.
5. If located on a curve, the sides of the ramp need not be parallel, but the minimum width of the ramp shall be 4'-0".
6. Side slope of ramp flares vary uniformly from a maximum of 10% to curb to conform with longitudinal sidewalk slope adjacent to top of the ramp, except in Case C and Case F.
7. The curb ramp shall be outlined, as shown, with a 1'-0" wide border with $\frac{1}{4}$ " grooves approximately $\frac{1}{4}$ " on center. See grooving detail.
8. Transitions from ramps and landing to walks, gutters or streets shall be flush and free of abrupt changes.
9. Maximum slopes of adjoining gutters, the road surface immediately adjacent to the curb ramp or accessible route shall not exceed 5 percent within 4'-0" of the top and bottom of the curb ramp.
10. Curb ramps shall have a detectable warning surface that extends the full width and 3'-0" depth of the ramp. Detectable Warning Surfaces shall conform to the details on this plan and the requirements in the Special Provisions.
11. The edge of the detectable warning surface nearest the street shall be between 6" and 8" from the gutter flowline.
12. Sidewalk and ramp thickness, "T", shall be $\frac{3}{8}$ " minimum.
13. Utility pull boxes, manholes, vaults and all other utility facilities within the boundaries of the curb ramp will be relocated or adjusted to grade by the owner prior to, or in conjunction with, curb ramp construction.
14. For retrofit conditions, removal and replacement of curb apron will be at the Contractor's option, unless otherwise shown on project plans.



RSP A88A DATED SEPTEMBER 1, 2006 SUPERSEDES STANDARD PLAN A88A
DATED MAY 1, 2006 – PAGE 115 OF THE STANDARD PLANS BOOK DATED MAY 2006.

REVISÉD STANDARD PLAN RSP A88A





REFERENCES
FIELD BOOK:
DRAWING #:

DATE	JANUARY 2022	SCALE	AS SHOWN
DRAWN	S METZ	SHEET	9 OF 13
DESIGN		VAULT NO.	
CHECKED			

AGENDA: February 8, 2022

TO: Elderly and Disabled Transportation Advisory Committee
FROM: Amanda Marino, Transportation Planner
RE: Caltrans D5 Hybrid Flashing Beacons on Mission St.

RECOMMENDATION

Staff recommends that the Elderly and Disabled Transportation Advisory Committee (E&D TAC) provide input to Caltrans District 5 Staff on installing Hybrid Flashing Beacons on Mission St.

BACKGROUND

Caltrans expeditiously installed a flashing beacon at Mission St. & King St. in October 2021 following a pedestrian car related crash. Caltrans staff is now planning to complete additional upgrades to the flashing beacons and share some details regarding installation of Pedestrian Hybrid Flashing Beacons (PHFB) at Mission & Olive / Mission & Berkshire.

DISCUSSION

Pedestrian Hybrid Flashing Beacons (PHFB) are traffic control devices used to help aid pedestrians and bicyclists cross intersections safely. The FHWA's data suggest that PHFBs can help reduce overall collisions by 30% and can help reduce 70% of pedestrian related collisions. The PHFB acts similarly to a regular traffic signal. Vehicles must stop at the crosswalk when the signal turns red allowing pedestrians and bicyclists to cross.

SUMMARY

Caltrans is requesting input from the E&D TAC to replace and upgrade the existing pedestrian flashing beacons at Mission & Olive / Mission & Berkshire with Pedestrian Hybrid Flashing Beacons (PHFB).

Attachments

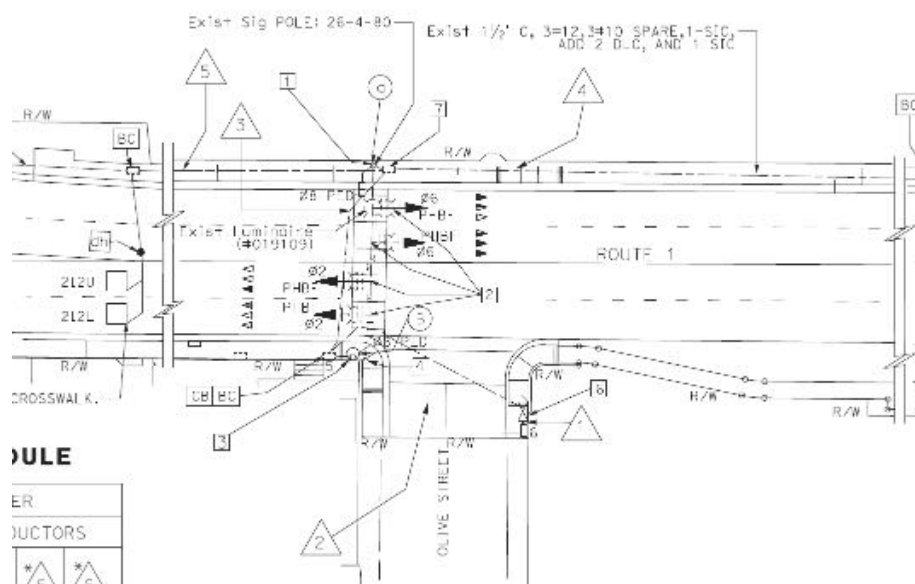
1. Caltrans Staff PHFB Information Sheet

1st Cabinet Location :



Santa Cruz PHFB Design

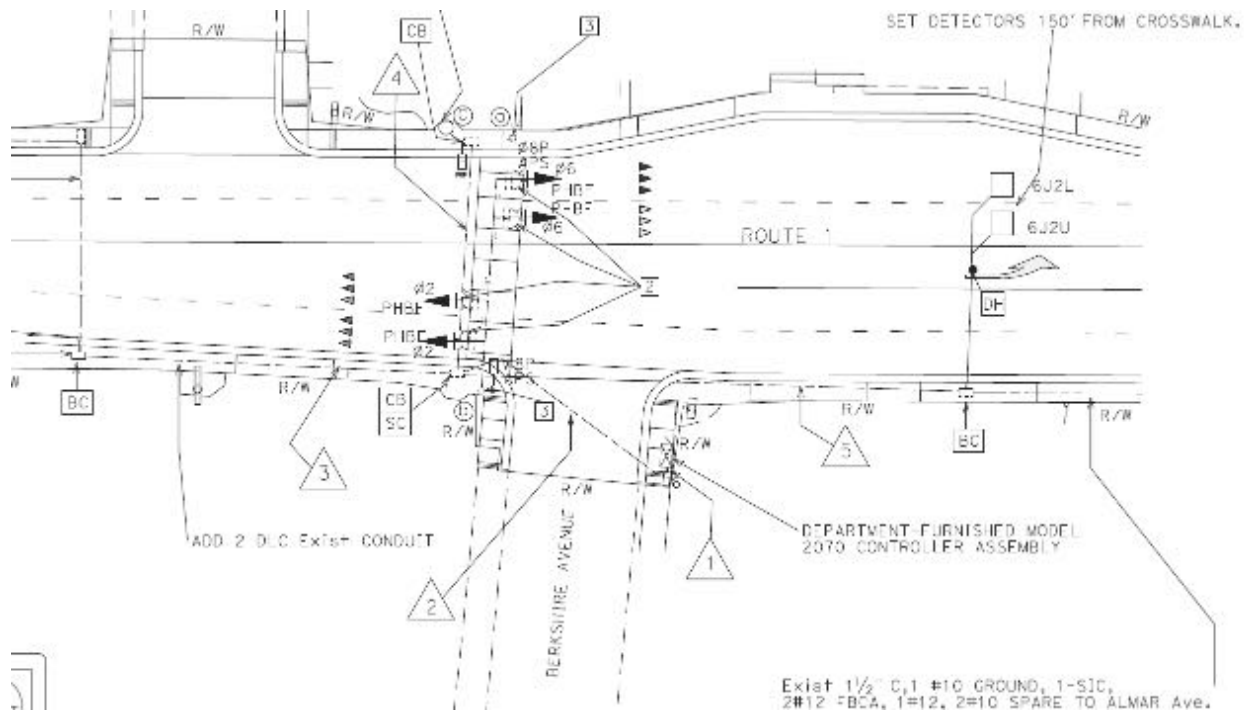
Olive:



2nd Cabinet Location:



Berkshire:



San Luis Obispo PHFB (Pedestrian Hybrid Flashing Beacon)

Some Examples: <https://www.youtube.com/watch?v=6MbZrRSj-9M>



District 4 pedestrian Hybrid Beacon System State Route 82



HAWK Video : <https://www.youtube.com/watch?v=eYZzi7w1GF8>

FHWA



Photo Credit: Mike Cynecki



Legend

SY Steady yellow
FY Flashing yellow
SR Steady red
FR Flashing red

AGENDA: February 8, 2022

TO: Elderly and Disabled Transportation Advisory Committee

FROM: Amanda Marino, Transportation Planner

RE: Clares Street Traffic Calming Improvements

RECOMMENDATION

Staff recommends that the Elderly and Disabled Transportation Advisory Committee provide input on the design for the Clares Street Traffic Calming Improvement Project

BACKGROUND

The Santa Cruz County Regional Transportation Commission (RTC) has discretion over about 5% of funds available to operate, maintain, and improve the transportation network in Santa Cruz County. One of the discretionary funding sources is the Surface Transportation Block Grant Program/Regional Surface Transportation Program Exchange (STBG/RSTPX) funds.

The City of Capitola is designing a “complete streets” project on the eastern portion of Clares Street to improve pedestrian and bicycle mobility. The Clares Street Traffic Calming Improvement Project is being funded primarily by the City, with an additional \$100,000 of STBG/RSTPX funds allocated by the RTC and a portion of funds allocated by the RTC for the 41st Ave repaving project to cover the intersection of Clares and 41st. The City is bringing the design plans to the Committee for review.

DISCUSSION

The Clares Street Traffic Calming Improvement Project involves measures to reduce vehicle speeds and improve safety for vehicles, pedestrians, and bicyclists. The project is located between 41st Avenue and Wharf Road (see ATTACHMENTS 1 & 2). The project will focus on adding traffic calming measures to improve pedestrian accessibility along Clares Street with the improvement of marked crosswalks. These crosswalks will include rapid-rectangular flashing beacons (RRFB) systems to improve pedestrian visibility. This project will focus on traffic calming features to improve bicycle accessibility along Clares St. and consists of narrowed vehicle lanes and new buffered bike lanes along the corridor. Completed final design is scheduled for Spring 2022, and construction is planned for Summer/Fall 2022.

SUMMARY

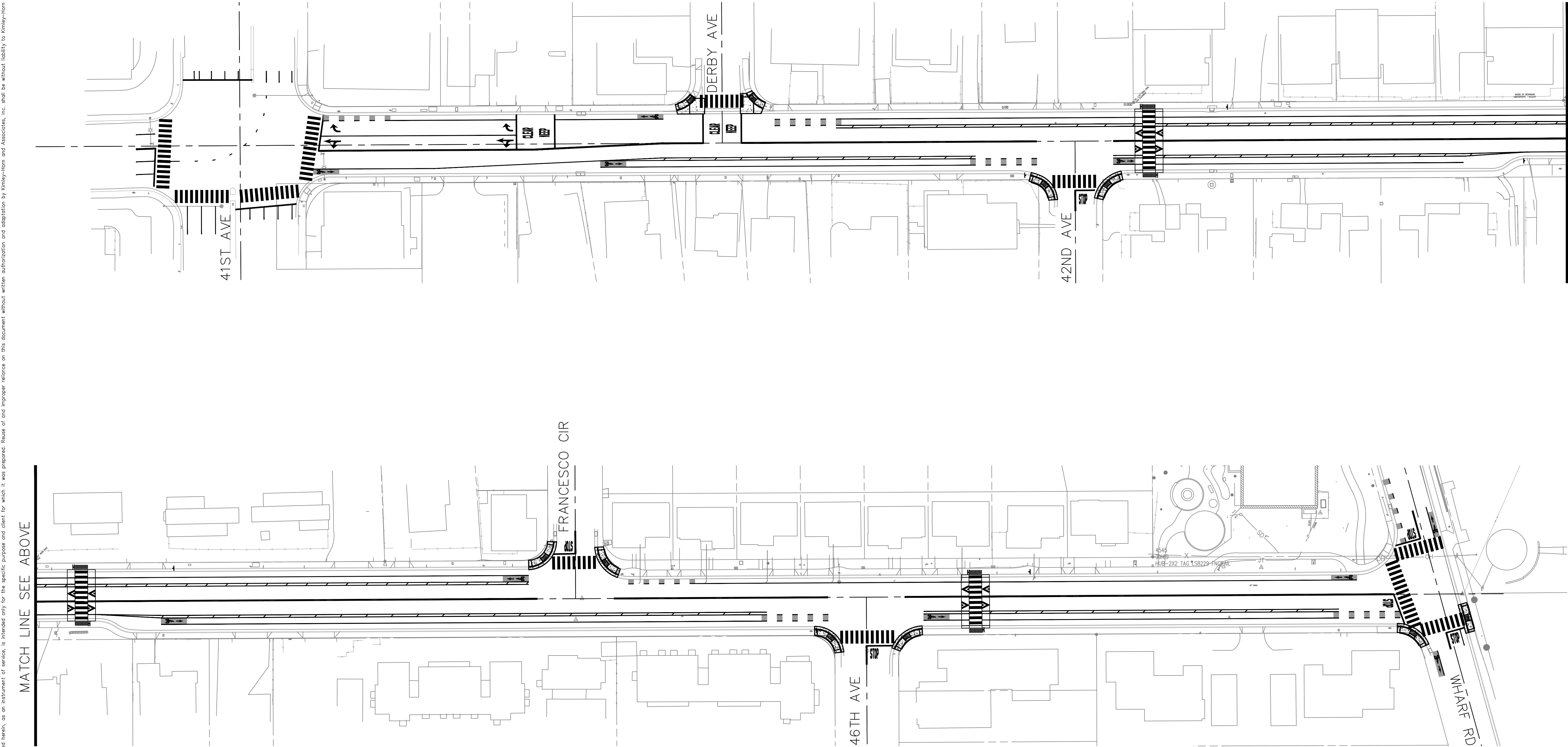
The City of Capitola is seeking the input of the Committee prior to construction of the Clares Street Traffic Calming Project.

ATTACHMENTS

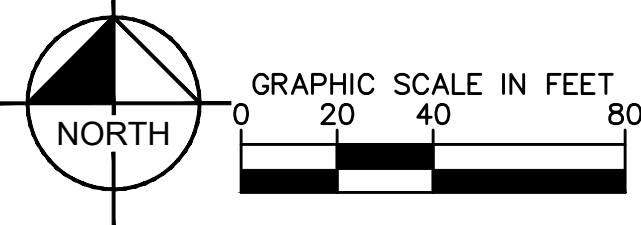
1. Clares Street Traffic Calming Improvements Overview Map
2. Clares Street Traffic Calming Signing and Striping Plan

Attachment 1

This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



65% PS&E
FOR REVIEW ONLY
JANUARY 2022

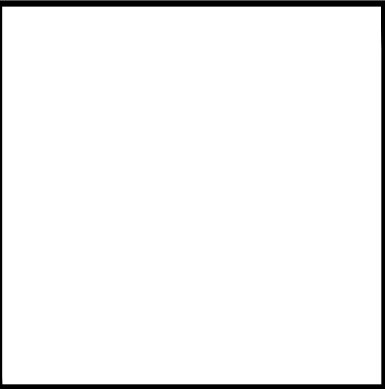


No.	REVISIONS	DATE	BY

© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
100 WEST SAN FERNANDO STREET, SUITE 250, SAN JOSE, CA 95113
PHONE: 669-800-4130
WWW.KIMLEY-HORN.COM

KHA PROJECT	097763133
DATE	1/31/2022
SCALE	AS SHOWN
DESIGNED BY	DW
DRAWN BY	DW
CHECKED BY	KM

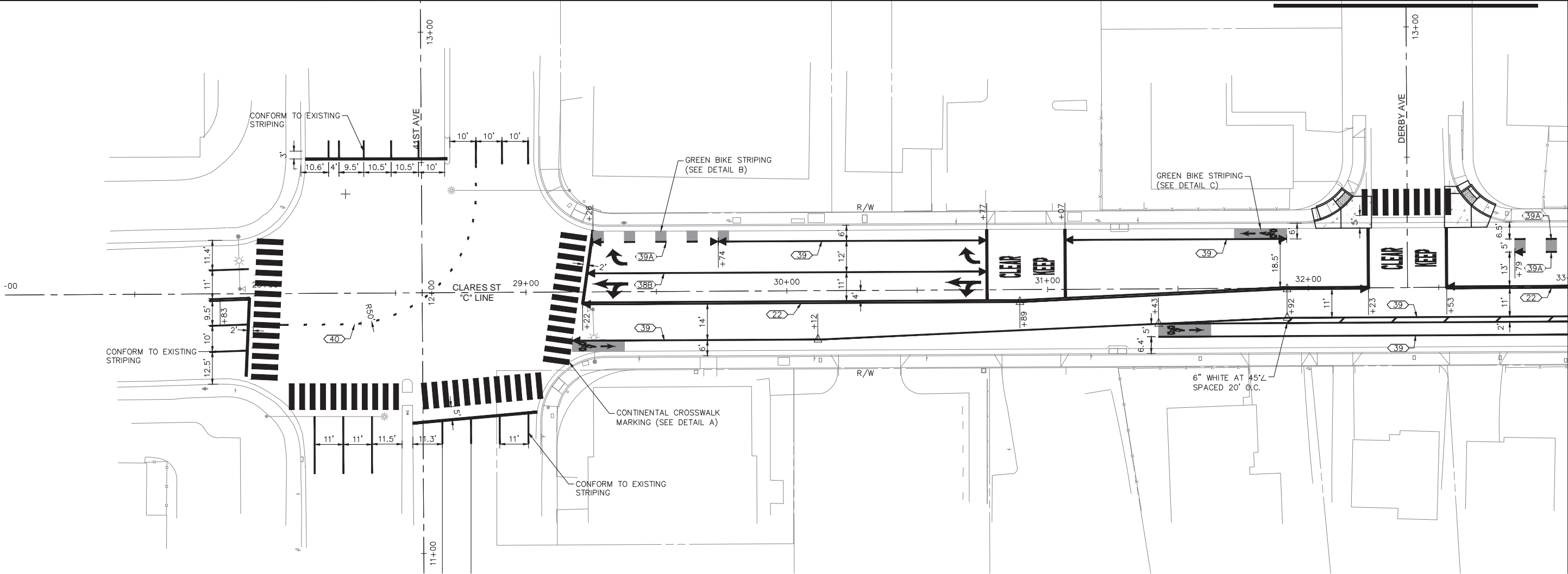
CLARES STREET TRAFFIC CALMING IMPROVEMENTS
PREPARED FOR
CITY OF CAPITOLA
SANTA CRUZ COUNTY CA



OVERVIEW MAP

SHEET NO. _____
OF 14 SHEETS

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



GENERAL SIGNING AND STRIPING NOTES

1. ALL EXISTING SIGNS SHALL BE PROTECTED IN PLACE UNLESS SPECIFIED FOR REMOVAL OR RELOCATION.
2. ALL EXISTING SIGNS SPECIFIED FOR REMOVAL SHALL NOT BE REMOVED UNTIL NO LONGER PERTINENT TO TRAFFIC CONTROL.
3. ALL EXISTING PAVEMENT DELINEATION CONFLICTING WITH PROPOSED STRIPING SHALL BE REMOVED PRIOR TO NEW STRIPING INSTALLATION.
4. ALL PAVEMENT DELINEATION MUST BE IN ACCORDANCE WITH THE CURRENT VERSION OF THE CALIFORNIA MUTCD AND CALTRANS STANDARD PLANS.
5. ALL STRIPING DIMENSIONS SHOWN ARE MEASURED FROM STRIPE TO STRIPE OR TO TOP OF CURB TO STRIPE WHICHEVER SCENARIO IS CLOSER.

SIGNING AND STRIPING NOTES

- 1 FURNISH AND INSTALL POLE MOUNTED RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLY PER VENDOR RECOMMENDATIONS (SEE DETAIL D)
- 2 SEE CONSTRUCTION DETAILS FOR SPEED TABLE WITH CROSSWALK PAVEMENT MARKINGS AND SIGNS PER CAPITOLA STANDARD DRAWINGS

LEGEND

- EXISTING SIGN
- PROPOSED SIGN
- EXISTING STRIPING DETAIL
- PROPOSED STRIPING DETAIL
- STRIPING CHANGE
- STRIPING LIMIT
- ANGLE POINT
- PAVEMENT MARKING

SIGN TABLE		
DETAIL	DESCRIPTION	EA
W11-2	PEDESTRIAN CROSSING	12
W13-1P	20 MPH	6
W16-7P	DIAGONAL LEFT ARROW	6
W16-7P (R)	DIAGONAL RIGHT ARROW	6
W17-1 (MOD)	SPEED TABLE	6
TOTAL		36

STRIPING AND MARKING TABLE BASE BID			
DETAIL	PATTERN OR LEGEND	LF	SQFT
9	LANE LINE	0	-
22	CENTERLINE	1899	-
38	CHANNELIZING LINE	154	-
39	BIKE LANE LINE (INCLUDES BUFFER)	8050	-
39A	INTERSECTION LINE (BIKE LANE)	528	-
40	INTERSECTION LINE (DASHED)	0	-
PM	"STOP" MARKING	-	132
PM	"KEEP CLEAR" MARKING	-	102
PM	STOP BAR MARKING	-	377
PM	CONTINENTAL CROSSWALK MARKING	-	2080
PM	BIKE SYMBOL/ARROW MARKING	-	105
PM	CHEVRON ARROW (SPEED TABLE)	-	144
PM	TYPE IV ARROW	-	30
PM	TYPE VII ARROW	-	54
PM	GREEN PAINT FOR BIKE LANE	-	1700
TOTAL		10631	4724

STRIPING AND MARKING TABLE ADD ALTERNATIVE 1			
DETAIL	PATTERN OR LEGEND	LF	SQFT
9	LANE LINE	186	-
22	CENTERLINE	12	-
38	CHANNELIZING LINE	30	-
39	BIKE LANE LINE (INCLUDES BUFFER)	12	-
39A	INTERSECTION LINE (BIKE LANE)	0	-
40	INTERSECTION LINE (DASHED)	120	-
PM	"STOP" MARKING	-	0
PM	"KEEP CLEAR" MARKING	-	0
PM	STOP BAR MARKING	-	133
PM	CONTINENTAL CROSSWALK MARKING	-	1000
PM	BIKE SYMBOL/ARROW MARKING	-	0
PM	CHEVRON ARROW (SPEED TABLE)	-	0
PM	TYPE IV ARROW	-	0
PM	TYPE VII ARROW	-	0
PM	GREEN PAINT FOR BIKE LANE	-	0
TOTAL		360	1133

65% PS&E
FOR REVIEW ONLY
JANUARY 2022

No.	REVISIONS	DATE	BY

© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
100 WEST SAN FERNANDO STREET, SUITE 250, SAN JOSE, CA 95113
PHONE: 669-800-4130
WWW.KIMLEY-HORN.COM

KHA PROJECT
09776.3133
DATE
1/31/2022
SCALE AS SHOWN
DESIGNED BY DW
DRAWN BY DW
CHECKED BY KM

CLARES STREET TRAFFIC
CALMING IMPROVEMENTS

PREPARED FOR
CITY OF CAPITOLA

SANTA CRUZ COUNTY CA

SIGNING AND STRIPING
PLAN

SHEET NO. 9
OF 14 SHEETS

SS-1

NORTH

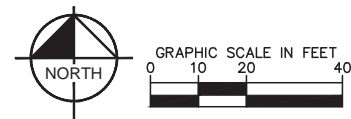
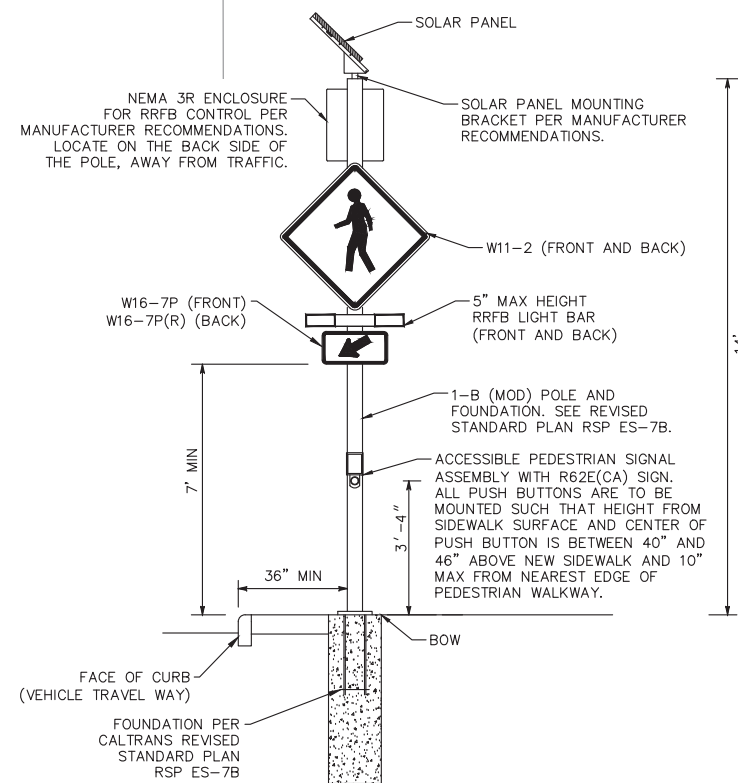
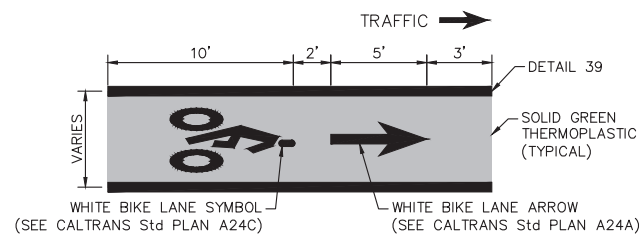
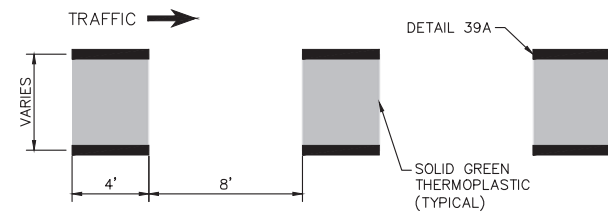
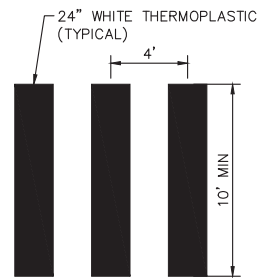
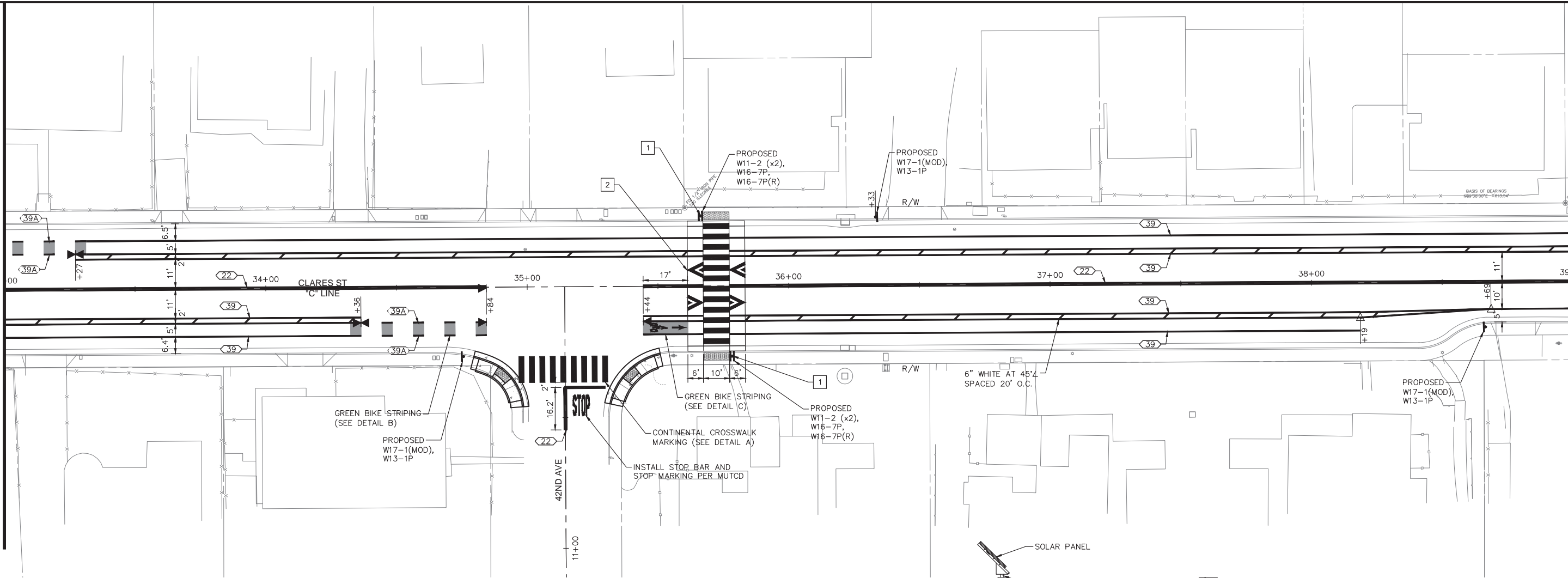
GRAPHIC SCALE IN FEET

0 10 20 40

CA: 1-800-227-2600
CALL BEFORE YOU DIG
UNDERGROUND SERVICE ALERT
CA: 1-800-227-2600
CALL TWO WORKING
DAYS BEFORE YOU DIG

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and approval by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

MATCH LINE 33+00 SEE SHEET SS-1



65% PS&E
FOR REVIEW ONLY
JANUARY 2022

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
100 WEST SAN FERNANDO STREET, SUITE 250, SAN JOSE, CA 95113
PHONE: 669-800-4130
WWW.KIMLEY-HORN.COM

KHA PROJECT	09776.3133
DATE	1/31/2022
SCALE	AS SHOWN
DESIGNED BY	DW
DRAWN BY	DW
CHECKED BY	KM

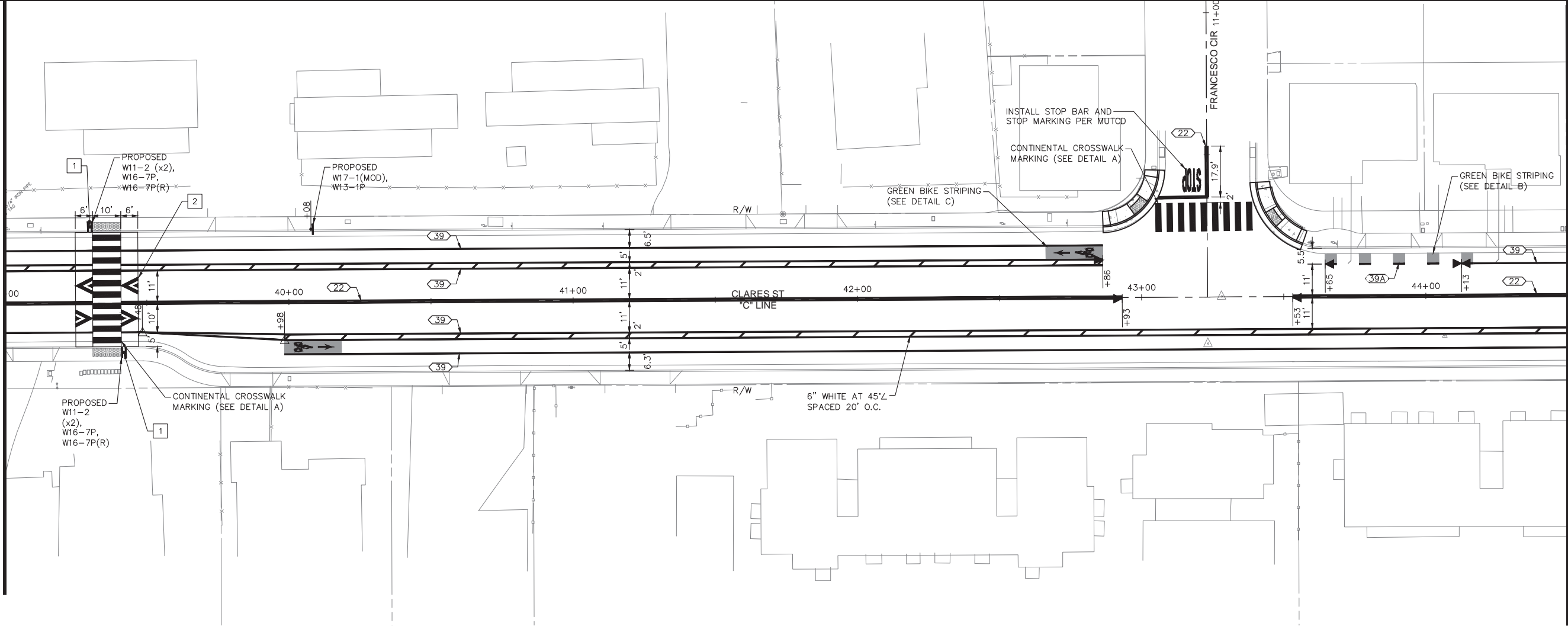
CLARES STREET TRAFFIC CALMING IMPROVEMENTS
PREPARED FOR
CITY OF CAPITOLA
SANTA CRUZ COUNTY CA

SIGNING AND STRIPING PLAN

SHEET NO. 10
OF 14 SHEETS
SS-2

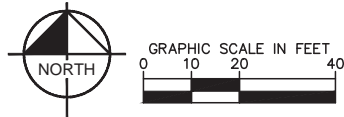
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and approval by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

MATCH LINE 39+00 SEE SHEET SS-2



MATCH LINE 44+50 SEE SHEET SS-4

65% PS&E
FOR REVIEW ONLY
JANUARY 2022



No.	REVISIONS	DATE	BY



© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
100 WEST SAN FERNANDO STREET, SUITE 250, SAN JOSE, CA 95113
PHONE: 669-800-4130
WWW.KIMLEY-HORN.COM

KHA PROJECT	09776.3133
DATE	1/31/2022
SCALE	AS SHOWN
DESIGNED BY	DW
DRAWN BY	DW
CHECKED BY	KM

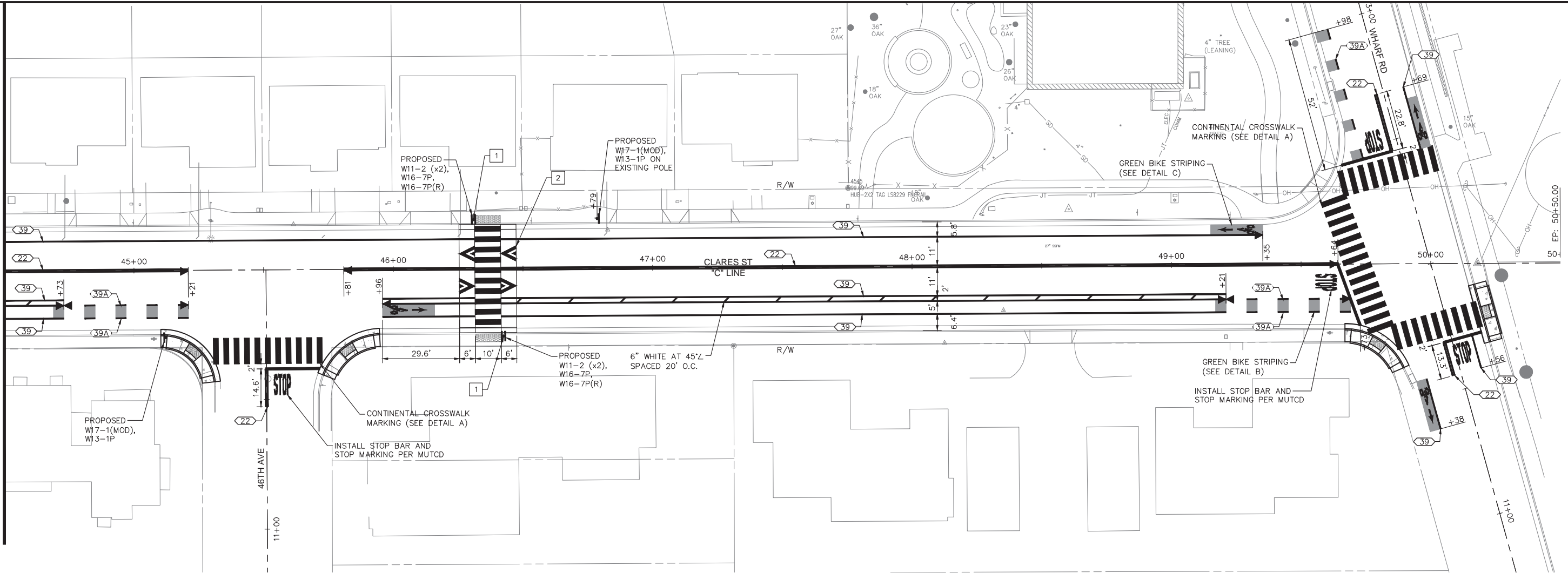
CLARES STREET TRAFFIC
CALMING IMPROVEMENTS
PREPARED FOR
CITY OF CAPITOLA
SANTA CRUZ COUNTY CA

SIGNING AND STRIPING
PLAN

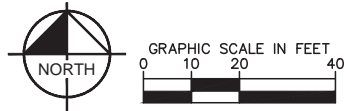
SHEET NO. 11
OF 14 SHEETS
SS-3

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

MATCH LINE 44+50 SEE SHEET SS-3



65% PS&E
FOR REVIEW ONLY
JANUARY 2022



No.	REVISIONS	DATE	BY



© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
100 WEST SAN FERNANDO STREET, SUITE 250, SAN JOSE, CA 95113
PHONE: 669-800-4130
WWW.KIMLEY-HORN.COM

KHA PROJECT	09776.3133
DATE	1/31/2022
SCALE	AS SHOWN
DESIGNED BY	DW
DRAWN BY	DW
CHECKED BY	KM

CLARES STREET TRAFFIC CALMING IMPROVEMENTS

PREPARED FOR
CITY OF CAPITOLA
SANTA CRUZ COUNTY CA

SIGNING AND STRIPING PLAN

SHEET NO. 12
OF 14 SHEETS

SS-4

TO: Elderly and Disabled Transportation Advisory Committee
FROM: Amanda Marino, Transportation Planner
RE: Draft County of Santa Cruz Active Transportation Plan

RECOMMENDATION

Staff recommends that the Elderly and Disabled Transportation Advisory Committee receive a presentation on the draft County of Santa Cruz Active Transportation Plan for unincorporated Santa Cruz County.

BACKGROUND

The County of Santa Cruz started work on an Active Transportation Plan in January 2020. The County of Santa Cruz Active Transportation Plan will update the 2011 Bicycle Plan, create the first comprehensive plan for new pedestrian facilities, and provide a roadmap for future improvements for walking and bicycling in unincorporated Santa Cruz County. The project is a partnership between the County of Santa Cruz Public Works Department, County Public Health, Ecology Action, and Bike Santa Cruz County, and is funded through a Caltrans Sustainable Communities Transportation Planning Grant as well as local matching funds.

The Active Transportation Plan covers unincorporated Santa Cruz County and does not include the cities of Santa Cruz, Watsonville, Capitola or Scotts Valley. Recommendations for Highway 9 from the San Lorenzo Valley Complete Streets Corridor Plan are not included in the draft plan project list, though they are included in the recommendations maps to show the complete picture of planned infrastructure improvements.

Public input was the foundation of the planning process and was gathered through a social media campaign, project website, in-person outreach, and virtual public meetings held in fall 2020. Community outreach resulted in 342 comments in the Street Story interactive map, 600 survey responses, and 291 comments from public meetings and in-person public outreach. Nearly half of public comments received came from Live Oak, so focus group meetings were held with representatives from Davenport, Boulder Creek, Ben Lomond, Brookdale, Felton, Santa Cruz Gardens, Amesti, and the College Road neighborhood in order to collect feedback from residents throughout the county. Public comments from the recent Scotts Valley Active Transportation Plan and County of Santa Cruz Complete Streets to Schools Plan that pertain to unincorporated Santa Cruz County were also reviewed.

Public comments were reviewed by the planning team and resulted in the draft spot treatment recommendations list. Copenhagenize Design Co., an urban design and planning firm based in Montreal, also supported the project as a consultant and developed recommendations for high-

priority corridors and intersections. High-priority corridors were selected through an analysis of nearby trip generators, bicycle and pedestrian collisions, slope, disadvantaged communities, average daily vehicle trips, sidewalk data, and the number of public comments received. Each street was scored using these criteria, and streets above a certain threshold were selected for development of corridor recommendations. Streets identified in the upcoming County of Santa Cruz General Plan update as Active Connectors, Main Streets, and Multi-modal Corridors were also added to the corridor recommendations list.

DISCUSSION

The draft Active Transportation Plan includes the following content:

- **Chapter 1: Introduction and Background.** The first chapter describes the vision and purpose of the plan and the plan's relationship to other local planning efforts.
- **Chapter 2: Existing Conditions.** This chapter outlines current conditions in unincorporated Santa Cruz County, including land use, commute trends, existing bicycle and pedestrian facilities, collision data, and current programs.
- **Chapter 3: Outreach.** The third chapter describes the outreach process and outlines key themes that emerged during public outreach. It also includes community survey data from the two temporary infrastructure demonstrations that were installed as part of the planning process.
- **Chapter 4: Pedestrian and Bicycle Plan.** This chapter includes goals related to walking and bicycling in unincorporated Santa Cruz County and recommendations for infrastructure improvements and programs to improve safety and comfort for people who are walking and biking.
- **Chapter 5: Implementation and Maintenance.** The final chapter discusses opportunities to fund and construct the recommended projects and programs and provides a list of high-priority projects for each Supervisorial District. It also includes a list of funding sources that the County of Santa Cruz can use to finance the recommended projects and programs and the methods the County will use to report on the plan's progress to the community and to maintain existing and future pedestrian and bicycle infrastructure.

The draft plan also includes several appendices, including outreach materials and survey data for the general public outreach and the temporary infrastructure demonstrations, prioritized project list, and planning-level project cost estimates.

Next Steps

The draft Active Transportation Plan will be released to the general public in mid-February for a 1-month public comment period at <https://ecoact.org/poweredbyme/>. Committee members are invited to review the draft plan and provide comments, which will be reviewed by the project team. The final plan is expected to be completed in March 2022 and will be used to guide future improvements and to help the County secure grant funding for project construction.

SUMMARY

Staff recommends that the E&D TAC receive a presentation and provide input on the Draft County of Santa Cruz Active Transportation Plan for unincorporated Santa Cruz County.

TO: RTC Advisory Committees
FROM: Rachel Moriconi, Transportation Planner
REGARDING: 2022 State and Federal Legislative Programs

RECOMMENDATIONS

Staff requests that committee members inform staff by February 11, 2022 of any State or Federal legislative issues that the Regional Transportation Commission (RTC) should consider, pursue, or monitor in 2022.

BACKGROUND

Each year the Regional Transportation Commission (RTC) adopts legislative platforms to guide its analysis of state and federal legislative or administrative actions that could impact transportation funding or implementation of the Regional Transportation Plan (RTP), Regional Transportation Improvement Program (RTIP), Measure D and priority transportation projects in Santa Cruz County. Working with local jurisdictions, the Central Coast Coalition (regional transportation agencies from Monterey, San Benito, Santa Barbara, San Luis Obispo, and Santa Cruz Counties), the California Association of Councils of Governments (CALCOG), the Self Help Counties Coalition, and other transportation entities, the RTC monitors legislative proposals, notifies state and federal representatives of the RTC's analysis of key issues, and provides input on other federal and state actions.

DISCUSSION

Staff recommends that committee members inform staff of any legislative priorities and/or issues that the RTC should consider, monitor, or pursue in 2022. Committee members can provide suggestions at this meeting or by emailing rmoriconi@sccrtc.org by February 11, 2022. The preliminary draft 2022 Legislative Program is attached ([Attachment 1](#)). The RTC board is expected to consider the 2022 State and Federal Legislative Programs at its March 2022 meeting.

The RTC legislative program is used to advance regional projects and key goals and targets in the *Santa Cruz County [Regional Transportation Plan](#)*, which focuses on sustainability to improve multimodal access and mobility in ways that improve health, reduce pollution and retain money in the local economy; reduce

collisions and improve safety; maintain existing transportation infrastructure and services; and deliver improvements cost effectively, equitably and responsive to the needs of all users of the transportation system and the natural environment. As part of the Legislative Program, RTC works to ensure that transportation-related statutes and guidelines are structured in a manner that recognizes Santa Cruz County's significant traffic congestion, maintenance, active transportation, and transit system needs and sustainability goals.

Generally, the RTC's legislative program covers legislative and administrative actions that:

- Involve funding or a funding mechanism for transportation projects and programs
- Involve the implementation of transportation and greenhouse gas emission reduction policies and programs
- Involve transportation and land use
- Involve the environmental review process
- Involve changes to the way transportation projects are delivered
- Affect the Commission directly (e.g. Commission responsibilities, policies or operations)

In 2022, a key focus area at the state and federal level will be on implementation of the multiyear federal transportation act (Infrastructure Investment and Jobs Act (IIJA)). RTC and AMBAG staff sit on several implementation working groups organized by CalSTA. Staff will also continue monitoring federal negotiations around stimulus and federal "Build Back Better" Act.

At the state level, Brown Act modifications related to online/virtual meetings, SB375/RTP/Sustainable Communities Strategy (SCS) implementation and "reform", and distribution of state surplus (estimated at \$30-60 billion) will likely dominate discussions around the state budget.

Governor Newsom's January Budget proposal includes an additional \$9 billion for transportation projects. Anticipated budget issues include:

- The Governor's proposal to delay the indexing of the gas tax this year (about 3 cent per gallon). Transportation stakeholders are concerned how the approximately \$525 million that would be foregone could impact the State Transportation Improvement Program (STIP) and local cities and counties and how the funds would be backfilled.
- Making more than just \$9 billion of the surplus available for infrastructure projects and programs – including ITAC request for additional funds for system preservation. The Governor's proposal currently includes:
 - \$500 million augmentation for the Active Transportation Program;

- \$3.75 billion for the Transit and Intercity Rail Capital Program (TIRCP) (\$2 billion for transit and rail projects; \$1.25 billion for specific transit projects in Southern California; \$500 million for grade separations);
- \$1.2 billion for port, freight, and goods movement infrastructure; \$1.1 billion workforce training and ZEV equipment related to supply chain;
- \$400 million Climate Adaptation Projects for state and local projects that support climate resiliency and reduce risks from climate impacts;
- \$150 million Highways to Boulevards Pilot to turn underutilized highways into multimodal corridors that support affordable housing and complete streets in DACs; and
- \$100 million for Highway Safety Improvement Program (HSIP) bike and ped safety projects that reduce severe injuries and fatalities.
- Infill infrastructure to support location efficient (low VMT) affordable housing.
- Potential region-specific asks (aka earmarks).

In 2021, the State Legislature introduced more than 2500 bills, resolutions and constitutional amendments. A few of the bills RTC staff tracked last year and the result of those bills included:

- AB 43 (Friedman) Traffic Safety: setting speed limits. - Chaptered
- AB 361 (Rivas, Robert) Open meetings: state & local agencies: teleconferences. - Chaptered
- AB 339 (Lee) Local government: open and public meetings. - Vetoed
- AB 122 (Boerner Horvath) Vehicles: required stops: bicycles. - Vetoed
- AB 1147 (Friedman) Regional transportation plans. (sustainable communities strategies) - Vetoed
- SB 261 (Allen) Regional transportation plans: sustainable communities strategies. 2-Year Bill
- SB 475 (Cortese) Transportation planning: sustainable communities strategies. 2-Year Bill

SUMMARY

Committee members are encouraged to review the preliminary draft 2022 Legislative Program and email rmoriconi@sccrtc.org by February 11, 2022 of any changes the RTC should consider.

Attachment 1: Draft 2022 Legislative Program

\\rtcserver2\shared\legislat\2022\legprogram2022-srcommittees.docx

a complaint with RTC by contacting the RTC at (831) 460-3212 or 1523 Pacific Avenue, Santa Cruz, CA, 95060 or online at www.sccrtc.org. A complaint may also be filed directly with the Federal Transit Administration to the Office of Civil Rights, Attention: Title VI Program Coordinator, East Building, 5th Floor-TCR, 1200 New Jersey Ave., SE, Washington, DC 20590.

I:\E&DTAC\2022\01-11 Special Meeting\01_Agenda-jan.docx

PRELIMINARY DRAFT



Santa Cruz County Regional Transportation Commission 2022 STATE Legislative Program

Focus Areas in 2022

Proposed changes from 2021 highlighted in underline/strikeout.

- Implementation of the federal Infrastructure Investment and Jobs Act (IIJA). Maximize funding available for priority projects in Santa Cruz County.
- State Budget. Maximize **General Fund surplus** funds for local system preservation, Active Transportation Program (ATP), and local/regional priority transportation projects. Ensure funding for local streets and roads and State Transportation Improvement Program (STIP) are not reduced.
- Support modifications to the **Brown Act** to enhance public and committee member participation in virtual meetings.
- Ensure legislative and administrative actions **support implementation of priority transportation projects and programs in Santa Cruz County**, including projects identified in the Measure D Expenditure Plan. Maintain and increase funding for RTC projects and programs, support streamlining and other actions which could expedite delivery of projects, and oppose efforts which could hinder implementation of RTC priorities.
- Support efforts to **reduce greenhouse gas emissions and improve mobility through increased funding for pedestrian, bicycle and transit projects**, increased equitable access to **zero emission vehicles** and infrastructure. Pursue changes to SB375
- Sustainable Communities Strategy (SCS) that support GHG emission reduction and reduce bureaucratic burdens.
- Support efforts related to **adaptation, resilience and response to natural hazards** and the impacts of climate change, including extreme storms, sea level rise and wildfires.
- Support legislative and administrative actions that will improve safety on state highways and local roads, including AB43 implementation (speed limit reductions) and Caltrans Director's order related to complete streets, especially where state highways serve as main streets.
- Support **Transportation Development Act (TDA) program modifications** which reduce the burden of outdated performance measures and eliminate the farebox recovery penalty for public transportation systems.
- Support state and federal **COVID-19 relief and stimulus funding** to support economic recovery and make up for state, local and transit agency transportation revenue losses.
- Support new transportation funding mechanisms to replace gas and diesel taxes, considering increased vehicle fuel economy and zero-emission vehicle adoption.



PRELIMINARY DRAFT

Ongoing Priorities

Transportation Funding

- **Protect Transportation Funding:** Preserve existing and new funding for transportation projects, maximize funding for Santa Cruz County transportation projects, and preserve regional discretion and priority-setting.
 - Stable, formula funding is essential for addressing the backlog of transportation infrastructure repairs and improvements in Santa Cruz County. Protect current and future taxes and fees and other transportation funds (including Highway Users Tax Account (HUTA), Transportation Development Act (TDA), State Transportation Improvement Program (STIP), Active Transportation Program (ATP), and other funds) from elimination or diversion to other State programs, General Fund loans, general obligation bond debt service, or to other non-transportation purposes.
 - Support actions that preserve the intent of Senate Bill 1 (SB1) and local measure funding to allow the State, regions and local agencies to maintain, protect and improve existing transportation funds dedicated to transit, congestion management on the state highway system, lifeline arterials, and goods movement routes while also addressing immediate and long-term unmet funding needs.*
 - Monitor implementation efforts of Executive Order N-19-19, which directs the California State Transportation Agency (CalSTA) to invest its annual \$5 billion portfolio to help reduce transportation-related greenhouse gas emissions, and to ensure that state funds, specifically SB 1 funds, continue to be used for transportation purposes. Ensure that state regulations do not negatively impact implementation of the voter-approved Measure D Expenditure Plan.
 - With increased emphasis on vehicle fuel economy and zero-emission vehicle adoption, explore and support new funding mechanisms to replace gas and diesel taxes for transportation investments. Monitor proposals such as pay-by-the-mile user fees, public private partnerships, vehicle registration fees, or wholesale energy taxes. Ensure that proposals are equitable to disadvantaged and rural areas.*
 - Oppose proposals that could tie transportation fund availability to local jurisdictions, to non-transportation and development projects.
- **COVID Recovery:**
 - Support state and federal COVID-19 relief and stimulus legislation to provide funding to address adverse funding impacts of the COVID-19 pandemic on transit and other local agencies and to expedite project implementation.
 - Temporarily **adjust maintenance of effort and performance criteria requirements**, for SB1 Local Streets and Roads Program, local sales tax measure funds, and fare-box recovery requirements for transit given impacts on local revenues from COVID-19.



PRELIMINARY DRAFT

- **Ensure Fair Distribution of Funding:**
 - Ensure state and federal funds are made available for projects in Santa Cruz County, are distributed equitably, and are not disproportionately distributed to large regions. Ensure competitive programs make funding reasonably available for multimodal projects in Santa Cruz County, that address local and regional priorities.
 - **Local Role:** Ensure a strong role for regional and local agencies in planning and determining transportation investment priorities. Support legislation that respects local authority, protecting or expanding local decision-making in programming expenditures of transportation funds, rather than the State making top-down funding decisions that are not community-based. Project and increase direct funding to regions through both federal and state programs; and reinforce and build upon the structure of SB45 that provides regions a strong voice in the programming of state funds.
 - **State Transportation Improvement Program (STIP):** Ensure equitable programming and allocation of STIP funds.
 - **“Disadvantaged Communities” Definition:** Ensure that legislation and programs aimed at benefiting disadvantaged communities use a definition of “disadvantaged communities” (DACs) that ensures that projects that benefit low-income and other transportation disadvantaged residents of Santa Cruz County are not excluded from funding opportunities that support sustainable communities, transportation choices, and investments in alternative modes of transportation. Ensure that the definition does not rely exclusively on communities defined as DACs by CalEnviroScreen, which disproportionately excludes many low-income communities in Santa Cruz County.
- **Increase Funding for All Transportation Modes:** Support measures that increase funding for and support implementation of transportation projects in Santa Cruz County, including funds for ongoing system maintenance, congestion reduction, safety, complete streets, active transportation bike, pedestrian, transit projects, transit-oriented development, and specialized transportation for seniors and people with disabilities in Santa Cruz County.
 - **New funding systems:** Phase in new funding systems which are tied to system use, rather than fuel consumption or fuel prices. May include new user fees, such as a Road User Charge or Vehicle Miles Traveled (VMT) fee and other alternative funding mechanisms.
 - **Expand local revenue-raising opportunities** and innovative financing options to address the significant backlog of transportation needs. Provide locals with the ability to supplement and leverage state funding for investments that protect state and local transportation assets.
 - **Expand the authority of the RTC and local entities to increase taxes and fees** for transportation projects, such as new gas taxes, vehicle registration fees, property-tax financing, and infrastructure financing districts.



PRELIMINARY DRAFT

- Support clarifying amendment to Government Code Section 65089.20 that will give RTPAs equal treatment with Congestion Management Agencies (CMAs) to seek voter approval for a local vehicle registration fee. *(SB83 cleanup)*
- **Lower Vote Threshold:** Support efforts to amend the California constitution to lower the voter threshold for local transportation and affordable housing funding measures, such as local sales tax or vehicle registration fee ballot measures, from the 2/3 supermajority to a simple majority or 55% vote. Support actions which would broaden eligibility in existing and/or new transportation funding streams to enable their use as a subsidy for low-income transportation system users (e.g. discounted fares for public transportation or shared mobility service).
- **Active Transportation Program (ATP):** Increase ATP funding and ensure potential reforms to the Active Transportation Program (ATP) do not reduce the proportion available for Santa Cruz County agencies to compete for, including funds to the competitive statewide, small urban and rural funding pots. Support efforts to simplify the Active Transportation Program (ATP) application and project delivery, build local capacity to deliver transformative projects, and provide regions greater flexibility to innovate and strategically invest funds to meet local needs.*
- **Cap & Trade:**
 - Increase percent of Cap & Trade revenues allocated to transportation projects and programs that help reduce greenhouse gas emissions in Santa Cruz County.
 - Support increases in Low Carbon Transit Operations Program (LCTOP) appropriations
 - Support policy changes to the Affordable Housing and Sustainable Communities program (AHSC) that increase funding opportunities for projects in Santa Cruz County.
 - Ensure continued funding for low and zero emission transit deployment. Ensure regulatory and legislative requirements related to transit electrification provide flexibility, consider cost and available technology, and do not place an undue burden on transit agencies.
 - Support legislation to devote a permanent Cap-and-Trade funding allocation to the Active Transportation Program.
- Support legislation to **increase the availability of funding for cities, counties, and regions** to support economic development, affordable housing, and implementation of sustainable community strategies, as well as policy tools to reduce single-occupancy vehicle travel in a manner that ensures equitable policy outcomes.
- Support innovative projects such as a new state-supported intercity passenger rail service on the planned Coast Route between the Bay Area and Goleta (*previously called the Coast Daylight*).*



PRELIMINARY DRAFT

- **Increase and Preserve Funding for Priority Projects in Santa Cruz County:**
 - Projects on Highway 1
 - Local Street and Roadway Preservation
 - Transit projects
 - 511 implementation
 - Bicycle and Pedestrian facilities, including the Monterey Bay
 - Sanctuary Scenic Trail Network (MBSST) and in San Lorenzo Valley
 - Santa Cruz Branch Rail Line preservation
 - Soquel Avenue-Freedom Boulevard Corridor
- **Transportation Development Act (TDA):**
 - Monitor potential modifications to the TDA. Ensure funding for transit, planning, administrative, and other TDA purposes in Santa Cruz County are not reduced. Oppose efforts that would reduce TDA funds which are essential for RTC administration and planning. Support the development of greater efficiencies within the TDA while streamlining and updating performance metrics and eliminating penalties associated with farebox recovery. Support development of alternative performance measures that are focused on incentivizing transit agency actions that improve transit service and increase ridership, consistent with state and regional climate and equity goals.
 - Ensure discount fares aimed at boosting ridership and improving social equity do not result in reduced state funding. Pursue relief from TDA audits and performance criteria during the current economic downturn.

Project Implementation

- **Streamlining, Expediting, Facilitating Project Delivery:** Support administrative and/or legislative efforts which may be required to implement or expedite delivery of priority projects. Includes actions that streamline funding applications, simplify program administration, efforts that modernize and accelerate project delivery.
 - Support the development of greater efficiencies of transportation project implementation, including California Environmental Quality Act (CEQA) reform, storm water runoff regulations, CA Fish and Wildlife, CA Water Quality Control Board and California Public Utilities Commission permit and approval processes, to streamline both project development and delivery for priority transportation and transit projects, including the Scotts Creek Bridge Replacement and implementation of the Regional Conservation Investment Strategy (RCIS), and eliminating any unnecessary, overly burdensome and/or duplicative mandates.
 - Support legislative and administrative actions required to secure permits that may be required to implement priority projects.
 - Opportunities to expedite transportation project delivery may include increasing contracting and financing options, increased flexibility in early allocation of programmed funds and initiating reimbursable work with local funds in advance of CTC allocation of all projects, efforts that expedite the Caltrans design review process, opportunities to expedite locally-sponsored projects on the state highway system, and increase in encroachment permit limits.
 - Support environmental streamlining measures for bike, pedestrian, transit, and infrastructure preservation within existing public rights of way, and other measures that expedite project delivery. Support efforts that provide for streamlined project delivery for transit projects that fulfill the goals of AB 32 and



PRELIMINARY DRAFT

SB 375, as well as other state and federal air quality mandates and mobility performance measures.

- Support delegation of fund allocation responsibilities to Caltrans.
- Allow advance payment of programmed funds, in order to expedite project delivery and resolve cash flow challenges faced especially by small agencies.
- **Advanced Mitigation:** Support implementation of “advanced mitigation” environmental programs, including approving up-front environmental mitigation funding for projects, such as the Highway 17 Wildlife Crossing. Support creation of a low-interest loan program to support advance mitigation and habitat conservation plans that mitigate the impacts of transportation infrastructure and make project implementation more efficient.
- **Safety:** Support legislation and programs that improve transportation safety for all users and support programs aimed at eliminating all traffic-related serious injuries and fatalities.
 - Speed limits: Support proposals that would allow local jurisdictions to reduce speed limits on both local roads and state highways and work with state representatives to modify the California Vehicle Code to allow for prima facie speed limits of 25 mph on state highways that function as main streets, especially in business and school zones to address findings and to address other recommendations of the AB2363 Zero Traffic Fatalities Task Force report.
 - Traffic Laws & Enforcement: Support proposals to increase enforcement and modification of traffic laws to better protect pedestrians and bicyclists, including proposals to authorize automated speed enforcement on a pilot program basis, and modifications to vehicle code to allow vehicles to cross a double-yellow line when passing cyclists.
 - Education: Support commercial driver, bus driver, motorist, bicyclist, and Safe Routes to Schools training and education programs which reduce collisions.
- **Active Transportation Facilities:** Support modification to rules, regulations, and government codes that will make roadways more bicycle and pedestrian-friendly, including: laws associated with sharing the road; ensuring complete streets components (e.g. accessible pedestrian signals) are considered during the design of all projects; increasing funds for pedestrian, bicycle, and new micro-mobility devices and services (e.g. bike share), and safety countermeasures (e.g. buffered or protected bike lanes, flashers and lighting at crosswalks); increasing funds to provide resources necessary for First/Last Mile improvements; Safe Routes to School Programs; and providing additional direction and consistency for accessible pedestrian design.
- **Land Use/Housing/Transportation Coordination:**
 - Support efforts to reduce vehicles miles traveled and promote job-housing balance which also protect locally-driven land use planning that implements broad policy goals set by the state to provide affordable housing in transit-rich areas. Encourage new developments to incentivize active transportation and transit use. Ensure SB743 (Steinberg, 2013) implementation supports infill development,



PRELIMINARY DRAFT

promotion of public health through active transportation, and expedites transportation project delivery. Support innovative measures to mitigate growth in vehicle miles traveled, such as regional mitigation banks.

- Support state goals to reduce homelessness. Monitor implementation of Governor Newsom's Executive Order N-23-20 which requires Caltrans to develop a model lease template to allow counties and cities to use Caltrans property adjacent to highways or state roads for short-term emergency homelessness shelter; and requests that special districts, cities, counties, and transit agencies, and others to examine their ability to provide shelter and house homeless individuals.
 - Support efforts to streamline SB375 implementation and extend the timeframe between required Regional Transportation Plan updates.
- **Federal Transportation Act Implementation:** Support legislation and administrative strategies to implement the federal authorization bill (Infrastructure Investment and Jobs Act (IIJA)), in a way that ensures the best possible outcome for transportation projects in Santa Cruz County.
 - **SHOPP Program:**
 - Support Caltrans' efforts to provide more outreach regarding State Highway Operation and Protection Program (SHOPP) projects and to include measureable targets for improving the state highway system. Support clarification of existing laws to permit the expenditure of SHOPP funds for operational projects on state highways.
 - Support inclusion of complete streets within SHOPP projects, as appropriate, but especially in areas where state highways serve as main streets, such as Highway 1/Mission St, Highway 9 through San Lorenzo Valley, and Highway 152 in Santa Cruz County.
 - **Commuter Programs:** Support policies and legislation aimed at reducing trips and vehicle miles traveled and associated traffic congestion, including, but not limited to, employer-based programs to help reduce the share of commuting by single-occupant vehicles, expanding broadband and incentives to facilitate telecommuting, expanding park and ride lots, and a regional commuter benefits ordinance. Support dedicated funding for Transportation Demand Management (TDM) programs and strategies.
 - **Shared Mobility Systems:** Support policies that enable technological innovations to improve mobility, while protecting the public's interest. Monitor legislation and regulations related to shared mobility, such as transportation network companies (TNCs) and real-time carpooling, to ensure that mobility benefits are maximized, especially for underserved populations, and access to critical data for transportation and land-use planning and operational purposes is assured. Support measures that allow for local control and regulation of shared mobility systems such as scooters, bikes, and other fleets.
 - **Connected and Autonomous Vehicles:** Monitor and engage in legislation and regulations to facilitate deployment of connected vehicles and autonomous vehicles. Oppose federal efforts to preempt local authority over the use of autonomous vehicles



PRELIMINARY DRAFT

in their communities. In partnership with California cities and counties, transit agencies, the business community, and other transportation organizations, engage in regulatory and legislative efforts related to connected and autonomous vehicles with the goal of accelerating their safety, mobility, environmental, equity and economic benefits. Similar to the “shared mobility” strategy, support access to critical data for transportation and land use planning and operational purposes.

- **Electrification and Zero Emission** vehicle fleets: Support funding and coordination, including policy, planning, and infrastructure, for low and zero emission vehicles.
 - Support additional dedicated funding to help transit operators and colleges convert their bus and van fleets to zero-emission in order to meet the state’s Innovative Clean Transit rule and accelerate the decarbonization of the transportation system. Support reduced utility pricing for public transit electric vehicle fleets.
 - Support proposals that provide funding for regions and localities to build infrastructure (including chargers, trenching, upgrading electrical capacity) and provide incentives for zero-emission vehicle and other modal device purchases, considering cost of increased usage of electricity, electric power storage capacity, proper safety protocols* and access for lower-income households.
- **Resilience:** Monitor and support legislation that invests in projects and programs to improve resilience to the impacts of climate change on transportation infrastructure and utilization of public transit in emergencies that address scenarios such as severe storm events, public safety power shut off events, wildfires, and sea level rise.*
- **Encroachments:** Support legislation that clarifies the authority under which rail property owners may remove, or by notice may require the removal of encroachments.
- **Unfunded Mandates:** Oppose unfunded mandates and seek funding for mandates imposed in recent years. Require new regulatory proposals to include an estimate of the cost and impact such proposals will have in the delivery of California’s transportation program.
- **Central Coast Representation:** Advocate for Central Coast representatives to be appointed to the California Transportation Commission (CTC) and other state boards and committees in order to ensure that the complexities of small, coastal, and rural jurisdictions addressing their infrastructure and mobility needs are considered.*
- **Modernization of the Brown Act:** Enact legislation to expand public and board participation in public meetings. In order to maximize participation and access by board and committee members, modify the Brown Act to enhance participation and eliminate requirement to notice of all remote board or committee member locations.*

Support legislative clean-up of AB 361 (Rivas) to provide greater flexibility to local and regional agencies to hold meetings virtually without the requirement to renew findings every 30 days.



PRELIMINARY DRAFT

Monitor and support legislation to provide long-term flexibility for regional and multijurisdictional agencies to conduct their business remotely outside of emergency conditions as a means of increasing public participation and board and committee member participation while also reducing the time and expenses associated with travel, vehicle miles traveled and the greenhouse gas and other tailpipe emissions from driving.

**Starred items are also part of the Central Coast Coalition's legislative platform.*

\\\\rtcserv2\\shared\\legislat\\2022\\2022-rtc-legislativeprograms-draft.docx



State Legislative Program-Page 9
For more information contact the RTC at 831-460-3200; info@sccrtc.org;
1523 Pacific Ave., Santa Cruz, CA 95060

PRELIMINARY DRAFT



Santa Cruz County Regional Transportation Commission 2022 FEDERAL Legislative Program

- **Priority Projects:** Seek and preserve funding for priority transportation projects and programs in Santa Cruz County, including:
 - Projects on Highway 1
 - Santa Cruz Branch Rail Line preservation
 - Transit operations and capital projects
 - Bicycle and pedestrian facilities, including the Monterey Bay Sanctuary Scenic Trail Network (MBSST/Rail Trail)
 - Local street and roadway preservation
 - 511 implementation
 - Highway 1 Scotts Creek Bridge Replacement
 - Highway 9/SLV Corridor Complete Streets
- **Transportation Act Reauthorization Implementation**
 - Support implementation of the Infrastructure Investment and Jobs Act (IIJA) in a manner that maximizes funding for implementation of priority projects in Santa Cruz County, including formula and discretionary funding and policies.
 - As Congress works on reauthorization of the FAST Act, which expires in September 2021, support California's reauthorization principles for a **long-term, fully funded** transportation authorization that supports local agencies achieving national, state and regional goals related to infrastructure condition, safety, mobility, and air quality.
- **Transportation Funding**
 - **Raise New Revenues & Grow Existing Programs:** Support raising and indexing federal gas taxes and development of new funding mechanisms to ensure the financial integrity and solvency of the Highway Trust Fund (HTF) and Mass Transportation Account. Increase federal transportation investment all modes to bring transportation infrastructure up to a good state of repair and meet growing transportation needs in Santa Cruz County.
 - **Increase funding:** Support ~~a reauthorization bill and other~~ legislative actions that increase funding for priority projects in Santa Cruz County, including:
 - **Active Transportation:** Bicycle and pedestrian safety and mobility projects, such as the Transportation Alternatives Program (TAP).
 - **Transit:** Small Transit Intensive Cities Program (STIC), funding for acquisition of transit capital (Bus and Bus Facilities, and Low and No Emissions Bus Programs), Capital Investment Grants, Americans with Disabilities Act (ADA) implementation, state of good repair, and other transit programs. Support tax credits for the purchase of electric buses.
 - **Local Roads and Highways:** Support robust funding for core programs such as the Surface Transportation Block Grant Program (STBG), Highway Safety Improvement Program (HSIP) and bridge programs needed for local entities to address the backlog of bridge and roadway projects.
 - **Self-Help Counties:** Support programs that reward areas which have approved self-help revenue measures like Measure D and the METRO dedicated sales taxes.
 - **Planning:** Federal planning funds to address increased planning, performance measure, monitoring, and model requirements.



PRELIMINARY DRAFT

- **Transit Oriented Development (TOD):** Grants or pilot programs for comprehensive planning and infill development to connect housing, jobs, and mixed-use development with transportation options.
- **Support COVID Relief and Economic Recovery:** Support federal funding to support economic recovery, local and state responses to the public health crisis, and backfill any transportation revenue losses due to COVID-19.
- **Infrastructure Initiative:** ~~If an infrastructure package, such as the “Moving Forward Act” or~~ Climate and social spending bills: If a funding package, such as the “Build Back Better” bill, advances, support inclusion of funding for sustainable transportation and system preservation ~~ensure that the initiative increases transportation investment opportunities for~~ projects in Santa Cruz County and addresses principles for reauthorization of the transportation act. Any infrastructure package should ensure projects in Santa Cruz County are not disadvantaged in accessing those funds. The initiative should also include a significant investment of **new** federal funds for transportation, stabilize the Highway Trust Fund and not be offset by reductions to other federal programs serving Santa Cruz County residents.
- **Air Quality, Greenhouse Gas Emissions, and Climate Resiliency:** Strengthen federal partnership to improve air quality, reduce greenhouse gas (GHG) emissions, and make our communities and transportation networks resilient to a changing climate.
 - **Funding:** Support development of new resources to support climate adaptation and reduce greenhouse gas emissions from transportation (similar to those included in the Senate FAST Act reauthorization bill (S. 2302)), expand eligibility for Congestion Mitigation and Air Quality (CMAQ) and other funding programs to Santa Cruz County.
 - **Electrification:** Support federal funding, tax credits, and coordination of vehicle electrification purchase (including buses), planning and infrastructure.
 - **Mitigation:** Defend against rollbacks of California’s air quality and climate change laws and regulation, such as fuel efficiency standards and cap-and-trade programs.
 - **Resiliency:** Support resiliency and climate change preparedness and efforts that could support local efforts to improve resiliency, respond to new or worsening storm, fire, and other environmental hazards and meet regional climate goals. Support efforts to increase planning funds that help regional governments address climate change and make regional transportation infrastructure more resilient.
 - **Disaster Recovery:** Ensure the federal government provides sufficient emergency relief appropriations and federal agency resources to support rebuilding and recovery efforts for wildfire, storm, and other natural disasters. Support legislative efforts to extend the timeframe for road projects qualifying for federal disaster reimbursement to move to the construction phase from two years to six years.
- **Federal Authorization Implementation:** Support legislation and administrative strategies to implement federal transportation authorization bills in a way that ensures the best possible outcome for transportation projects in Santa Cruz County. Ensure that U.S. Department of Transportation (DOT) implementation of MAP-21, FAST Act, IJA and any new transportation act rules and regulations do not have a negative impact on local projects and programs.

PRELIMINARY DRAFT

- **Discretionary Grants:** Advocate for discretionary transportation grant awards for priority transportation projects in Santa Cruz County, including the Better Utilizing Investments to Leverage Development (BUILD, formerly TIGER) and Capital Investment Grant program.
- **Innovative Financing:** Ensure proposals for public-private partnerships and innovative financing are favorable for project implementation in Santa Cruz County. Support and expand the Transportation Infrastructure Finance and Innovation Act (TIFIA) program and make the Railroad Rehabilitation and Improvement Financing (RRIF) program more accessible to smaller public agencies.
- **Department of Transportation Budget and Annual Appropriations.** Ensure that Congress appropriates funding consistent with amounts authorized in federal transportation authorizations (e.g. ~~FAST Act~~ ~~IIJA~~), even if Continuing Resolutions (CR) are needed to keep transportation programs running each fiscal year.
- **Oppose rescissions or arbitrary cuts** that could reduce funding for transportation projects in Santa Cruz County.
- **Support transparent congressionally-directed spending** (earmarks) to allow for Congressional support of priority projects in Santa Cruz County
- **Oppose unfunded mandates** and support legislation that provides funding for past mandates.
- **Performance Measures:** Support development of performance measures which are consistent with RTC approved goals, policies, and targets and which recognize data limitations of many regions. Support open collaboration, data sharing and funding to successfully implement state and federal performance-based planning and management requirements.
- **Protect and expand transportation fringe benefits.** Reinstate the commuter benefit, which was eliminated under the tax reform bill. In addition, advocate for expanding pre-tax transportation fringe benefit eligibility to include shared mobility options, such as bike-share and shared ride carpool services.
- **Shared Mobility:** Advocate for federal legislative and regulatory updates that support shared mobility options such as bike-share, shared rides, carpooling, and shared scooters. Support expanding pre-tax transportation fringe benefit eligibility to include shared mobility options. This change would support the now-permanent Bay Area Commuter Benefits program by expanding federal tax incentives utilize alternatives to single occupancy travel to commute to work.
- **Autonomous Vehicles:** Oppose federal efforts to preempt local authority to regulate the use of autonomous vehicles in their communities.
- **Streamline Project Delivery:** Support regulations to streamline and integrate federal project delivery requirements for project planning, development, review, permitting, and environmental processes in order to reduce project costs and delays.

||rtc\serv2\shared\legislat\2021\2021-rtc-legislativeprograms-.docx